

STATE OF INDIANA
INDIANA STATE HIGHWAY COMMISSION

BRIDGES OVER 20' SPAN				
FISCAL YEAR	STATE	PROJ. NO.	FISCAL YEAR	TOTAL SHEETS
4	IND.	1-64-2(32)75	1970	1 26

INDEX						
PROJECT	STRUCTURE	TYPE	SPAN	OVER	STATION	CONTRACT NO.
1-64-2(32)75	I-64-75-5674	COMPOSITE STEEL BEAM	2 SPANS: 105'-0, 105'-0 SQUARE	SECTION 29 ROAD OVER I-64	STA. 49+62.93 LINE "S-1-A" STA. 1960+99.71 LINE "A"	B-9514
SHEET NO.	SHEET DESIGNATION	SUBJECT		S.P.R. APPROVAL		
1	INDEX AND TITLE SHEET					
2	ONE SHEET	SOIL BORINGS (I-64-75-5674S)				
3	ONE SHEET	CONTOURS				
4	S1 (I-64-75-5674S)	LAYOUT				
5	S2	GENERAL PLAN				
6	S3	ABUTMENT NO. 1 DETAILS				
7	S4	ABUTMENT NO. 1 DETAILS AND BILL OF MATERIALS				
8	S5	BENT NO. 2 DETAILS AND BILL OF MATERIALS				
9	S6	BENT NO. 2 DETAILS				
10	S7	BENT NO. 3 DETAILS AND BILL OF MATERIALS				
11	S8	FRAMING PLAN AND STRUCTURAL STEEL DETAILS				
12	S9	STRUCTURAL STEEL DETAILS				
13	S10	FLOOR DETAILS				
14	S11	FLOOR DETAILS AND BILL OF MATERIALS				
15	ONE SHEET	SUMMARY				
16A	ONE SHEET	BRIDGE SUMMARY				
16B	ONE SHEET	BRIDGE ESTIMATE OF QUANTITIES				
16C	RD. SHEET NO. 192-195	CROSS SECTIONS - LINE "A"		RD. PROJ. I-64-2(2)75		
16D	NINE SHEETS	CROSS SECTIONS - LINE "S-1-A"				
17A	S10A	FLOOR DETAILS				
1A	RD. SHEET NO. 7	TYPICAL CROSS SECTION - GRADING		RD. PROJ. I-64-2(2)75		
1B	RD. SHEET NO. 10	TYPICAL CROSS SECTION - GRADING		RD. PROJ. I-64-2(2)75		
1C	RD. SHEET NO. 23	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1D	RD. SHEET NO. 24	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1E	RD. SHEET NO. 25	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1F	RD. SHEET NO. 26	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1G	RD. SHEET NO. 27	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1H	RD. SHEET NO. 28	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1I	RD. SHEET NO. 29	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1J	RD. SHEET NO. 30	ROAD PLAN & PROFILE SHEET - LINE "A"		RD. PROJ. I-64-2(2)75		
1K	ONE SHEET	PLAN & PROFILE SHEET - LINE "S-1-A"				
1L	RD. SHEET NO. 2	PL. 91		RD. PROJ. I-64-2(2)75		
1M	RD. SHEET NO. 10	DETAILS - R.C. BRIDGE DRAINAGE TURNOUT		RD. PROJ. I-64-2(2)75		

BRIDGE PLANS
FOR SPANS OVER 20 FEET
ON

(1) 40 PE PHASE I
(5) 73 PE FINAL PHASE
(10) 73 R/W

F. A. PROJECT NO. I-64-2(32)75 CONSTR.
SECTION 29 ROAD OVER I-64

BEGINNING AT A POINT ON PROPOSED LINE "A" APPROX. 324.7 FEET WEST OF ITS INTERSECTION WITH LINE "S-1-A" AND EXTENDING EAST A DISTANCE OF APPROX. 575.0 FEET TO A POINT ON PROPOSED LINE "A" APPROX. 250.3 FEET EAST OF ITS INTERSECTION WITH LINE "S-1-A", ALL IN SECTION 29, T-3-S., R-2-W., CLARK TOWNSHIP, PERRY COUNTY.

	I-64	S-1-A	
ROADWAY LENGTH:	0.109 MI.	0.244 MI.	MAX. GRADE: 9.00% (LINE "S-1-A")
BRIDGE LENGTH:	0.000 MI.	0.040 MI.	
TOTAL LENGTH:	0.109 MI.	0.284 MI.	

THESE PLANS HAVE BEEN REDESIGNED TO INCLUDE THE SPECIAL CRITERIA OF "HIGHWAY DESIGN AND OPERATIONAL PRACTICES RELATED TO HIGHWAY SAFETY" DATED FEB., 1967
BY SORRELL & MATTIS ASSOCIATES, INC. JULY 16, 1969
FOR REID, QUEBE, ALLISON, WILCOX & ASSOCIATES, INC.

INDEX CONTINUED			
SHEET NO.	SHEET DESIGNATION	SUBJECT	DATE
16	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16A	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16B	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16C	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16D	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16E	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
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16Q	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16R	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16S	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16T	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16U	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16V	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
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16Y	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
16Z	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
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18	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
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18Z	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
19	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
19A	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72
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19Z	BRIDGE STA. 07	STANDARD BRIDGE DETAILS	8-2-72

TRAFFIC DATA		
I-64	SEC. 29 RD.	
A.D.T. (1963)	6384 V.P.D.	41 V.P.D.
A.D.T. (1969 PROJECTED)	12,000 V.P.D.	65 V.P.D.
DESIGN SPEED	70 M.P.H.	50 M.P.H.
ACCESS CONTROL	FULL	NONE

FINAL PHASE DESIGNED AND PREPARED BY
SORRELL & MATTIS ASSOCS. INC.
ENGINEERS
INDIANAPOLIS, IND.

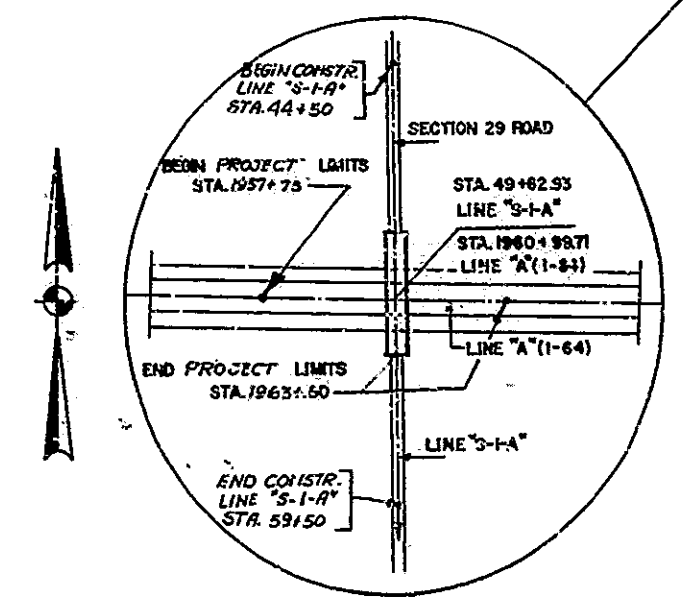
James D. Mattis 7-16-69

FINAL PHASE SUBMITTED FOR APPROVAL BY
REID, QUEBE, ALLISON, WILCOX & ASSOCS.
ENGINEERS
INDIANAPOLIS, IND.

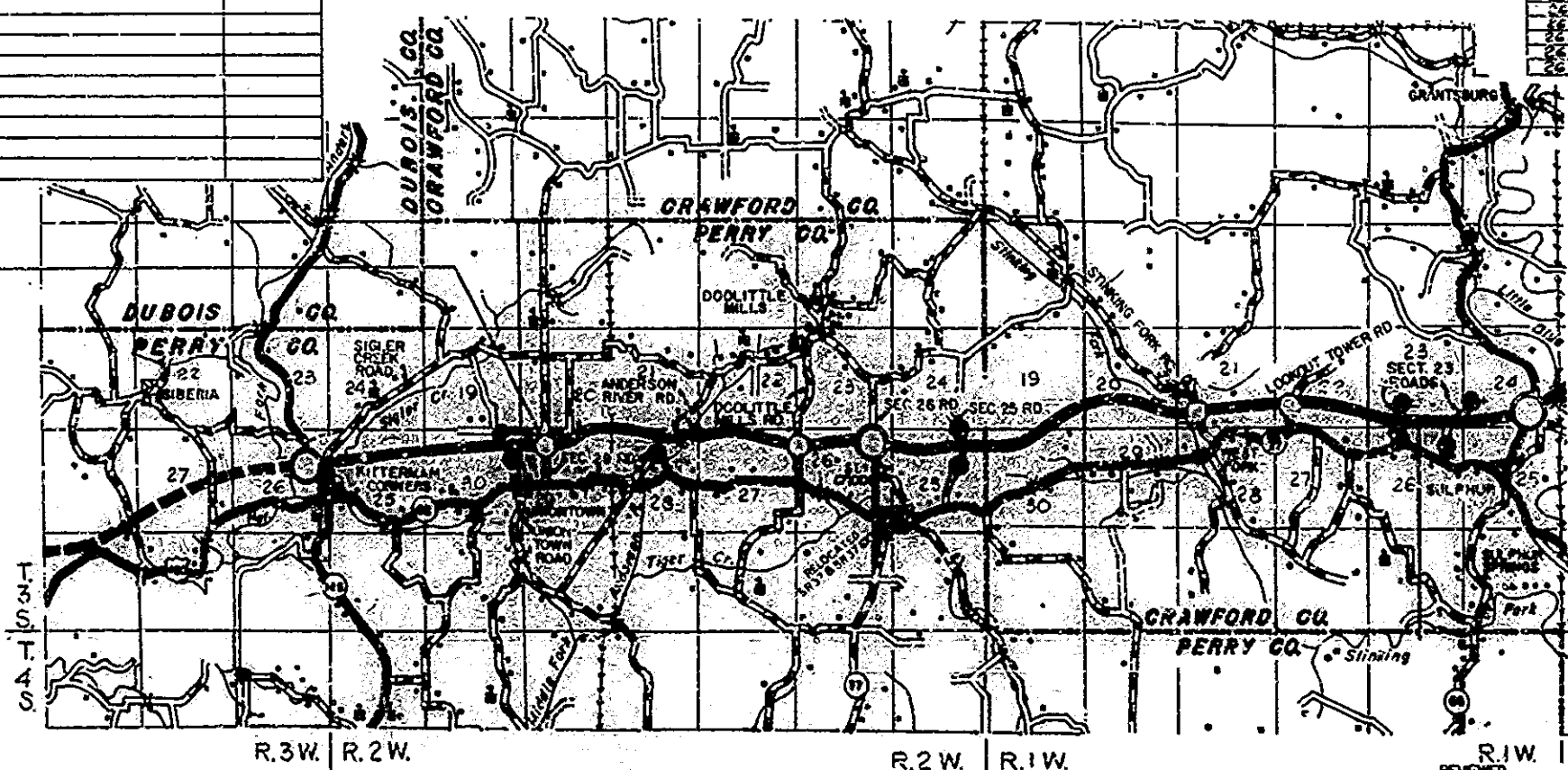
William F. Quebe 7-16-69

APPROVED 9-24-69
W. F. Quebe
CHIEF ENGINEER - PUBLIC ROADS DIVISION

DEPARTMENT OF COMMERCE
BUREAU OF PUBLIC ROADS
APPROVED: _____
DIVISION ENGINEER DATE 19



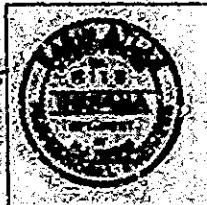
PROJECT LIMITS
I-64-75-5674S



PHASE I SUBMITTED FOR APPROVAL DATE April 26, 1969

INDIANA STATE HIGHWAY COMMISSION
STANDARD SPECIFICATIONS DATED 1977
TO BE USED WITH THESE PLANS.

Rev. 9-7-73 Sheet # 15, 6, 7, 9, 10, 13, 14, 14-B, 1 added 13A, Deleted 24.
H. W. COFFNER, INC.
ENGINEERS
20 N. WACKY DR. CHICAGO, ILLINOIS



REVISIONS	
DATE	SHEET NO.
7-17-73	Proj. Designation Changed from I-64-2(2)75 to I-64-2(32)75
7-16-73	13thrd. M.I.C. Note #1, 2, 3, 4, 5 Deleted; 14thrd. M.I.C. Note #1, 2, 3, 4, 5, 6, 7, 8, 9, 10, 11, 12, 13, 14, 15, 16, 17, 18, 19, 20, 21, 22, 23, 24, 25, 26, 27, 28, 29, 30, 31, 32, 33, 34, 35, 36, 37, 38, 39, 40, 41, 42, 43, 44, 45, 46, 47, 48, 49, 50, 51, 52, 53, 54, 55, 56, 57, 58, 59, 60, 61, 62, 63, 64, 65, 66, 67, 68, 69, 70, 71, 72, 73, 74, 75, 76, 77, 78, 79, 80, 81, 82, 83, 84, 85, 86, 87, 88, 89, 90, 91, 92, 93, 94, 95, 96, 97, 98, 99, 100, 101, 102, 103, 104, 105, 106, 107, 108, 109, 110, 111, 112, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 124, 125, 126, 127, 128, 129, 130, 131, 132, 133, 134, 135, 136, 137, 138, 139, 140, 141, 142, 143, 144, 145, 146, 147, 148, 149, 150, 151, 152, 153, 154, 155, 156, 157, 158, 159, 160, 161, 162, 163, 164, 165, 166, 167, 168, 169, 170, 171, 172, 173, 174, 175, 176, 177, 178, 179, 180, 181, 182, 183, 184, 185, 186, 187, 188, 189, 190, 191, 192, 193, 194, 195, 196, 197, 198, 199, 200, 201, 202, 203, 204, 205, 206, 207, 208, 209, 210, 211, 212, 213, 214, 215, 216, 217, 218, 219, 220, 221, 222, 223, 224, 225, 226, 227, 228, 229, 230, 231, 232, 233, 234, 235, 236, 237, 238, 239, 240, 241, 242, 243, 244, 245, 246, 247, 248, 249, 250, 251, 252, 253, 254, 255, 256, 257, 258, 259, 260, 261, 262, 263, 264, 265, 266, 267, 268, 269, 270, 271, 272, 273, 274, 275, 276, 277, 278, 279, 280, 281, 282, 283, 284, 285, 286, 287, 288, 289, 290, 291, 292, 293, 294, 295, 296, 297, 298, 299, 300, 301, 302, 303, 304, 305, 306, 307, 308, 309, 310, 311, 312, 313, 314, 315, 316, 317, 318, 319, 320, 321, 322, 323, 324, 325, 326, 327, 328, 329, 330, 331, 332, 333, 334, 335, 336, 337, 338, 339, 340, 341, 342, 343, 344, 345, 346, 347, 348, 349, 350, 351, 352, 353, 354, 355, 356, 357, 358, 359, 360, 361, 362, 363, 364, 365, 366, 367, 368, 369, 370, 371, 372, 373, 374, 375, 376, 377, 378, 379, 380, 381, 382, 383, 384

SEC. 20, T-3-S, R-2-W
CLARK TOWNSHIP
PERRY COUNTY

IRA f HATTIE
ANDERSON

VERNAL f AMOS
UNDERHILL

SEC. 29, T-3-S, R-2-W
CLARK TOWNSHIP
PERRY COUNTY

IRA f HATTIE
ANDERSON

FLORENCE f ARLIE
RICKENBAUGH

TELEPHONE SERVICE BY:
PERRY SPENCER R.T. CO.
TELL CITY, IND.

ELECTRIC SERVICE BY:
SOUTHERN INDIANA R.E.C. INC.
TELL CITY, IND.

ALL R/W ON THIS SHEET TO BE AS
SHOWN. LIMITED ACCESS PROVISIONS
TO APPLY WHERE INDICATED. R/W
ON THIS SHEET TO BE MEASURED
FROM LINE "S-I-A" EXCEPT AS SHOWN.

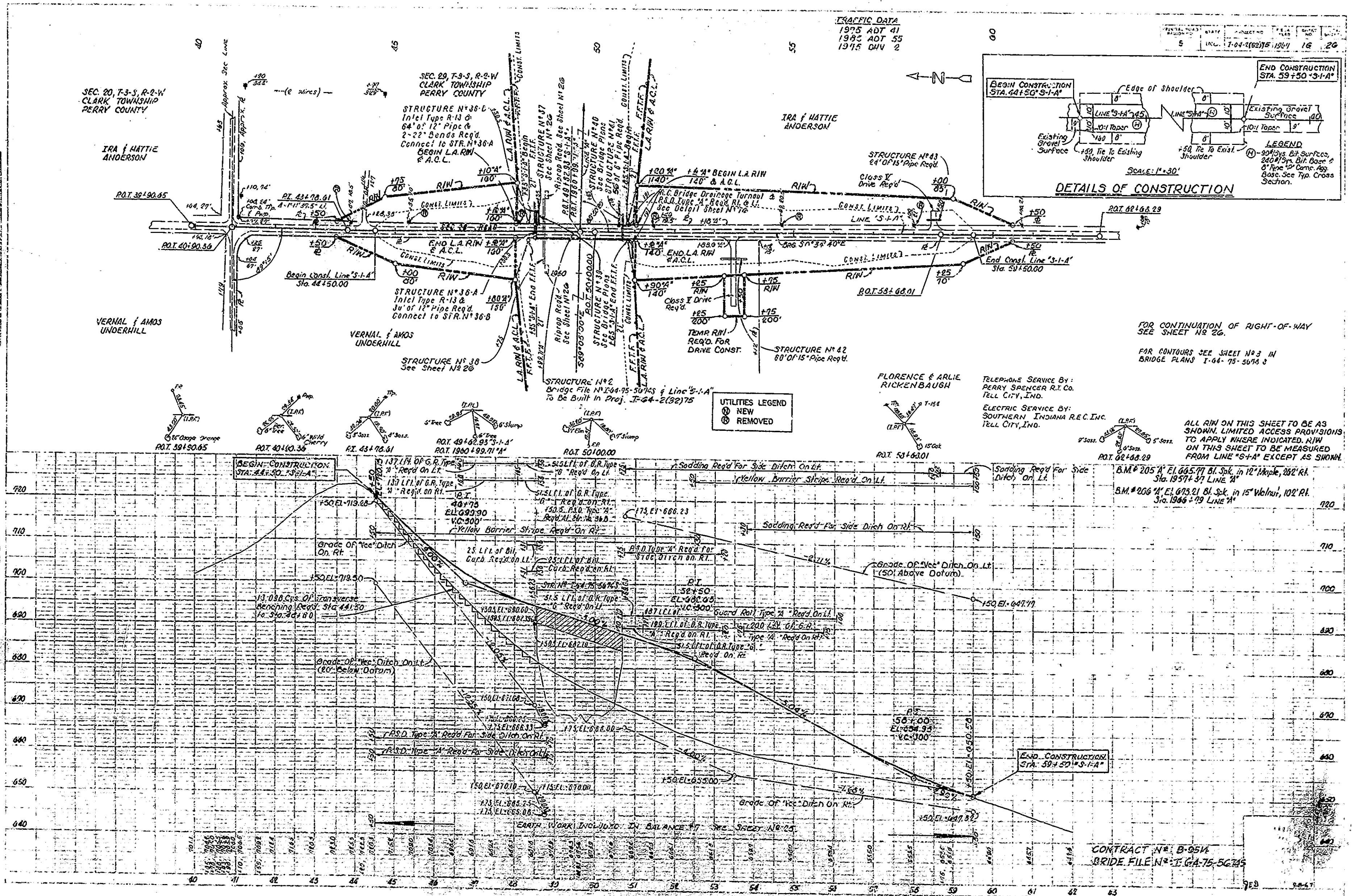
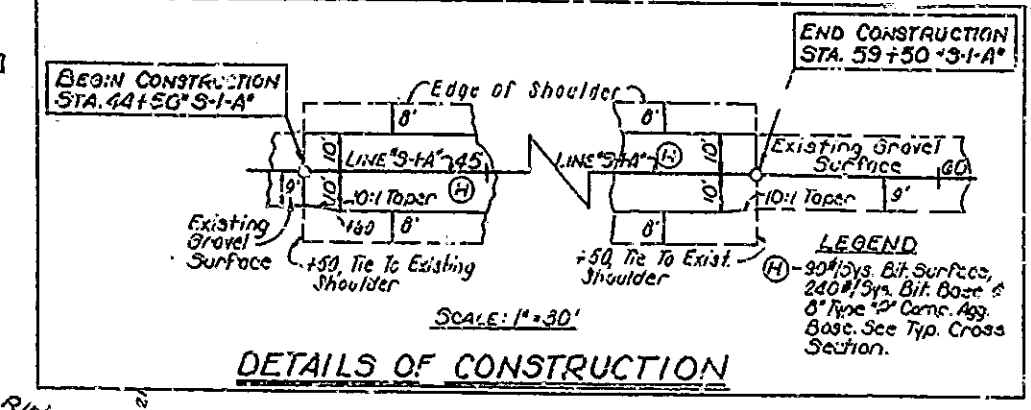
CONTRACT NO. B-954
BRIDGE FILE NO. T-64-75-5675

SEC. 29 ROAD

TRAFFIC DATA
1975 ADT 41
1985 ADT 55
1995 OHV 2

00

PROJECT NO. 7-64-1(62)75 102/1
SHEET NO. 16 26

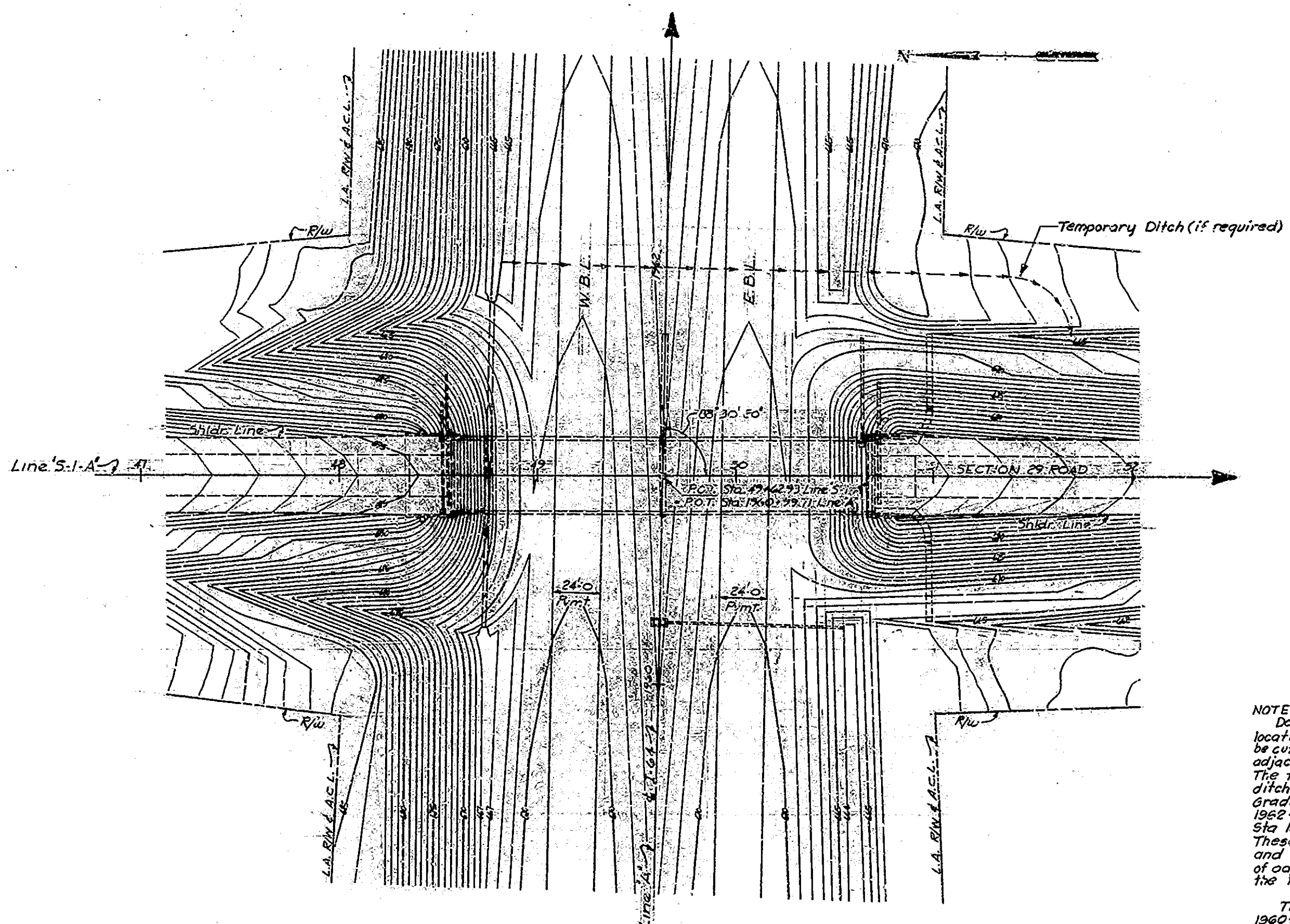


PLAN
SCALE: 1" = 40'
DATE: 10/1/75
BY: J. W. BROWN

PROFILES
SCALE: 1" = 10'
DATE: 10/1/75
BY: J. W. BROWN

Revised by JWB

BRIDGES OVER 20' SPAN					
FED. ROAD DIST. NO.	STATE	PROJECT NO.	FISCAL YEAR	CHECK NO.	TOTAL SHEETS
4	IND.	I-64-2 (32) 75	1970	3	26



NOTE:
 Dotted line (at Sta. 1962+00-Line "A") represents location of a Temporary Drainage Ditch which shall be cut if the grading for this Project precedes adjacent mainline grading in Project I-64-2(21)75. The temp. ditch shall connect with the normal Vee ditch left of approximate Sta 51+75-Line "S-I-A". Grades of the mainline ditches forward of Sta. 1962+00 shall be modified to flow back to Sta 1962+00 as directed by the Engineer. These ditches shall be restored to normal grade and the temp. ditch filled following the completion of adjacent grading and prior to completion of the Project.

The mainline ditch right of Sta 1957+75 to 1960+25 shall be cut in Project I-64-2(21)75.

CONTOURS
INDIANA STATE HIGHWAY COMMISSION

SCALE: 1" = 30'
 SUBMITTED FOR APPROVAL *James D. Matlock* July 16, 1969
 PROJECT: I-64-2(32)75
 BRIDGE CONTRACT NO. B-9514
 BRIDGE FILE: I-64-75-56745

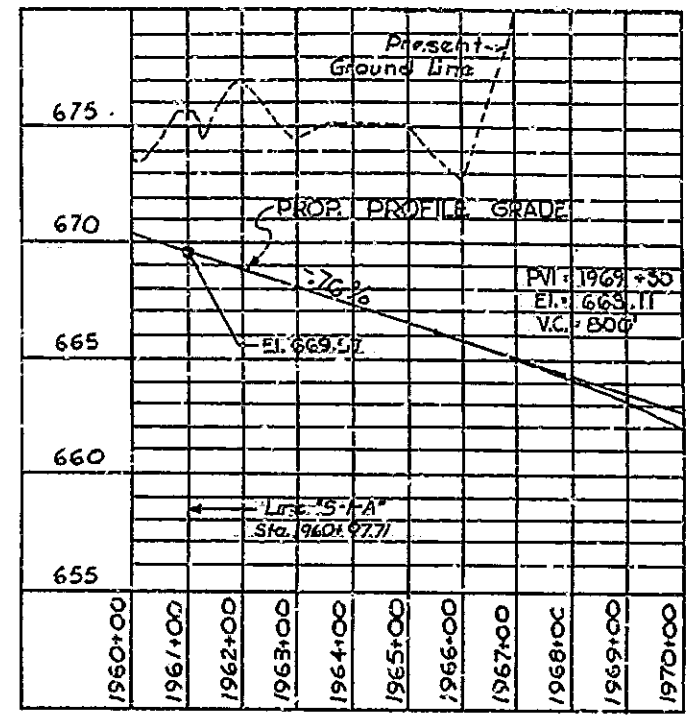
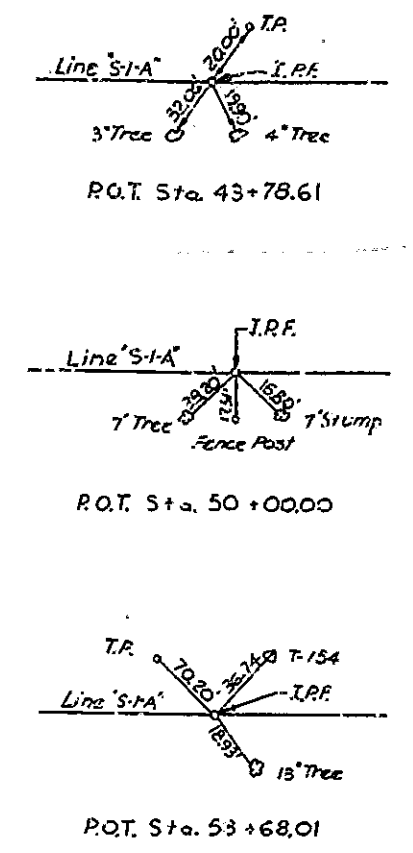
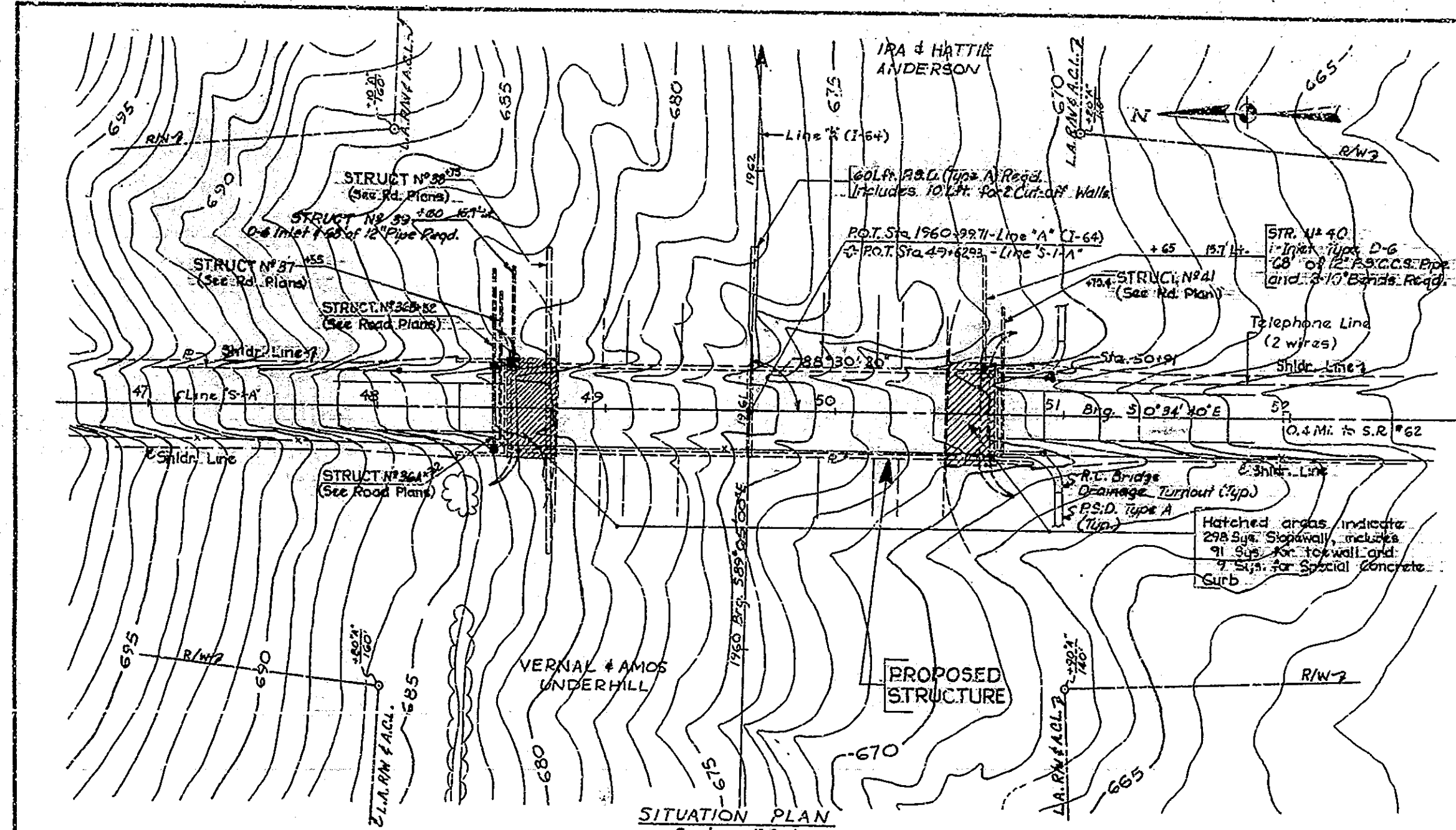


DESIGNED: CKD
 DRAWN: LG 5-22-69 CKD, EM 5-27-69
 TRACED: CKD

Rn 7-16-75 Temp. Ditch Added.

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS
I-64-2(32)75	SECTION 29 ROAD	3	26

BRIDGES OVER 20' SPAN					
PUB. ROAD DIST.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-64-2(37)2	1979	4	20



PROFILE OF LINE "A"
Scale: Horiz. 1"=200' Vert. 1"=5'

SEC. 29, T3S, R2W
CLARK TWP.
PERRY COUNTY

This is STR. No. 2
on Rd. Proj. I-64-2(u) 73
For Slopes, see "CONTOURS" sheet.

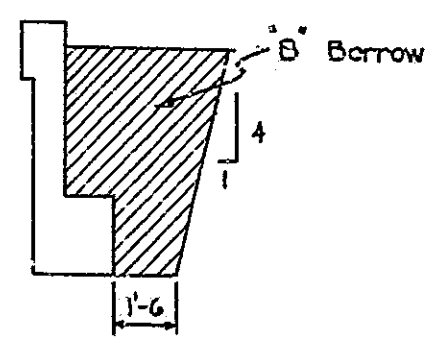
NOTES:
Present Structure - None
See Art. 102.05 of the Specifications regarding Test Pit Data.

SURVEY BOOKS:
Plan Roll I-64-2-9313T
Book Road # 9311 L (A)

UTILITIES
Telephone Service by Perry Spencer R.T. Co.
1301 Main St. Tall City, Indiana

EARTHWORK TABULATION

* FILL + 20% 47560 CYS
* UNCLASSIFIED EXCAV. 47530 CYS
SURPLUS EXCAV. 30 CYS
BORROW 0 CYS
* Includes 18098 Cys. for Benching along Line "S-1-A"



TYP. SECTION THRU WINGS
Not to Scale

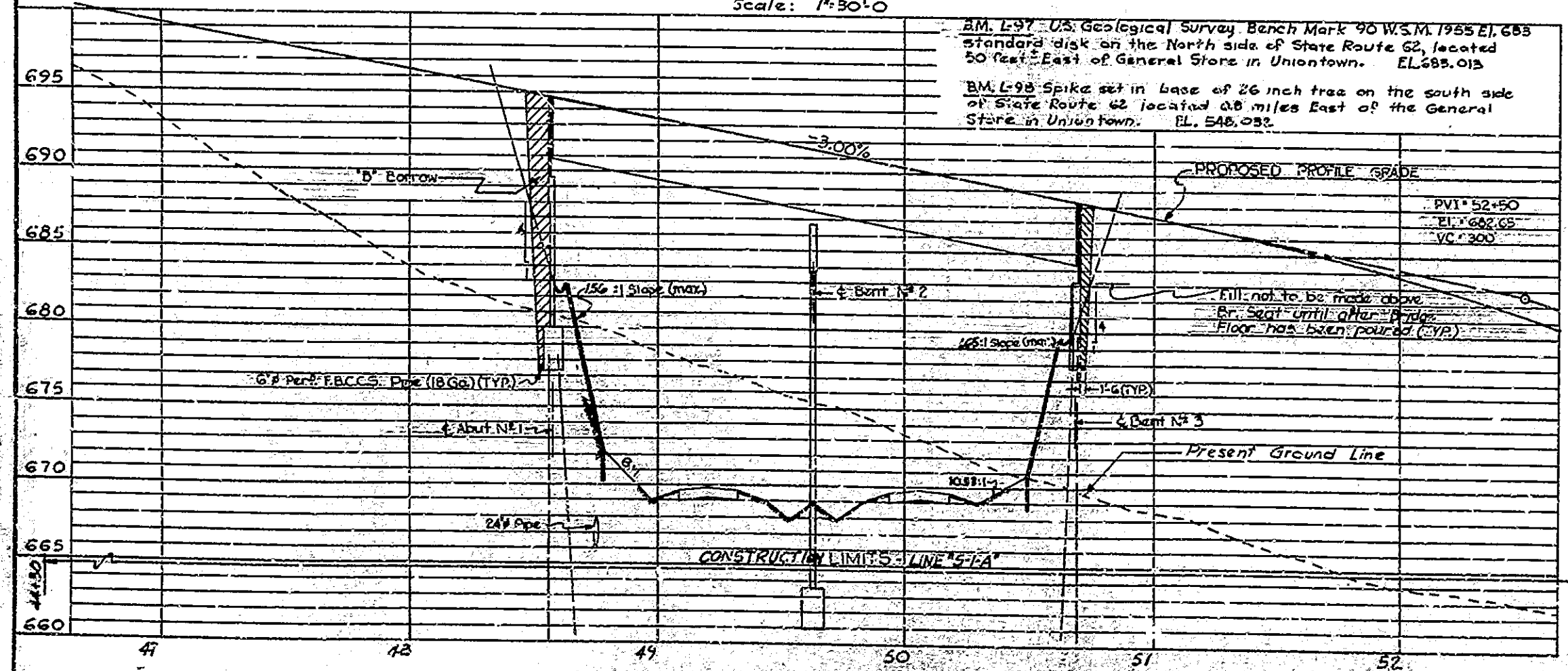
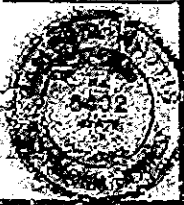
LAYOUT

CONTINUOUS COMPOSITE STEEL BEAM BRIDGE
2 SPANS @ 105'-0", SQUARE, 34'-0" CLEAR RDWY., 2 @ 3" CURBS
SECTION 29 ROAD OVER I-64

INDIANA STATE HIGHWAY COMMISSION
PERRY COUNTY

SCALE: AS NOTED July 16, 1969

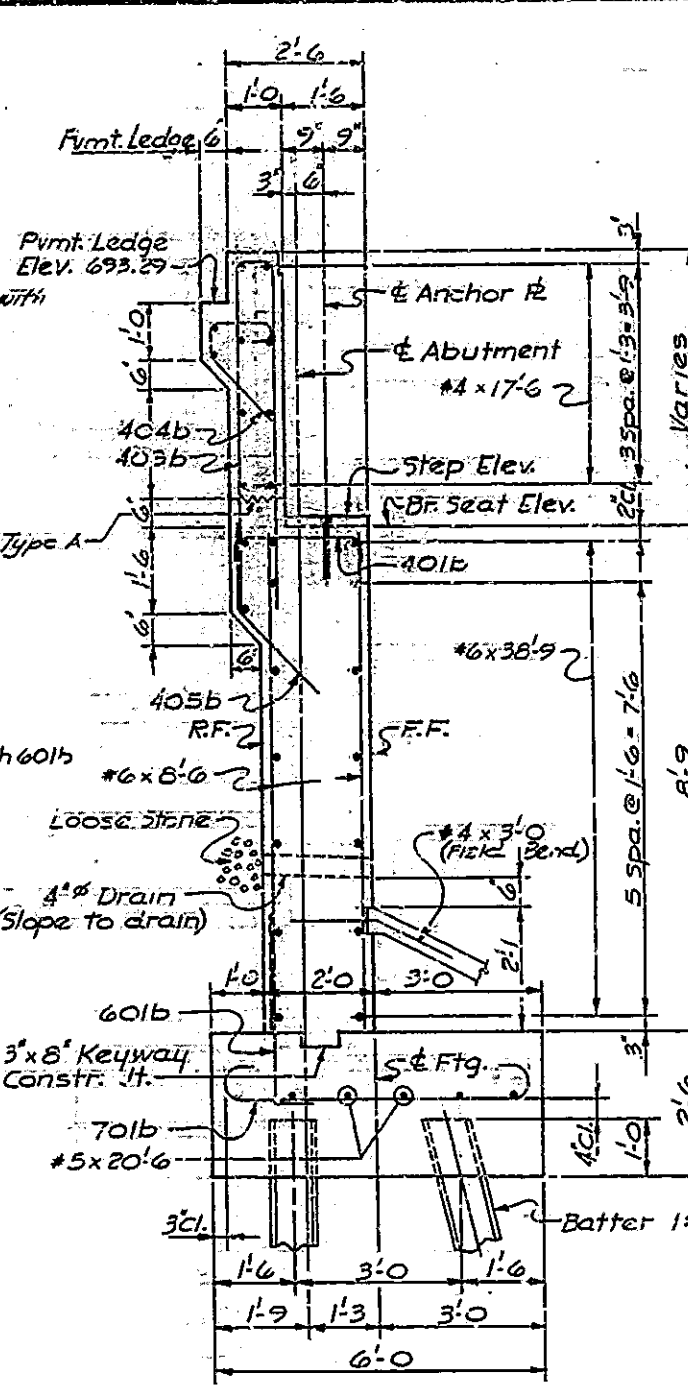
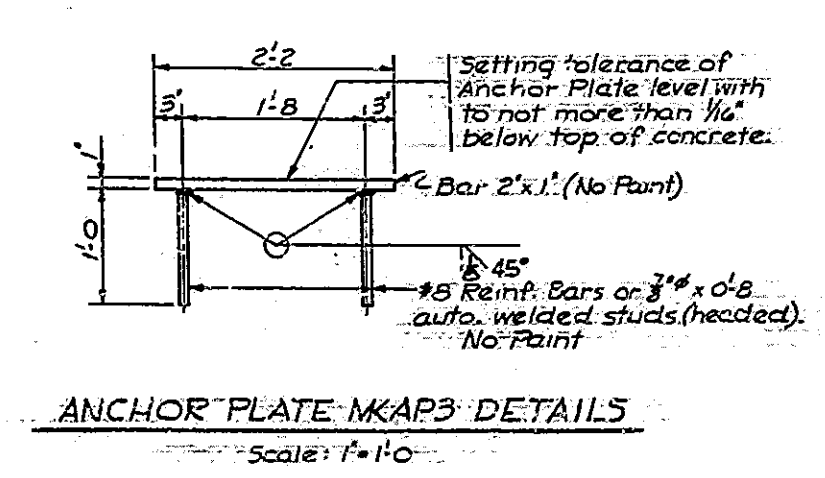
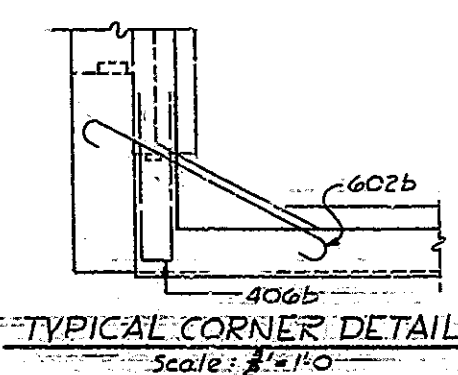
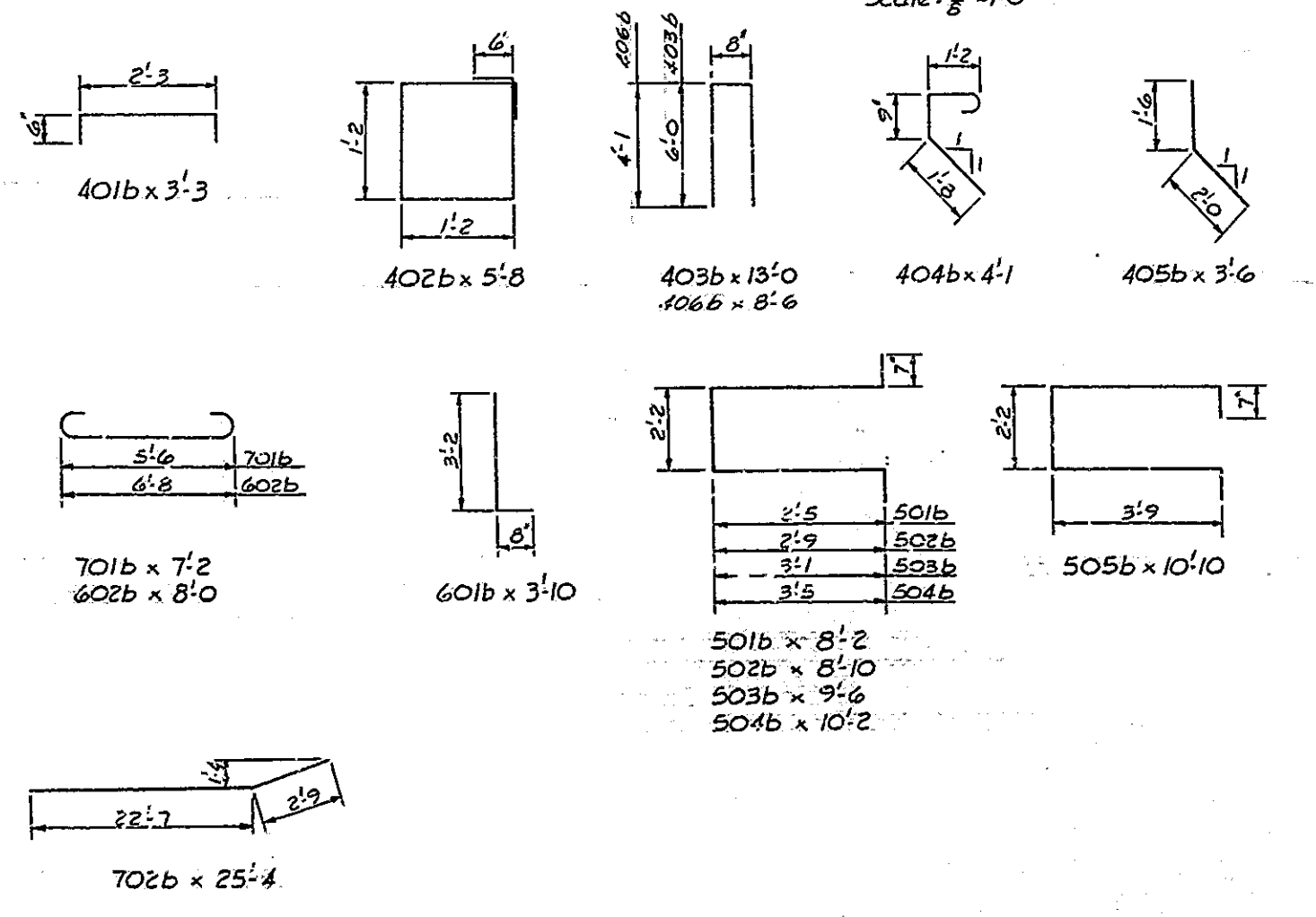
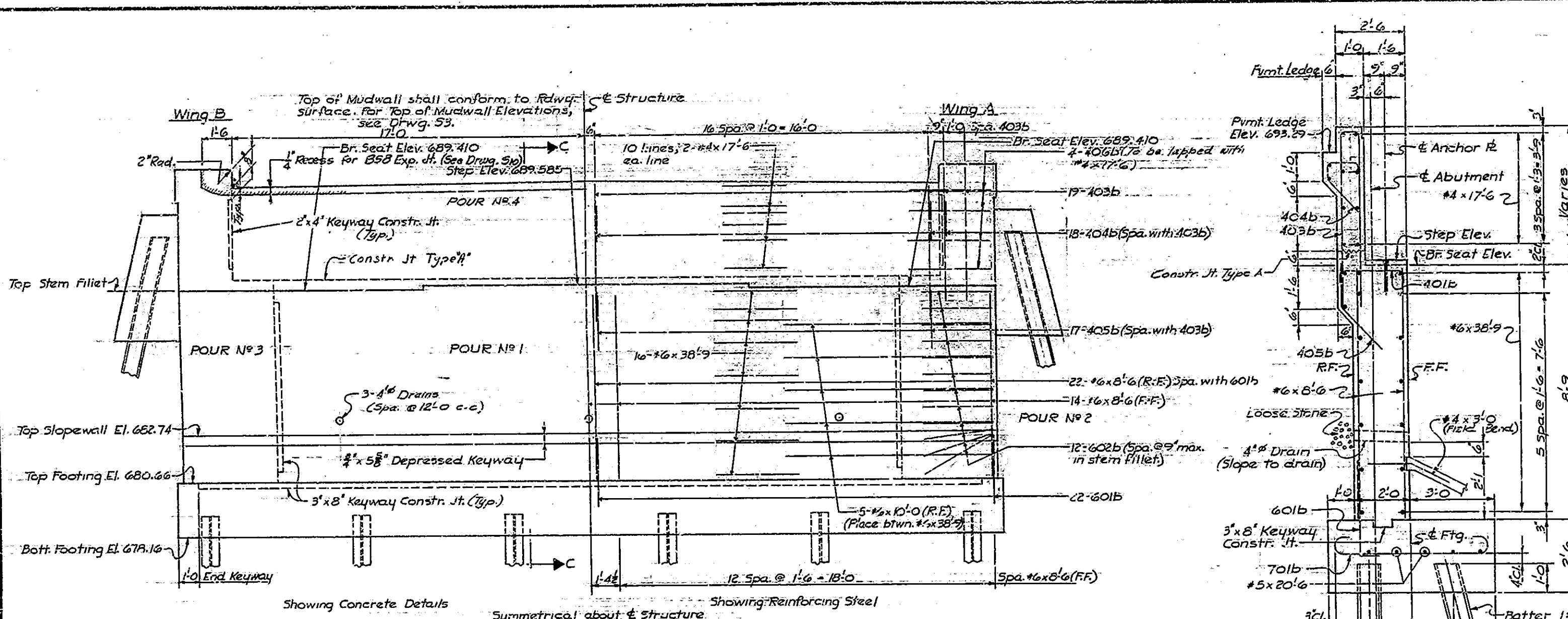
SUBMITTED FOR APPROVAL *James D. Mott*
DRAWING: 51 OF 11
PROJECT: I-64-2(37)2
BRIDGE CONTRACT NO. 3-9514
BRIDGE FILE: I-64-75-5674 S



PROFILE ON PROPOSED ROADWAY LINE "S-1-A"
SCALE: VERTICAL 1"=5'-0" HORIZONTAL 1"=30'-0"

DESIGNED BY: CND
DRAWN BY: JLB
CHECKED BY: JLB
TRACED BY: JLB

REV. 9-7-73 SEE ETC.
REV. 7-16-73 TCC/CAN. CHL: H.R.P.



SECTION C-C
Scale: 1/2" = 1'-0"

BILL OF MATERIALS (Cont)

MISCELLANEOUS	
Anchor Plates MKAP3	7 ea.
14-10 BP42 Piles	
12 x 20'0 approx. 4	
2 x 30'0 approx. 2	300 Lft.

BRIDGES OVER 20' SPAN

PUB. ROAD DIST. NO.	STATE	FED. AID PROJ. NO.	STATE PROJ. NO.	SHEET NO.	TOTAL SHEETS
4	IND.	I-64-2 (32)75	1970	7	26

BILL OF MATERIALS

REINFORCING STEEL

Size & Mark	No of Bars	Length	Weight (Lbs)
701b	14	7'-2"	
702b	20	25'-4"	
Total	34		2120
601b	24	3'-10"	
602b	24	8'-0"	
#6	16	38'-9"	
#6	16	21'-0"	
#6	4	20'-3"	
#6	4	18'-6"	
#6	4	18'-0"	
#6	4	16'-9"	
#6	4	14'-0"	
#6	4	12'-3"	
#6	8	11'-6"	
#6	26	10'-0"	
#6	2	9'-3"	
#6	71	8'-6"	
#6	4	8'-3"	
#6	2	7'-6"	
#6	4	6'-6"	
#6	2	5'-6"	
Total	66		4168
501b	2	8'-2"	
502b	2	5'-10"	
503b	2	9'-6"	
504b	2	10'-2"	
505b	2	10'-10"	
#5	12	20'-6"	
#5	4	17'-0"	
#5	4	13'-9"	
#5	4	13'-0"	
#5	4	12'-3"	
#5	4	11'-3"	
#5	4	10'-6"	
#5	4	9'-9"	
#5	4	9'-0"	
#5	4	8'-3"	
#5	8	7'-9"	
#5	4	6'-0"	
#5	4	5'-6"	
#5	4	5'-3"	
#5	4	4'-6"	
#5	4	3'-6"	
Total	65		948
401b	38	3'-3"	
402b	48	5'-8"	
403b	36	13'-0"	
404b	36	4'-1"	
405b	34	3'-6"	
406b	6	8'-6"	
#4	20	17'-6"	
#1	40	3'-0"	
Total	34		1132
TOTAL STEEL			8368
CONCRETE (Cys)			
Class "A" Conc.			
Pour No 1			21.4
Pour No 2			14.3
Pour No 3			13.3
Pour No 4			6.5
Total Class "A" in Substructure			56.5
Class "B" Conc. in Ftg.			22.0

ABUTMENT NO. 1 DETAILS &
BILL OF MATERIALS

INDIANA STATE HIGHWAY COMMISSION

SCALE: As Noted

SUBMITTED FOR APPROVAL: *James D. ...* July 16, 1969

DRAWING: 5 of 11
PROJECT: I-64-2(32)75
BRIDGE CONTRACT NO. 8-9514
BRIDGE FILE: I-64-75-56745

DESIGNED BY: J.S. ...
DRAWN BY: J.S. ...
CHECKED BY: J.S. ...

REV. 9-7-73 Reinf. Bars, Bill of Mat'l
REV. 7-16-73 Constr. Jt., 4" Drains, Reinf. Bars, Bill of Materials, Notes, Piers

Notes:
For Reinforcing Bar Notes, see Br. Std. C.
For additional details, see Drawing 53.
Pour No. 1 to be poured after floor slab has been poured.
For Construction Joint Type A see Br. Std. C.

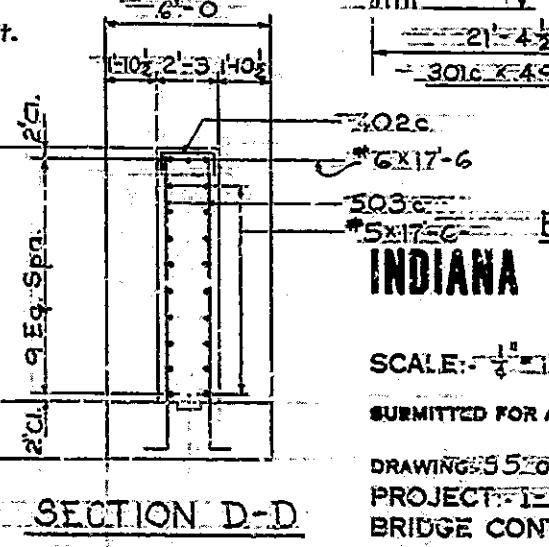
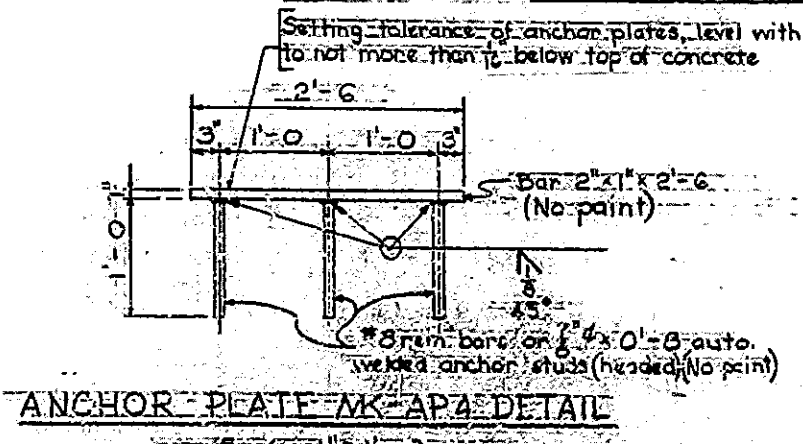
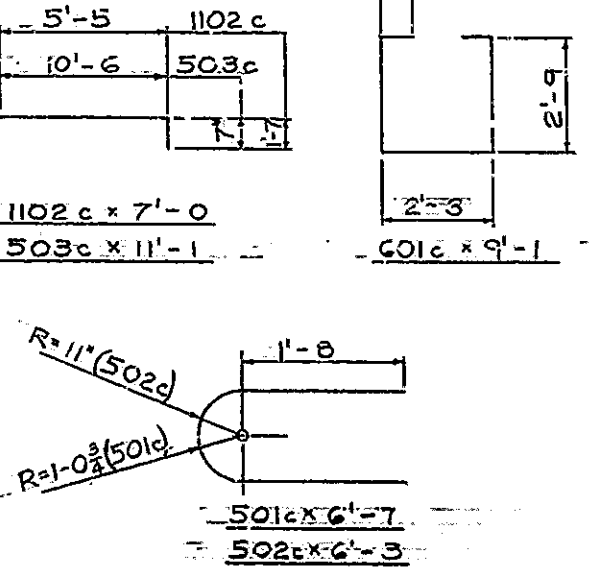
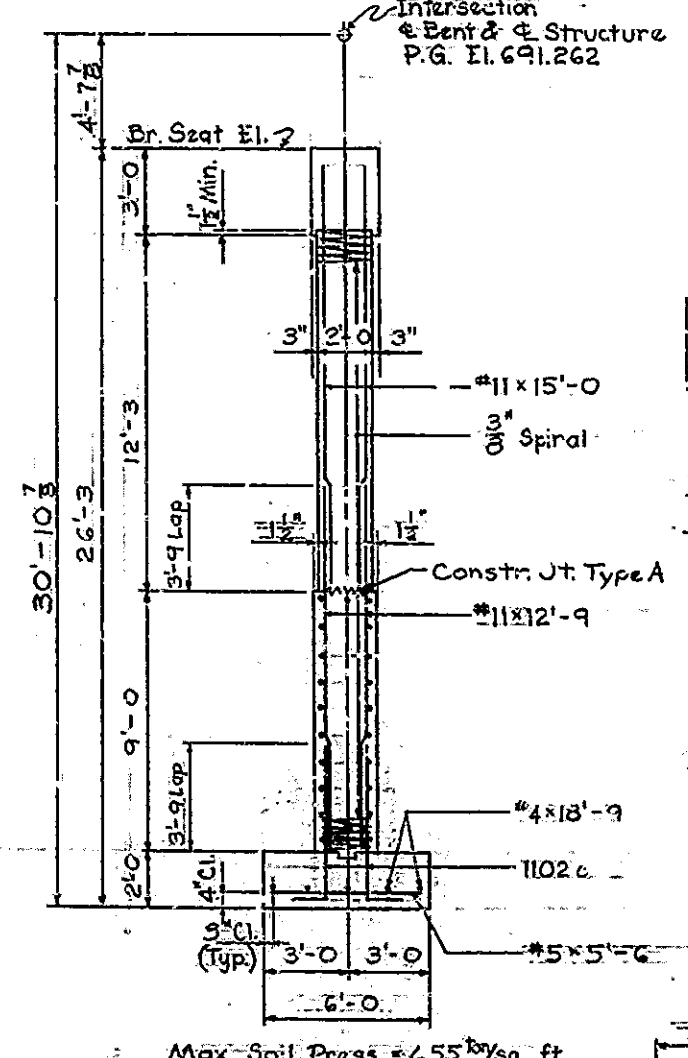
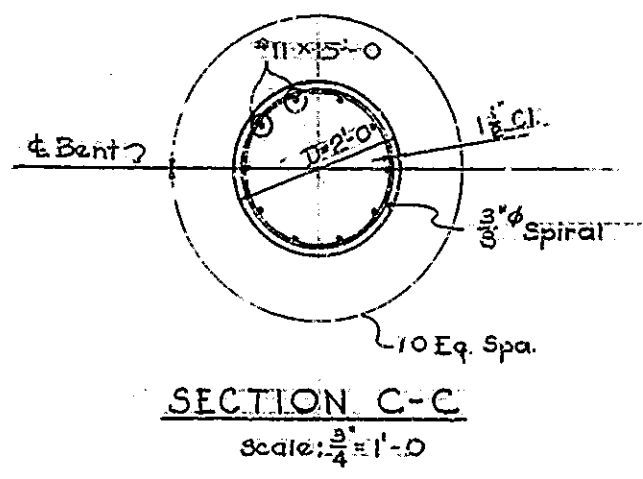
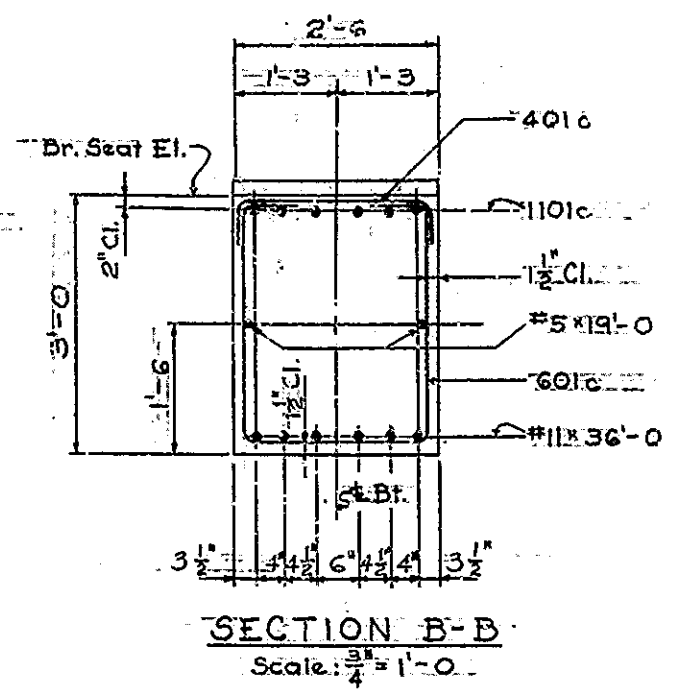
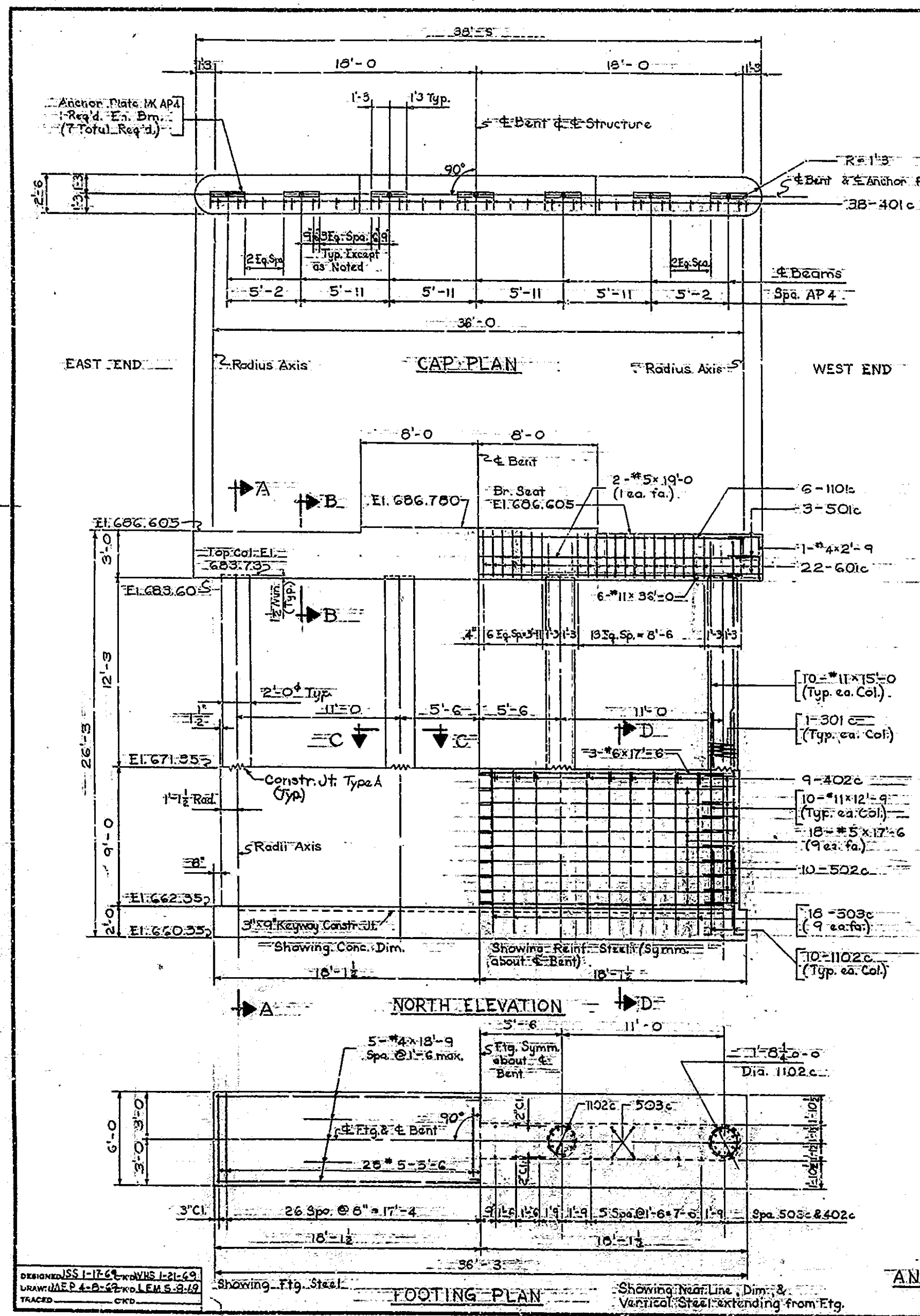


BRIDGE OVER 20' SPAN					
FED. ROAD DIST. NO.	STATE	PROJECT NO.	TOTAL SHEETS	SHEET NO.	TOTAL SHEETS
4	IND.	1-64-1870	8	8	26

BILL OF MATERIALS

REINFORCING STEEL			
Size & Mark	No of Bars	Length	Weight
#10c	6	39'-2"	
#102c	40	7'-0"	
#11	6	36'-0"	
#11	40	15'-0"	
#11	40	12'-9"	
TOTAL #11 978#			
#601c	44	9'-1"	
#6	6	17'-6"	
TOTAL #6 758#			
#501c	6	6'-7"	
#502c	20	6'-3"	
#503c	36	11'-1"	
#5	4	19'-0"	
#5	36	17'-6"	
#5	55	5'-6"	
TOTAL #5 1640#			
#401c	38	3'-3"	
#402c	18	2'-11"	
#4	10	18'-9"	
#4	2	2'-9"	
TOTAL #4 243#			
#301c	4	494'-9"	744#
TOTAL STEEL 13,166#			
CONCRETE		MISCELLANEOUS	
Class A Conc.		Anchor P's MK-AP4	
Cap	108 cys.	T ea.	
Columns 4 @ 1.45 cys.	5.8 cys.		
TOTAL Class A Conc.	114.6 cys.		
Class B Conc.			
Crashwall	26.1 cys.		
Footing	16.1 cys.		

NOTES:
 For Reinforcing Bar Notes, see Br. Std. C1.
 Anchor Plates AP4 to be preset in the concrete.
 Provide 1/2 turns at both top and bottom and 1/4 turns for lap of spiral reinforcing.
 See Draw. S-11 for pipe clamp attachments to cap and crashwall.
 For Construction Joint Type A see Br. Std. C3.



BENT No. 2 DETAILS & BILL OF MATERIALS
INDIANA STATE HIGHWAY COMMISSION

SCALE: 1/4" = 1'-0", UNLESS NOTED
 SUBMITTED FOR APPROVAL: *James D. [Signature]* July 16, 1969
 DRAWING: 55 OF 11
 PROJECT: I-64-2(3)-75
 BRIDGE CONTRACT NO. B-3514
 BRIDGE FILE: I-64-2(3)-75-674.5



Rev. 7-16-73 TCC/WTN/CHM.

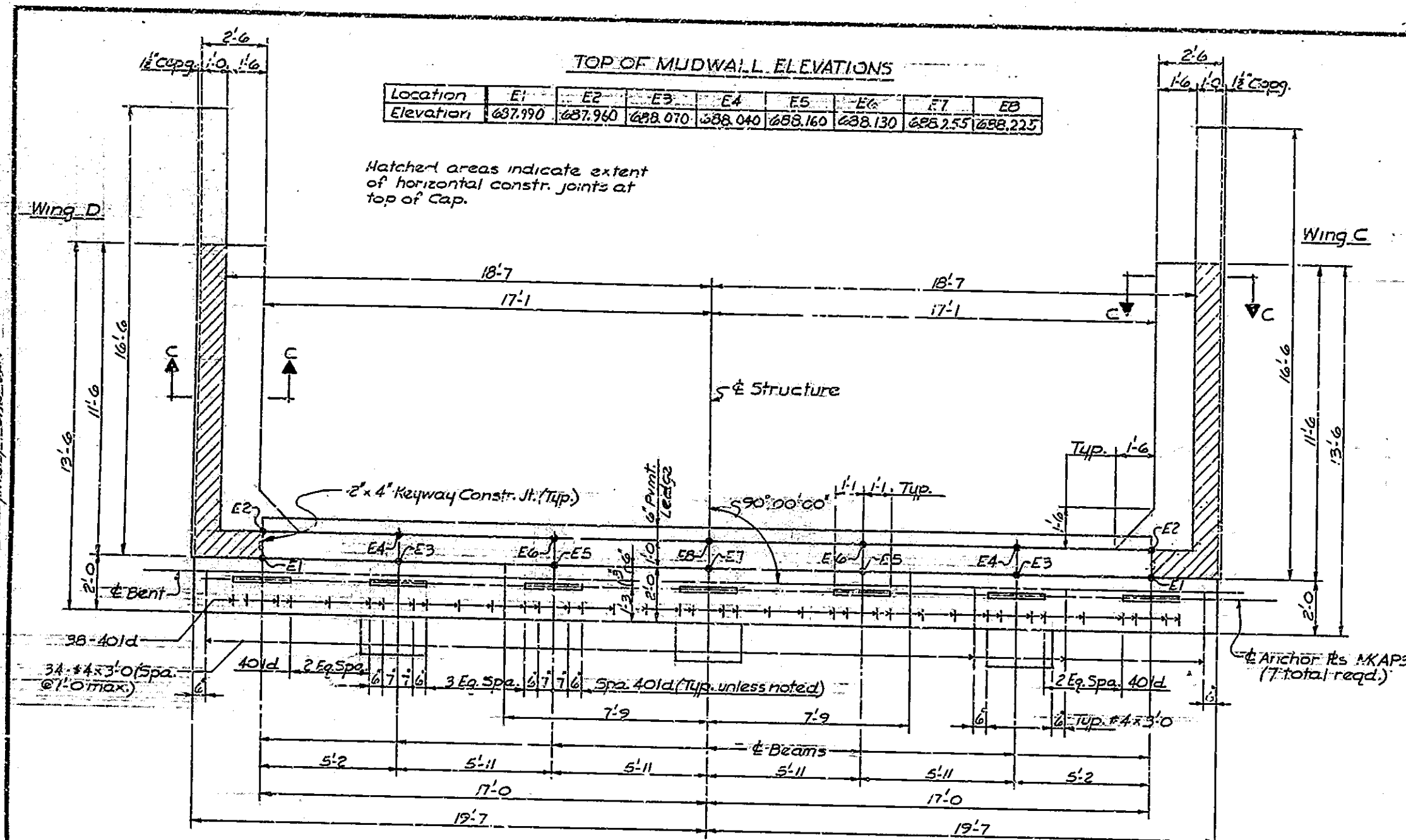
DESIGNED: SS-1-17-69
 DRAWN: MRP-4-9-69
 CHECKED: LEM-5-8-69
 TRACED: CWD

PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS

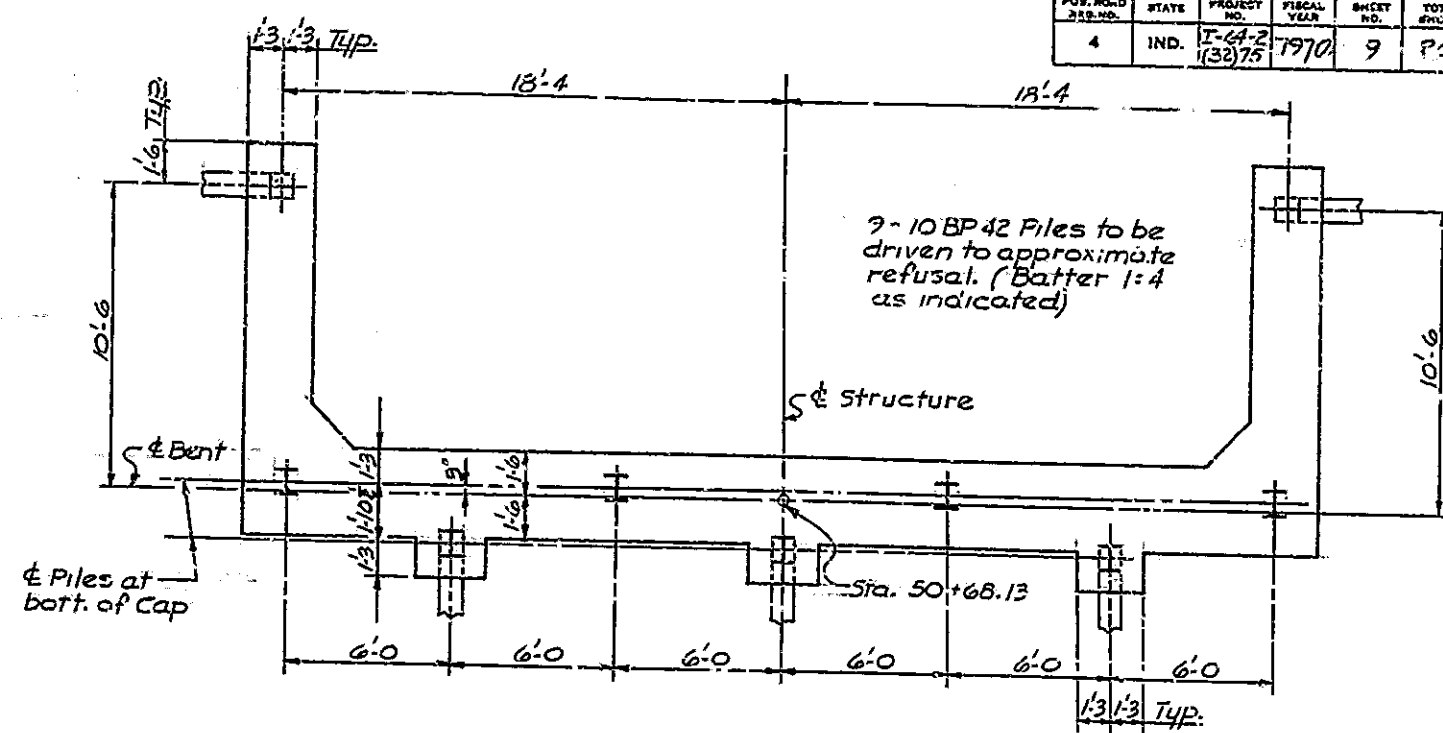
TOP OF MUDWALL ELEVATIONS								
Location	E1	E2	E3	E4	E5	E6	E7	E8
Elevation	687.990	687.960	688.070	688.040	688.160	688.130	688.255	688.225

Hatched areas indicate extent of horizontal constr. joints at top of Cap.

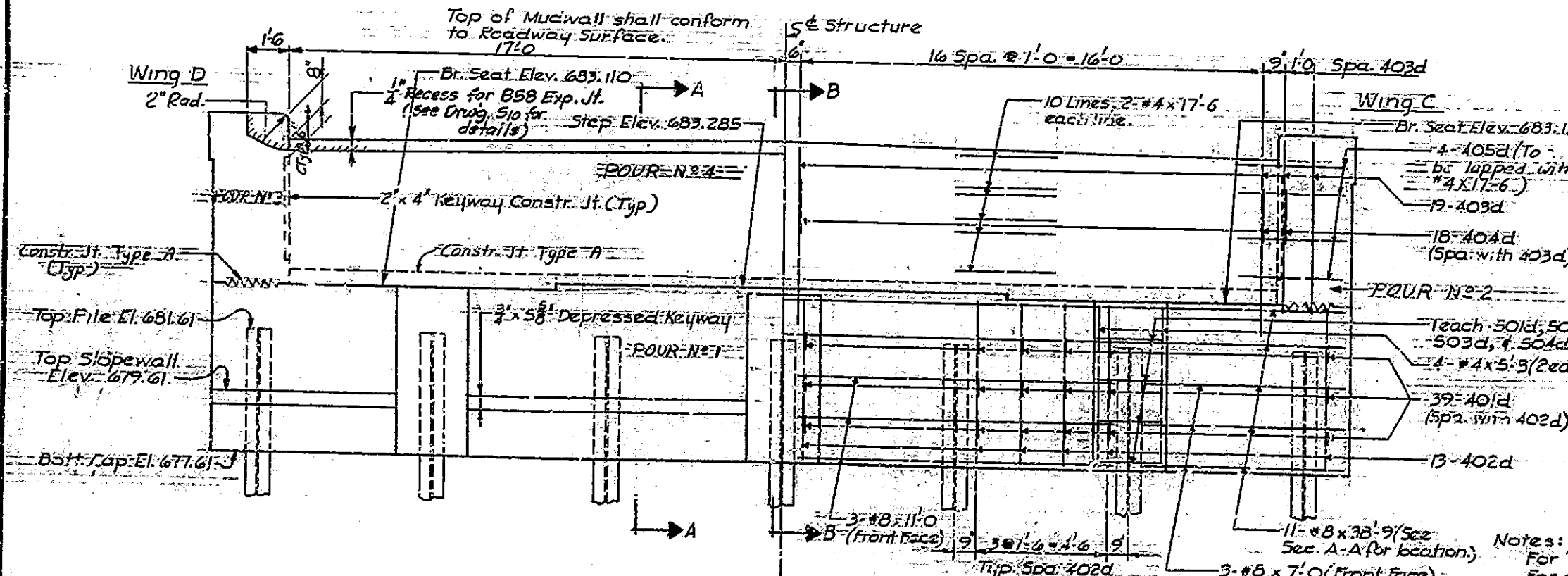
BRIDGES OVER 20' SPAN					
PUR. NO. & DIST.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-64-2 (33)75	1970	9	9



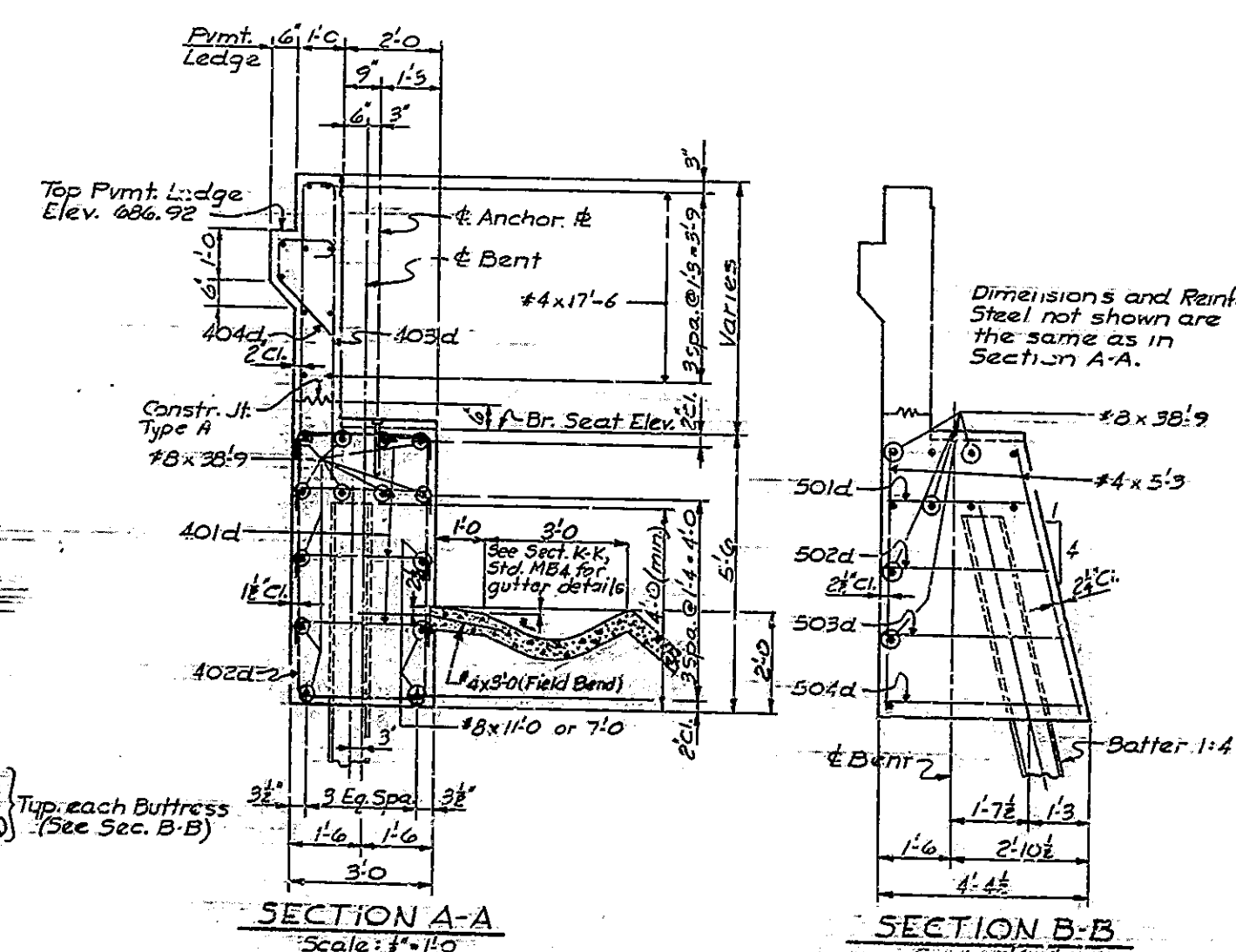
PLAN
Scale: 3/8" = 1'-0"



PILING PLAN
Scale: 1/2" = 1'-0"



NORTH ELEVATION
Scale: 3/8" = 1'-0"



SECTION A-A
Scale: 3/8" = 1'-0"

SECTION B-B
Scale: 3/8" = 1'-0"

BENT NO. 3 DETAILS

INDIANA STATE HIGHWAY COMMISSION

Notes:
For Reinforcing Bar Notes, see Br. Std. C,
For additional details and Bill of Materials,
see Drwg. S7.
For Anchor Plate MKAPS Details, see Drwg. S4.
Anchor Plates are to be preset in the
concrete.
Pour No. 2 to be poured after Floor Slab
has been poured.
For Constr. Jt. Type A, See Br. Std. C3.

SCALE: As noted
SUBMITTED FOR APPROVAL: *James D. Matthe* July 16, 1967
DRAWING: 5/6 OF 11
PROJECT: I-64-2 (33) 75
BRIDGE CONTRACT NO. B-9514
BRIDGE FILE: I-64-75-56745



REV. 7-16-73 JCK, EJC
REV. 7-16-73 JCK, EJC

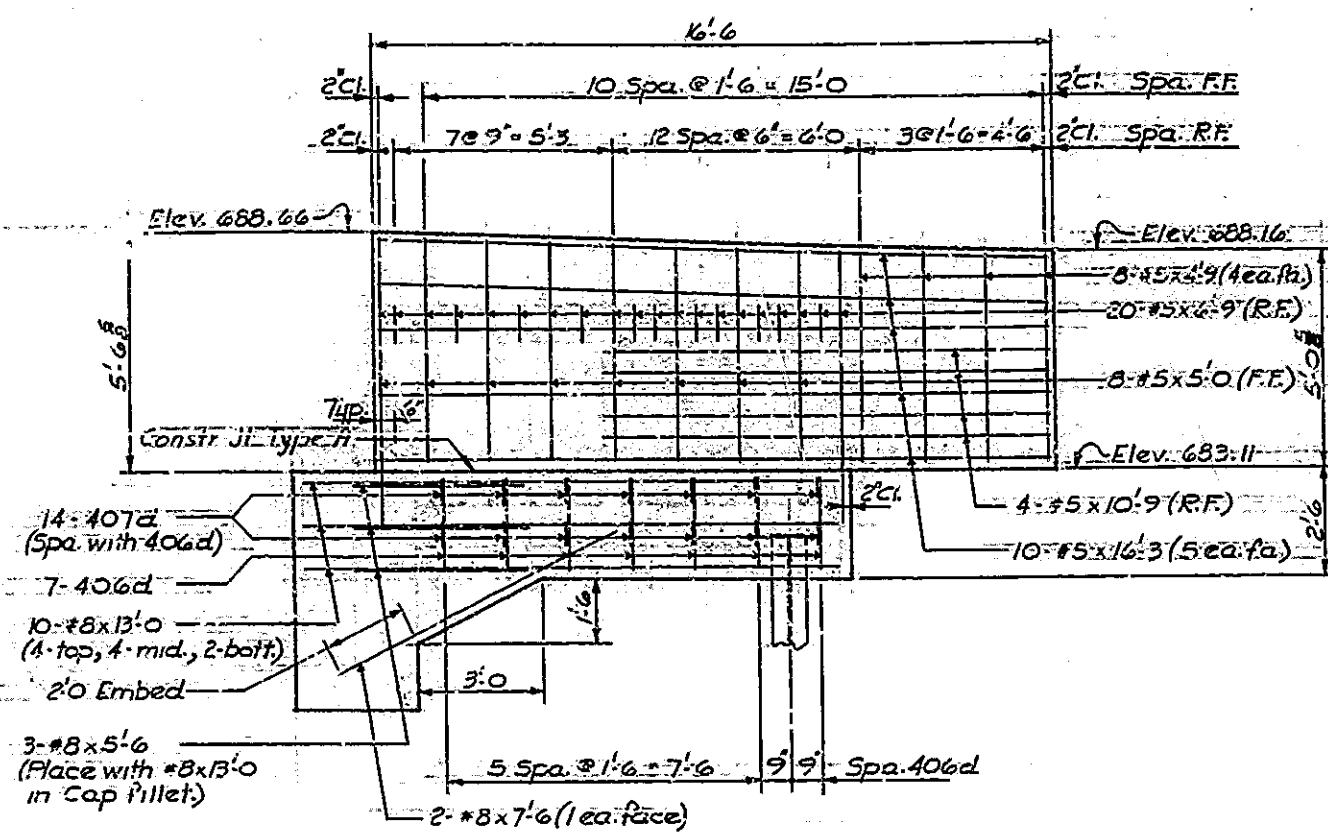
DESIGNED: 55-130-47
DRAWN: 55-130-47
CHECKED: 55-130-47

Rev. 9-7-73 Mudwall elev.
Rev. 7-16-73 Constr. Jt. Reinf. Bar Pour Notes

BRIDGE OVER 20' SPAN					
PUB. ROAD DIST. NO.	STATE	PROJECT NO.	FISCAL YEAR	SHEET NO.	TOTAL SHEETS
4	IND.	I-64-2 (32)75	1970	10	26

BILL OF MATERIALS

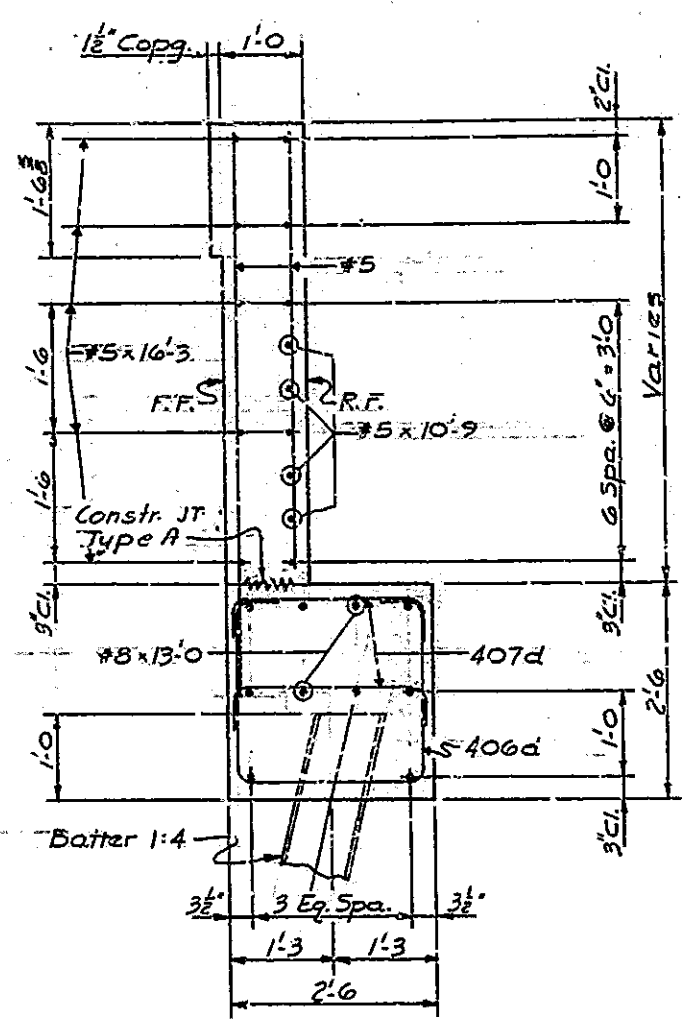
REINFORCING STEEL			
Size & Mark	No of Bars	Length	Weight (Lbs)
#8	11	38'-9"	
#8	20	13'-0"	
#8	6	11'-0"	
#8	4	7'-6"	
#8	6	7'-0"	
#8	6	5'-6"	
Total #8			2289
501d	3	8'-5"	
502d	3	8'-11"	
503d	3	9'-7"	
504d	3	10'-3"	
#5	20	16'-3"	
#5	8	10'-9"	
#5	40	6'-9"	
#5	16	5'-0"	
#5	16	4'-9"	
Total #5			989
401d	116	3'-9"	
402d	26	14'-3"	
403d	38	13'-0"	
404d	36	4'-1"	
405d	8	8'-6"	
406d	14	7'-5"	
407d	28	3'-3"	
#4	20	17'-6"	
#4	12	5'-3"	
#4	34	3'-0"	
Total #4			1486
TOTAL STEEL			4764
CONCRETE (Cys)			
Class "A" Conc.			
Pour No 1			31.3
Pour No 2			3.8
Pour No 3			3.8
Pour No 4			6.4
Total Class "A" in Substr.			45.3
MISCELLANEOUS			
Anchor Plates "KAP2"			7ea.
9-10 BP42 Piles @ 33'-0 approx.			2704#



WING C ELEVATION

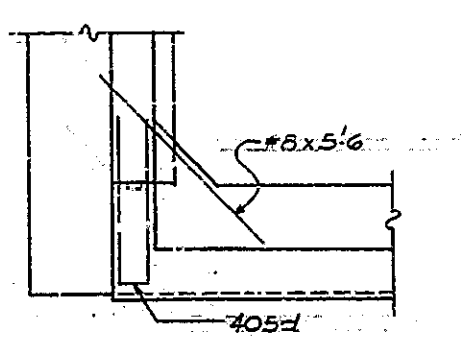
WING D ELEVATION SAME BY OPPOSITE HAND

Scale: 3/8" = 1'-0"



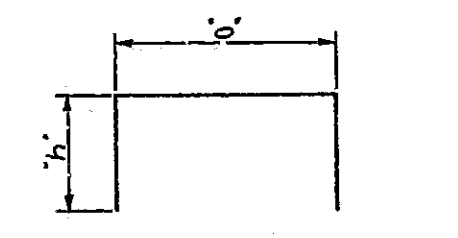
SECTION C-C

Scale: 1/2" = 1'-0"

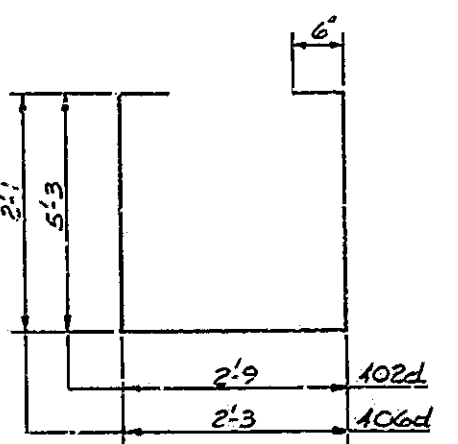


TYPICAL CORNER DETAIL

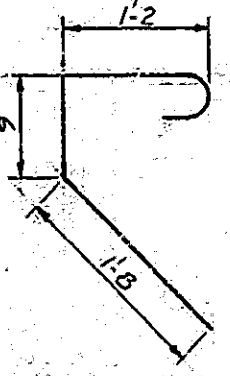
Scale: 3/8" = 1'-0"



MARK	Length	Weight
401d	2'-9"	31.3
402d	14'-3"	148.6
403d	13'-0"	138.9
404d	4'-1"	14.1
405d	8'-6"	8.6
406d	7'-5"	7.5
407d	3'-3"	3.3
#4	17'-6"	17.6
#4	5'-3"	5.3
#4	3'-0"	3.0



402d x 14'-3
406d x 7'-5



404d x 4'-1

Notes:
For Reinforcing Bar Notes, see Br. Std. C.
For additional details, see Drwg. S6.
For Constr. Jt. Type A See Br. Std. C.

BENT NO. 3 DETAILS & BILL OF MATERIALS

INDIANA STATE HIGHWAY COMMISSION

SCALE: As noted
SUBMITTED FOR APPROVAL: *James D. Miller* JULY 16, 1969
DRAWING: 57 OF 11
PROJECT: I-64-2(32)75
BRIDGE CONTRACT NO. B-9514
BRIDGE FILE: I-64-75-56745

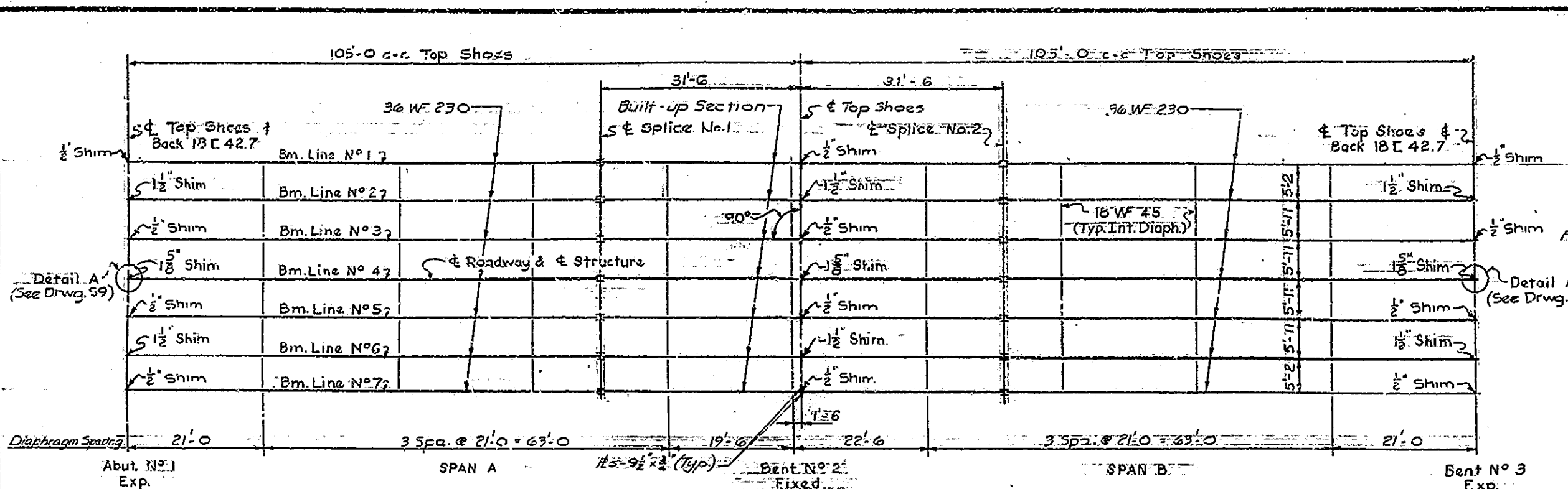


Rev. 9-7-73 JCL, EJC
Rev. 7-16-73 JCC, WBP, CHA, CAN

DESIGNED: JCS:BCG
DRAWN: JCC:WBP
CHECKED: JCS:BCG
TRACED: CND

Rev. 9-7-73 Reinf. Bar, Bill of Materials.
Rev. 7-16-73 Constr. Jt. Reinf. Bars, Bill of Materials, Notes

BRIDGE OVER 20' SPAN					
PUR. ROAD	STATE	PROJECT	LEGAL	SHEET	TOTAL
NO.				NO.	SHEETS
4	IND.	I-64-2	1970	11	26



NOTE: All Longitudinal Dimensions are parallel to the profile grade.
All Field Splices are required.

FRAMING PLAN
Scale: $\frac{3}{32} = 1'-0"$

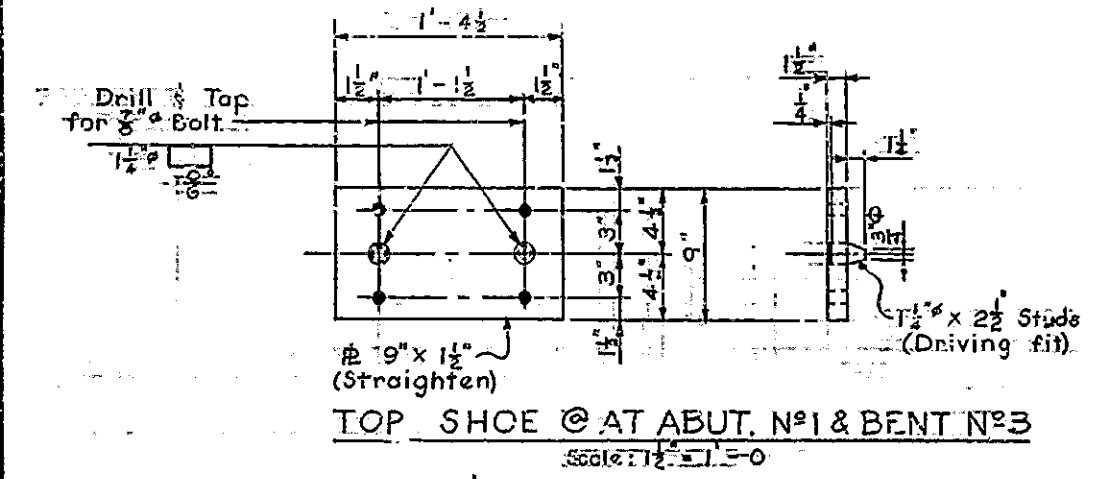
FABRICATION NOTES (Continued):

All shop butt welds in flange plates shall be ground smooth and flush with the base metal on all surfaces. This shall apply to both parts of equal thickness and unequal thickness. Grinding shall be done in the direction of stress and in such a manner that the metal is kept below the blue brittle range. Any defects exposed by the grinding shall be cleaned, filled with weld metal, and reground to a uniform finish. Sheared plates or universal mill plates shall be used for girder webs, and shall be ordered with sufficient additional width to allow for trimming of edges to provide built-in camber for deadload deflection and vertical curve. Trimming shall be by flame cutting. The facing surfaces of the web and flange plates and the adjacent surfaces that are to be milled or welded shall be cleaned by grinding prior to assembly and welding of web to flange.

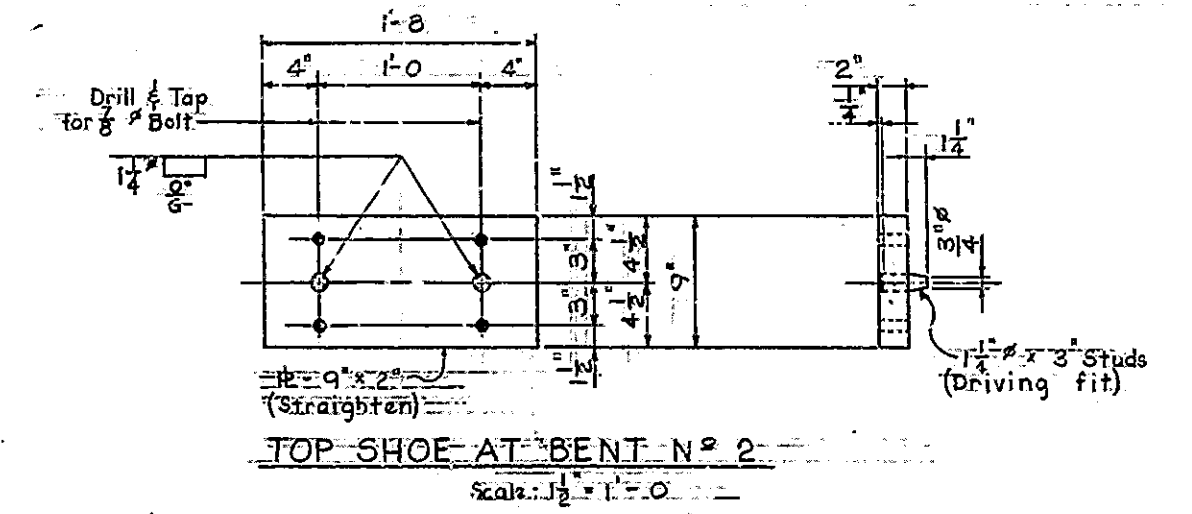
DATA USED FOR DESIGN AND DETAILS:

LIVE LOADS: HS-20-AA loading with impact and distribution of loads in accordance with the 1969 AASHTO Specifications.
DEAD LOADS: Actual weight plus 35 pounds per square foot of roadway to provide for future wearing surface.
FLOOR SLABS: Designed for a 16,000 pound wheel plus impact, and with one inch monolithic wearing surface.
ALLOWABLE STRESSES: To be in accordance with 1969 AASHTO Specifications.
FABRICATION NOTES:
Materials as listed on the shop drawings which do not require mill test reports may be changed from that shown on the contract plans subject to approval. The material specification shall be given on the shop drawings if different than that on contract plans. See Art. 711.07 of the Specifications.
Structural steel for welding may be flame cut if the flame cutting is mechanically guided. Hand flame cutting shall be used only when approved, and the surface is further treated by milling, grinding, or chipping and grinding.

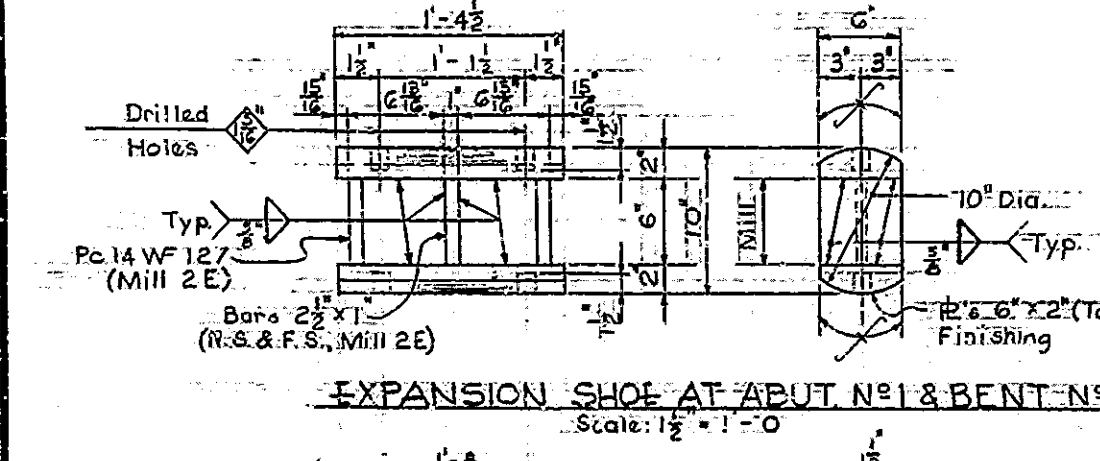
High strength bolts are to be used unless noted. Rivets shall not be used in the assembly of structural steel.
Open holes are to be used unless noted.
All paint shall be in accordance with current State Highway Specifications.
Shop Paint: Basic Lead Silico Chromate.
Field Paint:
Beams must be cambered to a smooth curve. Camber must be checked after shop welding is completed and while the beams are supported in such a way as to have no bending moment in the direction of camber.
Holes for beam splices and splice plates shall be subpunched or subdrilled and reamed to size while assembled. See Art. 711.26 of the Specifications.
The Contractor shall prepare detailed working or shop drawings to enable him to fabricate, erect, and construct all parts of the work in conformity with the Engineer's drawings and the Specifications and shall submit five (5) copies of these to the Engineer. See Art. 711.04 of the Specifications.
The shop details shall show a plan of matchmarking for all reamed pieces.
All splice plates are to be removed, cleaned, and dechurned after reaming. Splice plates shall not extend beyond the end of the beam after bolting for shipment.
The Shop Plans shall indicate whether reaming or drilling is to be done in shop or field. If shop reaming or drilling is used the beams shall be assembled in accordance with the "No Load Camber and Reaming Diagram".



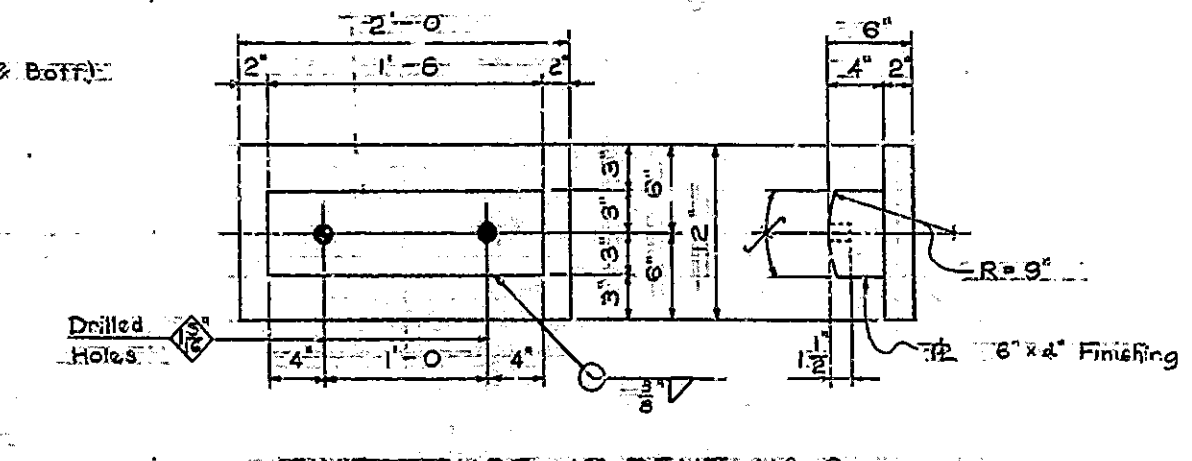
TOP SHOE @ AT ABUT. N°1 & BENT N°3
Scale: $\frac{1}{2} = 1'-0"$



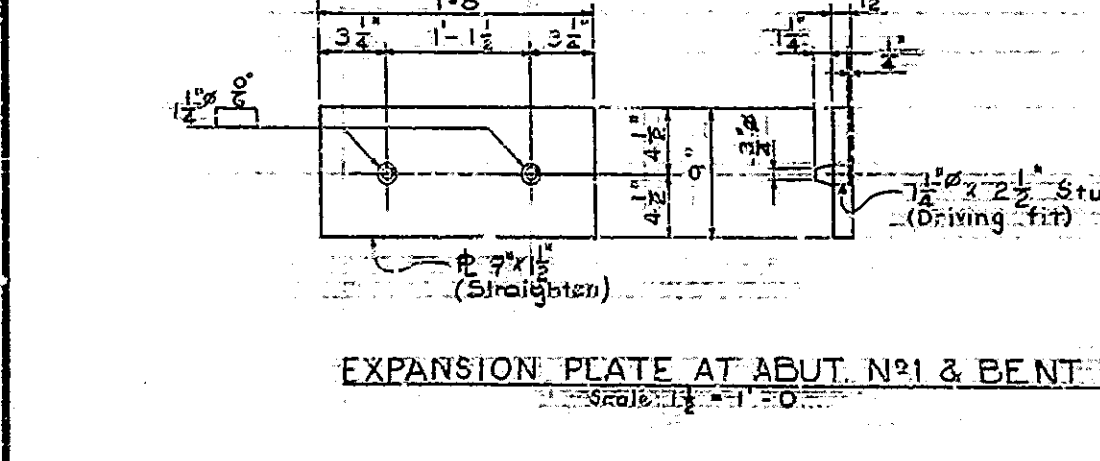
TOP SHOE AT BENT N°2
Scale: $\frac{1}{2} = 1'-0"$



EXPANSION SHOE AT ABUT. N°1 & BENT N°3
Scale: $\frac{1}{2} = 1'-0"$



FIXED SHOE AT BENT N°2
Scale: $\frac{1}{2} = 1'-0"$



EXPANSION PLATE AT ABUT. N°1 & BENT N°3
Scale: $\frac{1}{2} = 1'-0"$

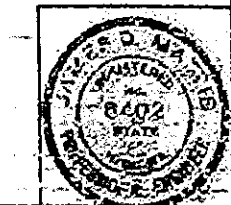
Note: Curved surfaces of shoes shall be machined after weldments have been made.

Flange splice bars shall have planed or rolled edges and holes in bars shall be subdrilled and reamed or drilled full size while assembled.
Holes in all material connecting top shoes to the beam flanges shall be 1/8 inch in diameter. Bolts connecting the top shoes to the beam flanges shall extend into the top shoe a minimum of 1 inch.
1/2" studs or channel shear connectors may be used in lieu of a welded stud shear connectors. If alternate shear connectors are used, the Contractor shall prepare design computations and structural steel details and submit five (5) copies of these to the Engineer.
As soon as the Engineer has approved the field welds, all welds and any surface from which the shop paint has been omitted or has become worn off or has otherwise become defective shall be thoroughly cleaned of all charred paint or any other foreign matter and shall be completely covered with one coat of shop paint.
Shims between beams and top shoes may be built up. No shim shall be less than 1/8 inch in thickness.
Diaphragm to beam connections may be bolted in lieu of being field welded. If the Contractor elects to use connections other than those shown in the Contract Plans, he shall submit details to the Engineer for approval. He shall assume full responsibility for the layout of all diaphragm connections and for the accuracy of all fitted parts. No increase in pay weight will be permitted.
Structural steel shall be erected using sufficient full size drift pins to permit placement of bolts without damage thereto and to facilitate setting splices to grade. At the time of erection not less than 50 percent of the holes in any connection shall be filled with bolts. The bolts shall not be tightened more than snug tight at this stage. Any drifting required shall be only such that will draw the parts into position but not sufficient to enlarge the holes or distort the metal. Unfair holes shall be reamed or drilled.
The weight of High Strength Bolts is not included in the estimated weight of structural steel. The cost of these bolts shall be included in the cost of the structural steel.
All structural steel shall conform to ASTM-A36.
Weight of structural steel (contract) = 23,500 lbs.

FRAMING PLAN AND STRUCTURAL STEEL DETAILS

INDIANA STATE HIGHWAY COMMISSION

SCALE: AS NOTED
SUBMITTED FOR APPROVAL: James DeWalt
DRAWING: S8011
PROJECT: I-64-2 (32)75
BRIDGE CONTRACT NO. B-951A
BRIDGE FILE: I-64-25-5674.5

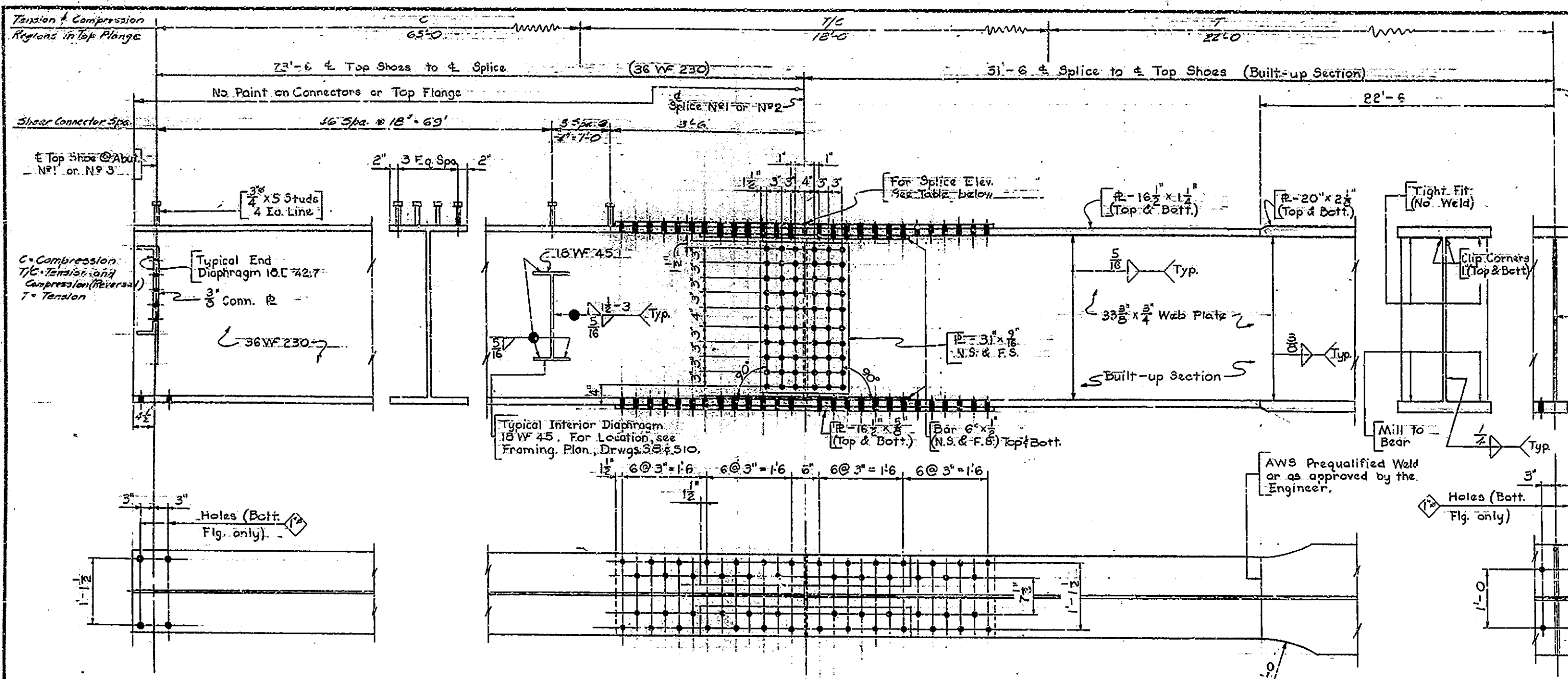


DESIGNED: JSS
DRAWN: LEP
CHECKED: CWS

Rev. 7-16-73 Weld Size, Design Data, Fabrication Notes, Framing Plan

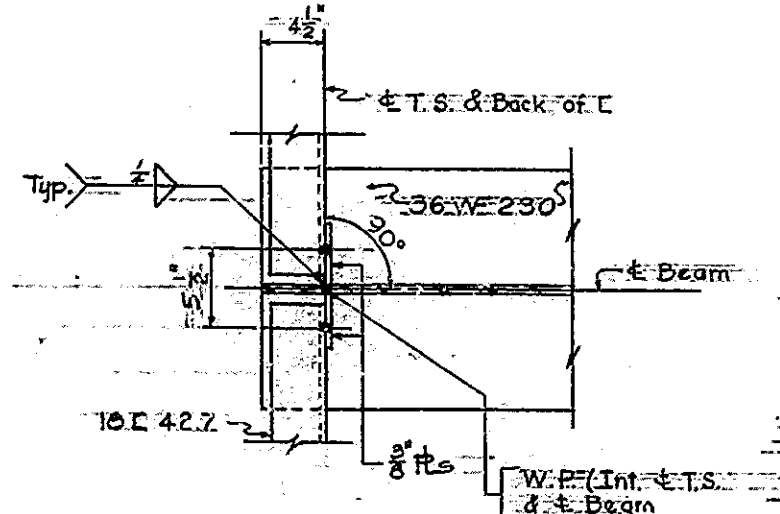
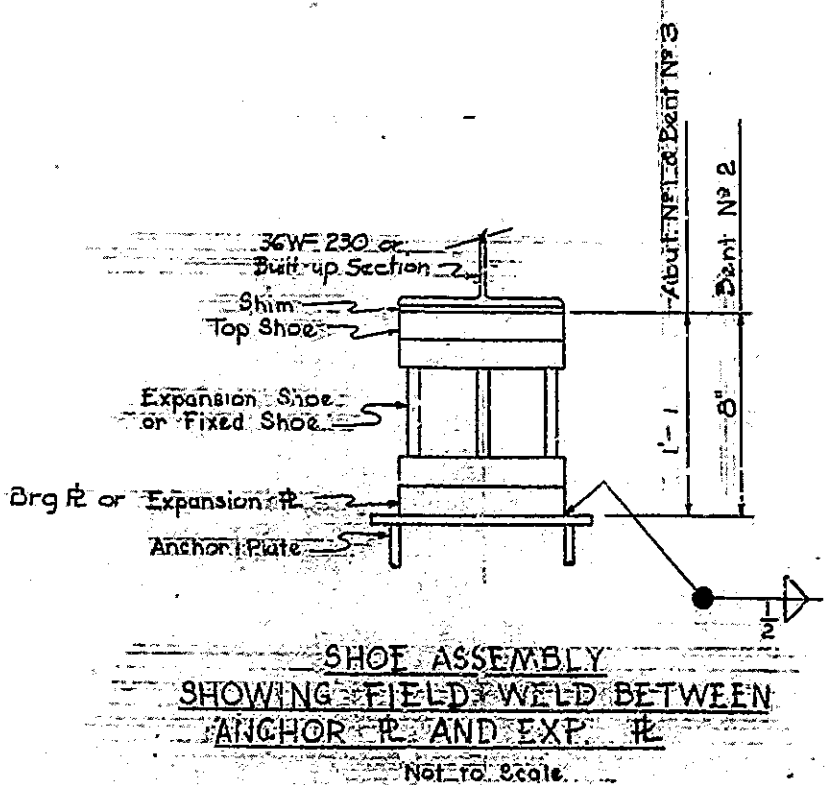
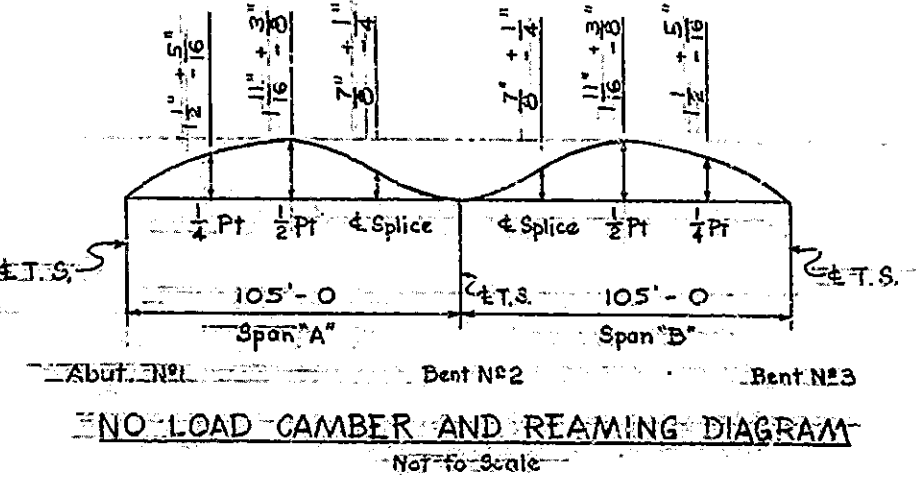
PROJECT NO.	LINE	SHEET NO.	TOTAL SHEETS	FILE

BRIDGES OVER 20' SPAN				
PUB. ROAD	STATE	PROJECT NO.	FISCAL YEAR	TOTAL SHEETS
4	IND.	1-64-2 (2) 75	1970	26



Symmetrical about
Top Shoe Bent N#2

BEAM AND SPLICE DETAILS
Scale: 1" = 1'-0"



Splice elevations are with falsework removed and carrying steel deadload only. Top of beam splice plates shall be adjusted to the below elevations before bolting field splices.

TABLE OF SPLICE ELEVATIONS

Beam No.	Splice N#1	Splice N#2
1	691.425	689.535
2	691.505	689.615
3	691.595	689.705
4	691.690	689.800
5	691.595	689.705
6	691.505	689.615
7	691.425	689.535

Notes: All butt welds shall be subject to radiographic inspection at the option of the Engineer.

For additional Structural Steel Details and Fabrication Notes, see Drwg. SB.

STRUCTURAL STEEL DETAILS

INDIANA STATE HIGHWAY COMMISSION

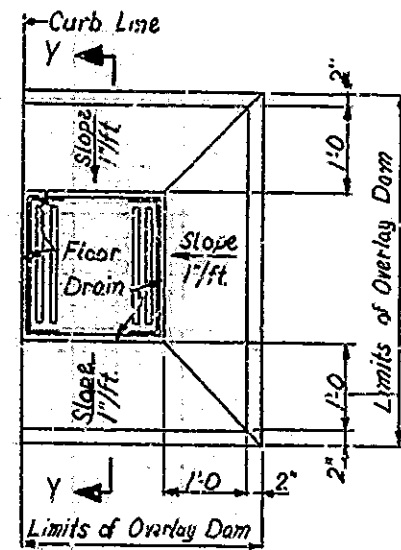
SCALE: AS NOTED
 SUBMITTED FOR APPROVAL: *James D. Patton* July 16, 1969
 DRAWING: 5 OF 11
 PROJECT: I-64-2 (2) 75
 BRIDGE CONTRACT NO. B-9514
 BRIDGE FILE: I-64-75-5674 B



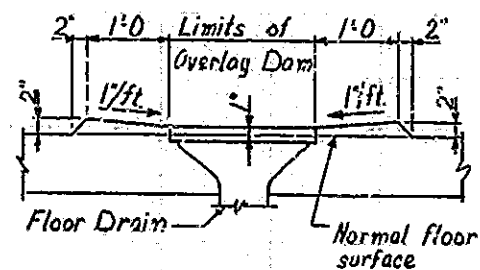
DESIGNED: SS 1-6-62 CVD MFS 1-2-69
 DRAWN: MFP 3-27-69 CVD L.F. 4-11-69
 TRACED: CVD

1-64-75-5674-3 Shear Connector Notes, Tension & Compression Regions in Top Flange added

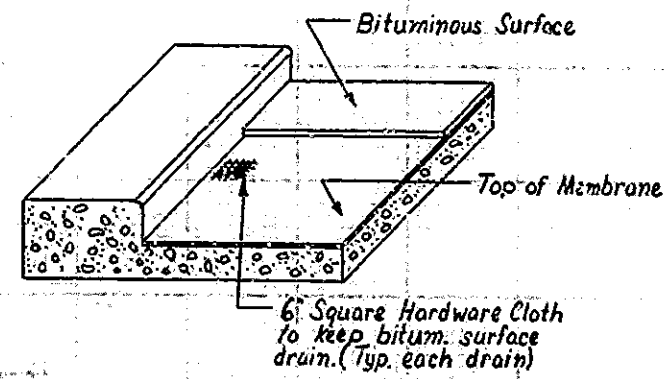
PROJECT NO.	DATE	BY	CHKD.	APP.



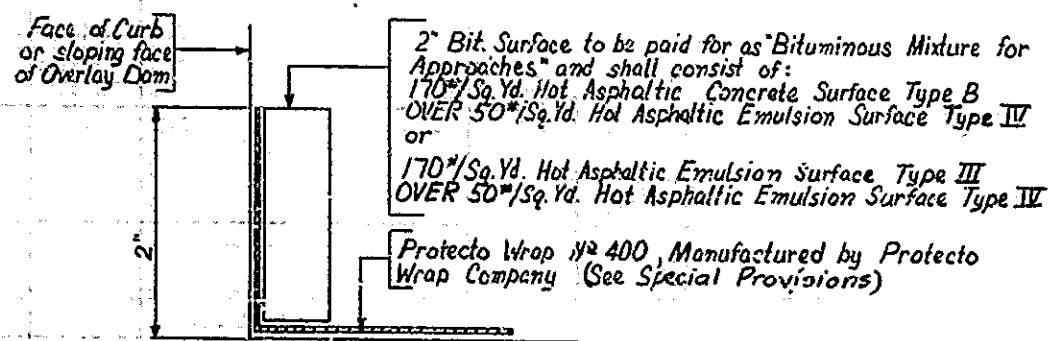
OVERLAY DAM AT FLOOR DRAINS
PLAN



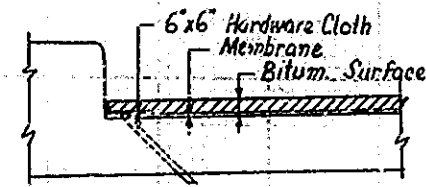
SECTION Y-Y



ASSEMBLY SKETCH OF DECK DRAIN



OVERLAY DETAIL



DECK DRAIN DETAIL

DESIGNED BY CTD
 DRAWN BY CTD
 TRACKED DAM 9-7-73

INDIANA STATE HIGHWAY COMMISSION

SCALE: NO SCALE

DATE: 9-7-73

Robert W. Setzler

DRAWING: 50 OF 11 SHEET: OF
 PROJECT: I-64-2 (S) 75
 CONTRACT NO. B-3374
 BRIDGE FILE: I-64-75-5674S



