

Indiana Department of Transportation

County St. Joseph Route SR 933 Des. No. 1173149 Project No. _____

FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
 GENERAL PROJECT INFORMATION

Road No./County:	SR 933/St. Joseph County
Designation Number:	1173149
Project Description/Termini:	Bridge Rehabilitation and Repair on SR 933 over the St. Joseph river.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager).
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services).
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA.
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA.

Note: For documents prepared by or for Environmental Services, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval [Signature] 4-10-12
 ESM Signature Date ES Signature Date

 FHWA Signature Date

Release for Public Involvement [Signature] 2-22-12
 ESM Initials Date

-RC- 4-6-12
 ES Initials Date
 Certification of Public Involvement [Signature] 4-6-12
 Manager, Public Hearings Signature Date

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

Reviewer Signature _____ Date _____

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Name and organization of CE/EA Preparer: Travis Mast, INDOT- La Porte District Environmental Section

Part I - PUBLIC INVOLVEMENT

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

Discuss what public involvement activities (legal notices, letters to affected property owners and residents, meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Remarks:

The Historic Bridge Programmatic Agreement requires that a public hearing be held for all projects involving a Select or Non-Select Bridge. Select bridges are defined in the Historic Bridge Programmatic Agreement as excellent examples of a given type of historic bridge.

In accordance with the INDOT (FHWA approved) Historic Bridge Programmatic Agreement, a public hearing was offered on March 21, 2012 at the South Bend Main Branch Library in the Dickenson Conference room. The Level 2 Categorical Exclusion document was available for public review for a minimum of 30 days ending on April 6, 2012. Copies of the Categorical Exclusion document were available for public review at the South Bend library located at 304 South Main St., South Bend, Indiana, the La Porte District located at 315 E Boyd Blvd, La Porte, Indiana and at the Hearings Examiner Office, Room N642, Indiana Government Center North, 100 North Senate Avenue, Indianapolis, Indiana.

The public notice requirements regarding INDOT’s section 106 “No Adverse Effect” finding on behalf of the FHWA was fulfilled through this 30-day comment period. No specific comments were received regarding the “No Adverse Effect” finding. See Appendix H.

The public hearing notification was advertised in a local area newspaper twice. The Legal Notice for the Public Hearing was published in the *South Bend Tribune* March 6 and 13, 2012. Comments were accepted for 30 days through April 6, 2012. Each identified property owner along the project corridor, as well as elected officials from the project vicinity, and section 106 consulting parties were sent a copy of the public notice.

Comments and questions were received during and after the public hearing. Comments were received via email, mail, and during the public hearing. These comments consisted of general maintenance, weight limit restrictions, maintenance of traffic, and truck volume along this segment of SR 933. See Appendix H for the publisher’s affidavit, public hearing presentation, along with comments and questions.

Public Controversy on Environmental Grounds

Will the project involve substantial controversy concerning community and/or natural resource impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks:

No controversy concerning community and/or natural resource impacts are expected at this time.

Opportunity for a Public Hearing Required

Yes

No

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Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Transportation INDOT District: LaPorte
Local Name of the Facility: SR 933 (N. Michigan St.)

Funding Source: Federal State Local Private

PURPOSE AND NEED:

Describe the problem that the project will address.

The current bridge is in need of repair based on several deficiencies identified throughout the structure. Specific deficiencies include cracking of the arch ring and deterioration of the bridge railings and post stones. Based on a bridge inspection on June 22, 2011, cracking was observed in the railings throughout the structure, along with transverse cracking on the underside of the arch ring. As a result of this inspection report, a weight limit of 10 tons was imposed on the bridge. The purpose of the project is to correct the above deficiencies, remove the weight restriction and ensure a safe transportation route at this location.

PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: St. Joseph
Municipality: South Bend

Limits of Proposed Work: 100 feet north and south of the structure on SR 933.
Total Work Length / Area: 0.001 mi Mile(s) / Acre(s)

Is an Interchange Modification Study / Interchange Justification Study (IMS/IJS) required?

Yes ¹	No
	X

If yes, when did the FHWA grant a conditional approval for this project?

Date:

¹If an IMS or IJS is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IMS/IJS.

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In the Remarks box below, describe in detail the scope of work for the project, including the preferred alternative. Include a discussion of logical termini. Discuss any major issues for the project and how the project will improve safety or roadway deficiencies if these are issues.

This project is located within the limits of the City of South Bend on SR 933 over the St. Joseph river, 1.59 miles north of SR 23 in Section 2, Township 37 North, Range 2 East on the U.S.G.S. South Bend West Quadrangle all within Portage Township, St. Joseph County, Indiana. The project is located within the Indiana Department of Transportation's (INDOT's) La Porte District and lies within the Michiana Area Council of Governments (MACOG) Metropolitan Planning Organization (MPO).

This structure, a three span spandrel arch bridge, number 933-31-71-03690-B, was constructed in 1914, repaired in 1977 and 2006. The existing SR 933 bridge over the St. Joseph River built with a 0° skew with clear bridge width at 55 ft and total bridge width at 72 ft. The bridge has four 10 ft drive lanes. This section of SR 933 is functionally classified as an "Urban-Principal Arterial" route and is not on the National Highway System.

The SR 933/Michigan Street/Leeper Bridge over the St. Joseph River (Bridge No. (933)31-71-3690-B/NBI No. 11046) is an "Outstanding" resource in the Leeper Park Historic District (LPHD), which was listed in the National Register of Historic Places (NR) in May, 2000. The bridge is therefore individually NR-eligible. The bridge was classified as "Select" by the 2010 Indiana Department of Transportation (INDOT) Historic Bridge Inventory. "Select" bridges are defined in the Historic Bridge PA as those structures that are excellent examples of a specific bridge type and that are most suitable for preservation. The SR 933/Michigan Street/Leeper Bridge also possesses Local Landmark status as conferred by the City of South Bend.

The preferred alternative for this project will include the removal and in-kind replacement of the existing bridge railing panels in both form and material between Abutment 1 and Abutment 4, the performance of minor repairs to the railing posts with materials to match original; and major repairs to the interior arch ring and restoration of any exterior surfaces. All work will be restricted to the existing bridge structure; no new right-of-way will be acquired.

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OTHER ALTERNATIVES CONSIDERED:

Describe all discarded alternatives, including the Do-Nothing Alternative and an explanation of why each discarded alternative was not selected.

Alternative No 1: The Do-Nothing alternative would result in any environmental impacts nor require additional funding. However, continued deterioration of the structure and eventual failure of this transportation element would inevitably occur. The Do-Nothing alternative was not selected as it does not address the purpose and need for this project.

Alternative No 2: The Bridge replacement alternative was considered yet not selected. Based on bridge inspection reports it was determined that this was not necessary and would result in more project cost than the preferred alternative. Also, given the Select bridge status, rehabilitation is required based on the Historic Bridge Programmatic Agreement.

Alternative No 3 (Preferred Alternative): The repair and rehabilitation alternative was selected as it meets the purpose and need for the project.

The Do Nothing Alternative is not feasible, prudent or practicable because (Mark all that apply):

It would not correct existing capacity deficiencies;

It would not correct existing safety hazards;

It would not correct the existing roadway geometric deficiencies;

It would not correct existing deteriorated conditions and maintenance problems, or

It would result in serious impacts to the motoring public and general welfare of the economy.

Other (Describe)

X
X

ROADWAY CHARACTER:

Functional Classification: Urban Principal Arterial
 Current ADT: VPD 2010 (7510) Design Year ADT: VPD 2030 (8920)
 Current Year DHV 600 Trucks 12% Design Year DHV 715 Trucks 12%
 Designed Speed (mph): 35 Legal Speed (mph): 35

Existing

Proposed

Number of Lanes:	<u>4</u>	<u>4</u>	
Type of Lanes:	<u>HMA</u>	<u>HMA</u>	
Pavement Width:	<u>55</u> ft.	<u>55</u> ft.	
Shoulder Width:	<u>N/A</u> ft.	<u>N/A</u> ft.	
Median Width:	<u>N/A</u> ft.	<u>N/A</u> ft.	
Sidewalk Width:	<u>2</u> ft.	<u>2</u> ft.	

Setting: Urban Suburban Rural
 Topography: Level Rolling Hilly

If the proposed action has multiple roadways, this section should be filled out for each roadway.

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DESIGN CRITERIA FOR BRIDGES:

Structure Number(s): 31-71-03690-B Sufficiency Rating: 79.6

	Existing	Proposed
Bridge Type:	<u>Reinforced Concrete Arch</u>	<u>Reinforced Concrete Arch</u>
Number of Spans:	<u>3</u>	<u>3</u>
Weight Restrictions:	<u>10</u> ton	<u>No</u> ton
Height Restrictions:	<u>No</u> ft.	<u>No</u> ft.
Curb to Curb Width:	<u>55</u> ft.	<u>55</u> ft.
Outside to Outside Width:	<u>72</u> ft.	<u>72</u> ft.
Shoulder Width:	<u>2</u> ft.	<u>2</u> ft.
Length of Channel Work:	<u>N/A</u> ft.	<u>N/A</u> ft.

Describe bridges and structures; provide specific location information for small structures.

Remarks: This project involves repairing bridge number (933)31-71-03690-B over the St. Joseph River on SR 933. This bridge has a current sufficiency rating of 79.6. This bridge was originally built in 1914, repaired in 1977 and 2006. Currently a weight restriction of 10 tons is in place. This bridge will have an unrestricted weight rating after construction.

Yes
 No
 N/A

Will the structure be rehabilitated or replaced as part of the project?
If the proposed action has multiple bridges or small structures, this section should be filled out for each structure.

MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe in remarks)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks: Access to all adjacent properties will be provided at all times throughout the duration of the project, in accordance with INDOT specifications. Northshore Blvd, will be closed at the intersection of SR 933 and Northshore Blvd. Phased construction will close one lane of the bridge on SR 933 at a time.

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ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 450,000 (2011) Right-of-Way: \$ 0 Construction: \$ 1,200,000 (2011)
 Anticipated Start Date of Construction: 2012
 Date project incorporated into STIP March 3, 2011
 If in an MPO area, location of project in TIP MACOG which was incorporated by reference into the STIP on March 3, 2011.

RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0.0 0.0	
Commercial	0.0 0.0	
Agricultural	0.0 0.0	
Forest	0.0 0.0	
Wetlands	0.0 0.0	
Other:	0.0 0.0	
Other:	0.0 0.0	
Other:	0.0 0.0	
TOTAL	0.0 0.0	

Remarks: This project will not require the acquisition of new permanent or temporary right-of-way.

Part III – Identification and Evaluation of Impacts of the Proposed Action

SECTION A – ECOLOGICAL RESOURCES

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Streams, Rivers, Watercourses & Jurisdictional Ditches	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
State Wild, Scenic or Recreational River	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

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Remarks:

The St. Joseph river flows under the structure at this location. This was confirmed during a site visit on October 24, 2011 performed by Travis Mast – INDOT Environmental Manager. This waterway will not be impacted as a result of this project. All work will take place above the OHWM. No other streams, rivers, watercourses, or jurisdictional ditches were identified within the proposed project area. (See Appendix F)

	<u>Presence</u>		<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	<u>Yes</u>	<u>No</u>
Other Surface Waters				
Reservoirs	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detention Basins	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

No other surface waters were identified within the proposed project area. This was confirmed during a site visit on October 24, 2011 performed by Travis Mast – INDOT Environmental Manager. (See Appendix F)

Wetlands	<u>Presence</u>		<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	<u>Yes</u>	<u>No</u>
	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0.0 acre(s) Total wetland area impacted: _____ acre(s)
 (If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments

Wetlands	<u>Documentation</u>		<u>ES Approval Dates</u>
	<u>Yes</u>	<u>No</u>	
Wetland Determination	<input checked="" type="checkbox"/>	<input type="checkbox"/>	November 4, 2011
Wetland Delineation Report	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
USACE Isolated Waters Determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Mitigation Plan	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

Individual Wetland

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Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in (Mark all that apply and explain):	Finding	
	Yes	No
Substantial adverse impacts to adjacent homes, business or other improved properties;	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Substantially increased project costs;	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Unique engineering, traffic, maintenance, or safety problems;	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Substantial adverse social, economic, or environmental impacts, or	<input type="checkbox"/>	<input checked="" type="checkbox"/>
The project not meeting the identified needs.	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Measures to avoid, minimize and mitigate wetland impacts need to be discussed in the remarks section

Remarks: The site was inspected on October 24, 2011 by Travis Mast INDOT – Environmental Manager for the presence or absence of wetlands. No suitable habitat or wetland criteria were present within or adjacent to the proposed project area. (See Appendix F)

	Presence		Impacts	
	Yes	No	Yes	No
Terrestrial Habitat	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Use the remarks table to identify each type of habitat and the acres impacted (i.e. forested, grassland, farmland, lawn, etc).

Remarks: Land use within the vicinity of SR 933 is primarily residential. There is a public park located to the south on both the east and west quadrants. The northwest quadrants consist of residential houses. This project will not require the disturbance of any terrestrial habitat. All work will take place from the bridge deck.

If there are high incidences of animal movements observed in the project area, or if bridges and other areas appear to be the sole corridor for animal movement, consideration of utilizing wildlife crossings should be taken.

	Yes	No
Karst		
Is the proposed project located within or adjacent to the potential Karst Area of Indiana?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Are karst features located within or adjacent to the footprint of the proposed project?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
If yes, will the project impact any of these karst features?	<input type="checkbox"/>	<input type="checkbox"/>

Use the remarks table to identify any karst features within the project area. (Karst investigation must comply with the Karst MOU, dated October 13, 1993)

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Remarks:

The project is located outside of the designated karst area of the state as identified in the October 13, 1993 MOU. No karst features were observed or are known to exist within or adjacent to the proposed project area.

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Threatened or Endangered Species				
Within the known range of any federal species?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Any critical habitat identified within project area?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Federal species found in project area (based upon informal consultation)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State species found in project area (based upon consultation with IDNR)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Is Section 7 formal consultation required for this action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

The U.S. Fish and Wildlife Service (USFWS) was consulted during early coordination. The proposed project is within the range of the federally endangered Indiana bat (*Myotis sodalis*), Mitchell's satyr butterfly (*Neonympha mitchellii michellii*) and the candidate eastern massasauga rattlesnake (*Sistrurus catenatus catenatus*). However, the USFWS agrees that the proposed project is not likely to adversely affect these endangered and candidate species.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised species list be published, it will be necessary for the Federal agency to reinitiate consultation.

SECTION B – OTHER RESOURCES

	<u>Presence</u>		<u>Impacts</u>	
	Yes	No	Yes	No
Drinking Water Resources				
Sole Source Aquifer (SSA)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the Project in the St. Joseph Aquifer System?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the FHWA/EPA SSA MOU Applicable?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Initial Groundwater Assessment Required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Detailed Groundwater Assessment Required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Source Water Protection Area(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Public Water System(s)	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Residential Well(s)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Wellhead Protection Area	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

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Remarks:

The project is located within the legally designated St. Joseph Aquifer System. A Wellhead Protection Area is located in the project area per IDEM's Wellhead Protection Area response on December 12, 2011.

This project is located with the St. Joseph Sole Source Aquifer System (SSA). Although projects qualifying as Categorical Exclusion documents under 23 CFR 771.117 are specifically exempted from review by the USEPA as per the MOU unless a review is specifically requested, it is INDOT policy that all CEs, level 2 and above for projects that are located in the SSA area shall include the USEPA on the list of early coordination recipients. The Environmental Protection Agency was contacted and responded in a letter dated December 12, 2011 stating that the proposed project does not pose substantial threats to the St. Joseph Sole Source Aquifer System, a Sole Source Aquifer designated under the authority of the Safe Drinking Water Act, Section 1424(e). The letter also states that unless future developments change the status of the proposal, no modifications or further review under the Sole Source Aquifer Program should be necessary. (See Appendix C-14)

	<u>Presence</u>		<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	<u>Yes</u>	<u>No</u>
Flood Plains				
Longitudinal Encroachment	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse Encroachment	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is the project located in a FEMA designated floodplain?	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project.	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Discuss impacts according to classification system described in the "Procedural Manual for Preparing Environmental Studies".

Remarks:

The proposed project falls under Category 2 for floodplain analysis based on the Indiana Department of Transportation Categorical Exclusion manual. The proposed project will not involve the replacement or modification of any existing drainage structures or the addition of any new drainage structures. As a result, this project will not affect flood heights or floodplain limits. This project will not increase flood risks or damage, and it will not adversely affect existing emergency services or emergency routes, therefore, it has been determined that this encroachment is not substantial. (See Appendix E)

	<u>Presence</u>		<u>Impacts</u>	
	<u>Yes</u>	<u>No</u>	<u>Yes</u>	<u>No</u>
Farmland				
Agricultural Lands	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
NRCS Form AD-1006/CPA-106 scored \geq 160?	<input type="checkbox"/>	<input checked="" type="checkbox"/>		

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Provide the NRCS Form AD-1006/CPA-106 score and state whether there is a significant loss of farmland as a result of the project in the remarks section. See CE Manual for guidance to determine which NRCS form is appropriate for your project.

Remarks:

The proposed project does not require the acquisition of permanent or temporary right-of-way, therefore no conversion of agricultural land will occur. Based on early coordination with the Natural Resources Conservation Service the proposed project will not result in a conversion of prime farm ground. (See Appendix C-17)

SECTION C – CULTURAL RESOURCES

	Category	Type	INDOT Approval Dates
Minor Projects PA Clearance			

Eligible and/or Listed Resource Present

Results of Research

	Yes	No
Archaeology		X
History/Architecture		X
NRHP Buildings/Site(s)		X
NRHP District(s)		X
NRHP Bridge(s)	X	

Project Effect

	Yes	Not Applicable	SHPO/ES/FHWA Approval Dates
No Historic Properties Affected		X	
No Adverse Effect	X		SHPO 3/23/2012; ES/FHWA 2/21/2012
Adverse Effect		X	

Documentation Prepared

Documentation

	Yes	Not Applicable	SHPO/ES/FHWA Approval Dates
Historic Properties Short Report		X	
Historic Property Report	X		SHPO 1/20/2012; ES/FHWA 12/21/2011
Archaeological Records Check/ Review	X		SHPO 1/20/2012; ES/FHWA 12/21/2011
Archaeological Phase Ia Survey Report		X	
Archaeological Phase Ic Survey Report		X	
Archaeological Phase II Investigation Report		X	
Archaeological Phase III Data Recovery		X	
APE, Eligibility and Effect Determination	X		SHPO 1/20/2012; ES/FHWA 12/21/2011
800.11 Documentation	X		SHPO 3/23/2012; ES/FHWA 2/21/2012
Memorandum of Agreement		X	

Describe all efforts to document cultural resources, including a detailed summary of the Section 106 process, using the categories outlined in the remarks box. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of paper(s) and the comment period deadline. Likewise include any further Section 106 work which must be completed at a later date, such as mitigation or deep trenching.

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Remarks:

Area of Potential Effect (APE):

The project’s APE was established to take into account the physical and visual impacts of the current bridge. The APE has been determined as those areas of existing and proposed right-of-way and incidental construction, including adjacent properties, from which the new bridge might be visible. The APE is at least 0.3 mile in all directions of the bridge and varies depending on view shed. (See Appendix D)

Coordination with Consulting Parties:

On December 21, 2011 , the following parties were invited to be Section 106 consulting parties and to aid in identification of historic properties. The following parties, identified in bold, responded to the December 21 mailing with none expressing any objections about the project and with none providing comments about additional historic properties not already identified in the HPR.

Consulting Party	Response Received	Appendix Page
Historic Michigan Road Byway Committee	12/26/2011	D-74
Historic Pres. Commission of South Bend & St. Joseph Co.	12/29/2011	D-75
Historic Landmarks Foundation, Indiana Northern Region Office	1/20/2012	D-76
Indiana State Historic Preservation Officer	1/20/2012	D-77-79
South Bend Area Genealogical Society	1/23/2012	D-80
St. Joseph County Historian		
Michiana Area County of Governments (MACOG)		
Center for History		
Hannah Lindahl Children’s museum		
Michiana Jewish Historical Society		
Indiana Historic Spans Task Force		
Mishawaka-Penn-Harris Public Library		
City of South Bend		
St. Joseph County Comm.		
IN Lincoln Hwy. Assoc. Inc.		
Downtown South Bend, Inc.		
South Bend Heritage Fdn., Inc.		

Archaeology:

An Indiana Archaeological Short Report (Coon, 10/12/2011) was prepared for the project area. No archaeological sites were found within the project area and it is the recommendation of the report that the project be allowed to proceed as planned. The SHPO agreed with the conclusions of the report in a letter dated January 1, 2012. (See Appendix

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County St. Joseph Route SR 933 Des. No. 1173149 Project No. _____

D)

Historic Properties:

To evaluate above-ground resources within the APE, historians examined documentary sources and conducted fieldwork. Documentary research on the project included a review of secondary and primary sources such as state and local histories, National Register nominations, the INDOT 2009 Historic Bridge Inventory, and historic map websites.

Above-ground resources more than fifty years old in the APE, with a moderate or higher level of integrity were evaluated for the purposes of this study. As previously mentioned, including the subject structure, SR 933/Michigan Street/Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018), eight (8) properties, including four (4) historic districts and four (4) scattered sites met the requisite age and/or conditions criteria for National Register eligibility assessment. The following historic districts (or portions thereof) are located within the APE for the proposed project: 1) West North Shore Historic District (WNSHD; 201-598-37001-020); 2) Northshore Triangle Historic District (WNSTHD); #201-5998-36001-290); 3) Leeper Park Historic District (LPHD; NR-listed; #201-598-34001-018); 4) Harter Heights Historic District (HHHD; #201-597/598-35001-277). The following surveyed South Bend Scattered Sites were located in the proposed project area: 1) #201-597-10019 (Mason House; 140 E. Northshore Drive; ca.-1917 Prairie; Notable); 2) #201-597-10020 (O'Hara House; 134 E. Northshore Drive; ca.-1928 English Cottage; Notable); 3) #201-597-10021 (Taylor House; 128 E. Northshore Drive; ca.-1928 Dutch Colonial Revival; Contributing); 4) #201-597-10022 (Mattes House; 122 E. Northshore Drive; ca.-1907 Shingle/Dutch Colonial Revival; Notable). No other surveyed South Bend resources were located in the proposed project area. (See Appendix D)

Documentation, Findings:

INDOT, acting on FHWA's behalf, has determined a "No Adverse Effect" finding is appropriate for this undertaking because the project will not introduce any new visual, atmospheric or audible elements that would alter any of the characteristics that qualify the Leeper Park Historic District (#201-598-34001-34018), Samuel Leeper, Jr. House (#201-598-37018), or North Pumping Station (site #201-598-370141) for listing in the National Register of Historic Places.

The project will also will not introduce any new visual, atmospheric or audible elements that would alter any of those characteristics or qualities that qualify the following resources, recommended eligible for National Register-listing: 1) Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018); 2) Harter Heights Historic District (HHHD)/#201-597/598-35001-277); 3) Northshore Triangle Historic District (NSTHD)/#201-598-36001-290); 4) West North Shore Historic District (WNSHD)/#201-598-37001-020).

In addition, per "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridge PA), the project scope activities conducted as part of Des. #1173149 shall adhere to the Secretary of the Interior's Standards for

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County St. Joseph Route SR 933 Des. No. 1173149 Project No. _____

Rehabilitation, and will not introduce negative impacts as defined in 36 CFR §800.5(a)(ii) to the NR-eligible Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018).

Public Involvement:

In accordance with the INDOT (FHWA approved) Historic Bridge Programmatic Agreement, a public hearing was offered on March 21, 2012 at the South Bend Main Branch Library in the Dickenson Conference room. The Level 2 Categorical Exclusion document was available for public review for a minimum of 30 days ending on April 6, 2012. Copies of the Categorical Exclusion document were available for public review at the South Bend library located at 304 South Main St., South Bend, Indiana, the La Porte District located at 315 E Boyd Blvd, La Porte, Indiana and at the Hearings Examiner Office, Room N642, Indiana Government Center North, 100 North Senate Avenue, Indianapolis, Indiana.

The public notice requirements regarding INDOT's section 106 "No Adverse Effect" finding on behalf of the FHWA was fulfilled through this 30-day comment period. No specific comments were received regarding the "No Adverse Effect" finding. See Appendix H.

SECTION D – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES

Section 4(f) Involvement

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES</u> <u>Approval/dates</u>
	Yes	No	Yes	No	
Parks & Other Recreational Land					
Publicly owned park	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Publicly owned recreation area	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Other (school, state/national forest, bikeway, etc.)	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Programmatic Section 4(f)	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
Individual Section 4(f) Evaluation	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
"De minimis" Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>			

	<u>Presence</u>		<u>Use</u>		<u>FHWA / ES</u> <u>Approval/dates</u>
	Yes	No	Yes	No	
Wildlife & Waterfowl Refuges					
National Wildlife Refuge	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
State Fish & Wildlife Area – recreation or refuge areas only	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	
Programmatic Section 4(f)	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
Individual Section 4(f) Evaluation	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
"De minimis" Impact	<input type="checkbox"/>	<input checked="" type="checkbox"/>			Upon Final CE Approval

	Yes	No	Yes	No	<u>FHWA / ES</u> <u>approval/dates</u>
Historic Properties					
Sites eligible and/or listed on the NRHP	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
Programmatic Section 4(f)	<input type="checkbox"/>	<input checked="" type="checkbox"/>			
Individual Section 4(f) Evaluation	<input type="checkbox"/>	<input checked="" type="checkbox"/>			

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"De minimis" Impact

Discuss Programmatic Section 4 (f) and De minimis Section 4(f) impacts in the remarks section below. Individual Section 4(f) documentation must be separate Draft and Final documents. For further discussions on Programmatic, De minimis and Individual Section 4(f) documents please refer to the "Procedural Manual for the Preparation of Environmental Studies". Discuss proposed alternatives that satisfy the requirements of Section 4(f).

Remarks:

Sites eligible and/or listed on the NRHP, considered 4(f) p properties, are located within the project area. This includes the following properties:

- West North Shore Historic District (WNSHD; #201-598-37001-020)*
- Northshore Triangle Historic District (NTHD; #201-498-36001-290)*
- Leeper Park Historic District (LPHD; #201-598-34001-018),*
- The North Pumping Station (#201-598-34014; 830 N. Michigan Avenue)*
- Samuel Leeper House (#201-598-37018; 113 North Shore Drive)*
- Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018)*
- Harter Heights Historic District (HHHD); #201-597-598-35001-277)*
- West North Shore Historic District (WNSHD; #201-598-37001-020)*

This undertaking will not convert any of these eligible and/or listed NRHP Section 4(f) historic properties to a transportation use. INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. (See Appendix D)

No other 4(f) properties are located within the project area.

Section 6(f) Involvement

Presence

Use

Yes	No	Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Section 6(f) Property

Discuss proposed alternatives that satisfy the requirements of Section 6(f). Discuss any Section 6(f) involvement.

Remarks:

The National Park Service (NPS) Land & Water Conservation Fund website (LWCF) was reviewed. The proposed project is adjacent to Leeper Park which has received LWCF funds. However no work will take place within this 6(f) resource. (See Appendix G)

SECTION E – Air Quality

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Air Quality

Conformity Status of the Project

	Yes	No
Is the project in an air quality non-attainment or maintenance area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If YES, then:		
Is the project in the most current MPO TIP?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is the project exempt from conformity?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If the project is NOT exempt from conformity, then:		
Is the project in the Transportation Plan (TP)?	<input type="checkbox"/>	<input type="checkbox"/>
Is a hot spot analysis required (CO/PM)?	<input type="checkbox"/>	<input type="checkbox"/>
Is an MSAT level 1a Analysis required?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Is an MSAT level 1b Analysis required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is an MSAT level 2 Analysis required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is an MSAT level 3 Analysis required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is an MSAT level 4 Analysis required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is an MSAT level 5 Analysis required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

This project is of a type qualifying as a categorical exclusion (Group 1) under 23 CFR 771.117(d), or exempt under the Clean Air Act conformity rule under 40 CFR 93.116, and as such, a Mobile Source Air Toxics analysis is not required.

SECTION F - NOISE

Noise	Yes	No
Is a noise analysis required in accordance with FHWA regulations and INDOT's noise policy?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	No	Yes/ Date
ES Approval of Noise Analysis	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

This project is a Type III project. In accordance with 23 CFR 772 and the INDOT Traffic Noise Policy (FHWA concurrence on July 13, 2011), this action does not require formal noise analysis.

SECTION G – COMMUNITY IMPACTS

Regional, Community & Neighborhood Factors

	Yes	No
Will the proposed action comply with the local/regional development patterns for the area?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed action result in substantial impacts to community cohesion?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the proposed action result in substantial impacts to local tax base or property values?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will construction activities impact community events (festivals, fairs, etc.)?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Remarks:

Inconvenience associated with construction such as increased travel times, possible utility interruptions, vehicular operating costs, construction noise, and fugitive dust should be expected. No economic impacts are anticipated for the proposed project. No impacts to the local tax base and property values are anticipated.

Indirect and Cumulative Impacts

Will the proposed action result in substantial indirect or cumulative impacts?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

The project is not likely to result in substantial indirect and cumulative impacts.

Public Facilities & Services

Will the proposed action result in substantial impacts on health and educational facilities, public utilities, fire, police, emergency services, religious institutions, public transportation or pedestrian and bicycle facilities? Discuss the maintenance of traffic, and how that will affect public facilities and services.

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

The proposed action will not have a substantial impact on public facilities and services. SR 933 will be detoured for portions of construction. Public services will be notified in advance of the closure of SR 933.

Environmental Justice (EJ) (Presidential EO 12898)

During the development of the project were EJ issues identified?

Are any EJ populations located within the project area?

Will the project result in adversely high or disproportionate impacts to the EJ population?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Remarks:

This project does not require a full analysis as no relocations or right-of-way will be required. Therefore there will be no disproportionately high adverse environmental or health impacts to low-income populations or minority population's area as result of this project.

Relocation of People, Businesses or Farms:

Will the proposed action result in the relocation people, businesses or farms?

Is a Business Information Survey (BIS) required?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

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Is a Conceptual Stage Relocation Study (CSRS) required?

Number of relocations: Residences: 0 Businesses: 0 Farms: 0 Other: 0

If a BIS or CSRS is required, discuss the results in the Remarks section.

Remarks: This project will not require the relocation of people, businesses or farms.

SECTION H – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

	<u>Documentation</u>	
	Yes	No
Red Flag Investigation	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Hazardous Materials Site Assessment Form	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Phase I Initial Site Assessment (ISA)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Phase II Preliminary Site Investigation(PSI)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Design/Specifications for Remediation required?	<input type="checkbox"/>	<input checked="" type="checkbox"/>

	No	Yes/ Date
ES Review of Investigations	<input checked="" type="checkbox"/>	<input type="checkbox"/>

Include a summary of findings for each investigation.

Remarks: A red flag survey was completed on September 29, 2011. A hazardous materials site inspection was also completed. No potentially hazardous sites were identified in the project vicinity or in the project area. This project will not require excavation; therefore review by INDOT Environmental Services has been waived. No further investigation is needed concerning hazardous materials. (See Appendix E)

SECTION I – PERMITS CHECKLIST

	<u>Required</u>	<u>Not Required</u>
Army Corps of Engineers (404/Section10 Permit)		
Individual Permit (IP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Nationwide Permit (NWP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Regional General Permit (RGP)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Pre-Construction Notification (PCN)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDEM		
Section 401 WQC	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Isolated Wetlands determination	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Rule 5	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Wetland Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Stream Mitigation required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
IDNR		
Construction in a Floodway	<input checked="" type="checkbox"/>	<input type="checkbox"/>

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Navigable Waterway Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Lake Preservation Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Mitigation Required	<input type="checkbox"/>	<input checked="" type="checkbox"/>
US Coast Guard Section 9 Bridge Permit	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Others (Please discuss in the Remarks section below)	<input type="checkbox"/>	<input type="checkbox"/>

Remarks:

It will be the responsibility of the designer to submit plans to INDOT Environmental Services to process permits. All appropriate permits must be obtained before construction may begin. This project may require and Indiana Department of Natural Resources Construction in a Floodway permit. As currently scoped no other permits are anticipated for this project. If the scope of the project changes the La Porte District Environmental Section should be notified immediately.

SECTION J- ENVIRONMENTAL COMMITMENTS

Information below must be included on Commitments Summary Form. List all commitments, indicating which are firm and which are optional.

Remarks:

- 1) During construction and operation appropriate safeguards and best management practices for storm water need to be in place to ensure that ground water is not endangered. Such precautions would include notifying general contractors that the site is sensitive, securing adequate precautions for fueling/servicing large equipment, and developing contingency plans to handle the release of any hazardous materials. **EPA, firm**

- 2) Appropriately designed measures for controlling erosion and sediment must be implemented to prevent sediment from entering the stream, or leaving the construction site; maintain these measures until construction is complete and all disturbed areas are stabilized. **IDEM, firm**

- 3) Revegetate “low maintenance” areas with a mixture of grasses (excluding all varieties of tall fescue), legumes, and native shrub or hardwood tree species as soon as possible upon completion; low endophyte tall fescue may be used “high maintenance” areas only. **DNR, for consideration**

- 4) If additional permanent or temporary right of way is determined to be required, the Office of Environmental Services will be contacted immediately. **INDOT, firm**

- 5) If any potential hazardous materials are discovered during construction the IDEM Spill Line should be notified with details of the discovery within 24 hours. INDOT Office of Environmental Services, Hazardous Materials Unit should then be contacted to organize the proper handling of the material to be in accordance with the IDEM guidelines. **INDOT, firm**

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- 6) Asphalt paving plants will be permitted to operate properly. The use of cutback asphalt, or asphalt emulsion containing more than 7% oil distillate, is prohibited and will not occur during the months of April through October. **IDEM, firm**
- 7) Minimize and contain within the project limits in-channel disturbance and the clearing of trees and brush. **DNR, firm**
- 8) No open burning of construction wastes is permitted without proper variance. **IDEM, firm**
- 9) Fugitive dust must be controlled by proper wetting, chemical stabilizers, or wind barriers. Dirt tracked onto paved roads should be minimized. **IDEM, firm**
- 10) Should any archaeological artifacts or human remains are uncovered during construction, demolition, or earth moving activities, state law (Indiana Code 14-21-1-27 and 29) requires that discovery must be reported to the Department of Natural resources within two (2) business days. **DNR, firm**
- 11) All solid wastes generated by the project, or removed from the project site, need to be taken to a properly permitted solid waste processing or disposal facility. Remove all broken concrete and construction debris upon completion of the project. **IDEM, firm**
- 12) All appropriate permits must be obtained before construction may begin. Required permits may include, but are not limited to, an IDEM 401 Water Quality Permit, ACOE 404 Nationwide and Lake Preservation Act. If more than one acre of disturbed soil is anticipated an IDEM Rule 5 Ground Water Notification of Intent must be filed before construction begins. **INDOT, firm**
- 13) The Michiana Regional Airport is located approximately 14,400' Northwest of the project. If any permanent structures or equipment utilized for the project penetrates the 100:1 slope from the airport FAA form 7460 (Notice of Proposed construction or alteration) must be filed. **INDOT, firm**

SECTION K- EARLY COORDINATION

Please list the date coordination was sent and all agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.

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Remarks:

Early coordination was initiated on May 31, 2011 with applicable federal, state, and local agencies. Review comments from those agencies that returned a reply have been incorporated into this study, as appropriate. The agencies contacted and the date on which they replied is identified below.

Agency	Response Received	Appendix Page
United States Fish and Wildlife Service	December 9, 2011	C-10
Michiana Area Council of Governments	December 27, 2011	C-12
Indiana Department of Environmental Management, Automatic Review	December 9, 2011	C-13
Groundwater Section, Indiana Department of Environmental Management	December 12, 2011	C-14
Environmental Protection Agency, Ground Water and Drinking Water Branch	December 12, 2012	C-15
Natural Resources Conservation Service	December 28, 2011	C-16
Indiana Geological Section	January 12, 2012	C-18
Aviation Section, Indiana Department of Transportation	December 15, 2011	C-19
Public Hearings, Indiana Department of Transportation	December 28, 2011	C-20
US Department of Housing & Urban Development		
Regional Environmental Coordinator, National Park Service		
Federal Highway Administration		
Environmental Coordinator, Indiana Department of Natural Resources		



INDIANA DEPARTMENT OF TRANSPORTATION

LaPorte District

Designation Number 1173149

Appendices

A. INDOT Supporting Documentation

- A1 CE Threshold Chart
- A2-A5 TIP Amendment August 2, 2011
- A6-A8 STIP Amendment July 13, 2011

B. Graphics

- B1 County Map
- B2 Aerial Map
- B3 Topo Map
- B4 Karst Region Map
- B5 Sole Source Aquifer Region
- B6 General Site Photographs
- B7-B14 Design Plans (full plans can be found in Appendix D)

C. Early Coordination

- C1-C9 Early Coordination Letter
- C10-C11 USFWS Response
- C12 MACOG Response
- C13 IDEM Automatic Response
- C14 IDEM Wellhead Protection Response
- C15 EPA Response
- C16-C17 NRCS Response
- C18 IGS Response
- C19 INDOT Aeronautical Division Response
- C20 INDOT Public Hearings Response

D. Section 106 Studies

- D1-D110 800.11 Documentation and Finding

E. Red Flag and Environmental Site Assessment

- E1-E8 Red Flag Survey
- E9 Environmental Site Assessment

F. Waters Report

- F1-F9 Waters Determination Report

G. Section 6(f)

- G1-G21 Section 4(f) determination correspondence
- G22-G23 Section 6(f) NPS Listing St. Joseph County

H. Public Involvement

H1	Public Involvement Signature Page
H2-H8	Public Hearing Presentation Notes and Slides
H9-H11	Attendance Sheet
H12-H40	Comments Received
H41-H47	Legal/Notice Publishers Affidavit
H48-H51	Legal Notice Recipients

Appendix A

INDOT Supporting Documentation

Categorical Exclusion Level Thresholds

	Level 1	Level 2	Level 3	Level 4
Relocations	None	≤ 2	> 2	> 10
Right of way¹	< 0.5 acres	< 10 acres	≥ 10 acres	≥ 10 acres
Length of added through lane	None	< 1 miles	≥ 1 mile	≥ 1 mile
Permanent Traffic pattern alteration	None	None	Yes	Yes
New alignment	None	None	< 1 mile	≥ 1 mile ²
Wetlands	< 0.1 acres	< 1 acre	< 1 acre	≥ 1 acre
Stream Impacts	≤ 300 linear feet of stream impacts, no work beyond 75 feet from pavement	> 300 linear feet impacts, or work beyond 75 feet from pavement	N/A	N/A
Section 4(f)*	None	None	None	Any impacts
Section 6(f)	None	None	Any impacts	Any impacts
Section 106	“No Historic Properties Affected” or falls within guidelines of Minor Projects PA	“No Adverse Effect” or “Adverse Effect”	N/A	If ACHP involved
Noise Analysis Required	No	No	Yes ³	Yes ³
Threatened/Endangered Species*	“Not likely to Adversely Affect”, or Falls within Guidelines of USFWS 9/8/93 Programmatic Response	N/A	N/A	“Likely to Adversely Affect” ⁴
Sole Source Aquifer Groundwater Assessment	Detailed Assessment Not Required	Detailed Assessment Not Required	Detailed Assessment Not Required	Detailed Assessment Required
Approval Level • ESM ⁵ • ES ⁶ • FHWA	Yes	Yes	Yes Yes	Yes Yes Yes

*These thresholds have changed from the March 2009 Manual.

¹Permanent and/or temporary right of way.

²If the length of the new alignment is equal to or greater than one mile, contact the FHWA’s Air Quality/Environmental Specialist.

³In accordance with INDOT’s Noise Policy.

⁴If the project is considered Likely to Adversely Affect Threatened and/or Endangered Species, INDOT and the FHWA should be consulted to determine whether a higher class of document is warranted.

⁵Environmental Scoping Manager

⁶Environmental Services

July 13, 2011

Mr. Jerry Halperin
MPO Coordinator
INDOT, IGC N955
100 N. Senate Avenue
Indianapolis, IN 46204

Dear Mr. Halperin:

Please amend the FY2012-2015 Transportation Improvement Program (TIP) and the FY 2012-2015 Indiana Statewide Transportation Improvement Program (STIP) to include the following:

LaPorte District Initiated – Resolution 34-11

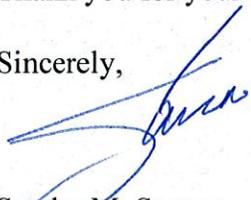
A. INDOT Amendment Exempt Project in St. Joseph County, IN
Resolution 34-11 Add Project Phase: PE
Des #:1173149 **Location: SR 933 over St. Joseph River, 1.59 mi N of SR 23**
Bridge Replacement – District Bridge Project SFY: 2012
Total: \$450,000 Federal: \$360,000 (ST STP) State: \$90,000

The Michiana Area Council of Governments, as the designated Metropolitan Planning Organization for the South Bend and Elkhart/Goshen, Indiana Urbanized Areas, is hereby submitting an amendment to the FY2012-2015 TIP and the 2012-2015 STIP for Intergovernmental Review under the provisions of Executive Order 1271. The document has been reviewed and endorsed by the MACOG Policy Board on July 13, 2011.

We are, therefore, offering our assurance that the document has been developed with and reviewed by the appropriate jurisdictions and governmental agencies within the South Bend and Elkhart/Goshen Urbanized Areas. This includes the Indiana Department of Transportation and member jurisdictions and governmental agencies of the MACOG Policy Board.

Thank you for your attention to this matter. Please contact this agency if you need further information.

Sincerely,



Sandra M. Seanor
Executive Director

SMS:cjk

Enclosure

Cc: Justin Sergent, Joyce Newland, Lisa Shrader, Marcia Blansett

F:\ABC\MACOG\MEETINGS\SFY 2012\POLICY BOARD\Resolution Letters\07-July 25-34\L07Resol34.doc

RESOLUTION NO. 34-11
A RESOLUTION ENDORSING AN AMENDMENT TO THE SOUTH BEND AND ELKHART/GOSHEN FY2012-2015 TRANSPORTATION IMPROVEMENT PROGRAM

WHEREAS, the SAFETEA-LU Safe, Accountable, Flexible, Efficient Transportation Equity Act: A Legacy of Users (2005) designating joint Federal Highway Administration/Federal Transit Administration rules and regulations, under Section 134, Title 23 of the US Code (h) and CFR, Part 450, Sub-part A, of the previous STAA act require the annual preparation of a Transportation Improvement Program by the designated Metropolitan Planning Organization(s) as a part of the transportation planning process in urbanized areas of 50,000 or more population; and

WHEREAS, the Michiana Area Council of Governments (MACOG) being the duly designated Metropolitan Planning Organization for the South Bend and Elkhart/Goshen Urbanized Areas, in cooperation with local governmental units and implementing agencies within the urbanized areas, has prepared a FY2012-2015 Transportation Improvement Program (TIP) which consists of improvements recommended in the MACOG Region.

WHEREAS, the MACOG Policy Board from time to time receives and amends/modifies its Transportation Improvement Programs.

WHEREAS, the MACOG consulted with the Interagency Consultation Group and the agencies concurred in the MPO finding that the TIP amendment projects are exempt, per 40 CFR 93.126 & 127, and there is no need to update the conformity analysis or issue a new conformity finding.

BE IT THEREFORE RESOLVED, that the South Bend and Elkhart/Goshen FY2012-2015 TIP will be amended to include the following:

Amend the FY2012-2015 Transportation Improvement Plan

LAPORTE DISTRICT

District Initiated – Resolution 34-11

A. INDOT Amendment Exempt Project in St. Joseph County, IN

Resolution 34-11 Add Project Phase: PE

Des #:1173149 **Location: SR 933 over St. Joseph River, 1.59 mi N of SR 23**

Bridge Replacement – District Bridge Project SFY: 2012

Total: \$450,000 Federal: \$360,000 (ST STP) State: \$90,000

RESOLVED THIS 13th day of July, 2011.

Michiana Area Council of Governments



Frank Lucchese, Chairman

DES # / Grant #	Sponsor	Amendment	Road	Project Description/Location	Project Category	MACOG Letting Date	SFY	LRP Open to Traffic Date	Phase	Type I	Federal I	Type II	Federal II	State	Local	Total
9133615	INDOT	Oct 66-09 p552	SR 23	from 0.2 mi S of Campeau St to 0.05 mi S of Edison (Eddy St to S Bend Blvd)	Added Travel Lanes		2012		RW	MMP	\$ 1,200,000			\$ 300,000		\$ 1,500,000
9133615	INDOT	Oct 66-09 p552	SR 23	from 0.2 mi S of Campeau St to 0.05 mi S of Edison (Eddy St to S Bend Blvd)	Added Travel Lanes		2012		CN	MMP	\$ 1,000,000			\$ 250,000		\$ 1,250,000
9133615	INDOT	Nov 75-10 p 883	SR 23	from 0.2 mi S of Campeau St to 0.05 mi S of Edison (Eddy St to S Bend Blvd)	Added Travel Lanes		2012		CN	MMP	\$ 8,320,000			\$2,080,000		\$10,400,000
1172781	TRANSPO			Preventive Maintenance	Transit PM		2012	Z700		5307	\$ 1,600,000				\$ 400,000	\$ 2,000,000
1172782	TRANSPO			Preventive Maintenance	Transit PM		2013	Z700		5307	\$ 1,600,000				\$ 400,000	\$ 2,000,000
1172783	TRANSPO			Preventive Maintenance	Transit PM		2014	Z700		5307	\$ 1,600,000				\$ 400,000	\$ 2,000,000
1172784	TRANSPO			Transit Operating/Preventive Maintenance	Transit Operating		2015	Z100		5307	\$ 1,680,000				\$ 420,000	\$ 2,100,000
1172785	TRANSPO			Expand Security System	Transit Operating		2012	Z100		5307	\$ 80,000				\$ 20,000	\$ 100,000
1172786	TRANSPO			Transit Communicatons Equipment	Transit Communications Eqpt		2012	Z400		5307	\$ 12,000				\$ 3,000	\$ 15,000
1172787	TRANSPO			Education & Training (1/2 of 1% allocation)	Transit Operating		2012	Z100		5307	\$ 16,000				\$ 4,000	\$ 20,000
1172788	TRANSPO			Maintenance/Shop Equipment	Transit Misc Equipment		2012	Z300		5307	\$ 80,000				\$ 20,000	\$ 100,000
1172789	TRANSPO			Rehabilitation of buses (8-10)	Transit Operating		2012	Z100		5307	\$ 536,342				\$ 134,086	\$ 670,428
1172790	TRANSPO			Passenger Facility Rehabilitation	Transit Facilities		2012	Z500		5307	\$ 252,000				\$ 63,000	\$ 315,000
1172791	TRANSPO			Purchase/Upgrade Computer Software	Transit Communications Eqpt		2013	Z400		5307	\$ 12,000				\$ 3,000	\$ 15,000
1172792	TRANSPO			Purchase Maintenance Equipment	Transit Misc Equipment		2013	Z300		5307	\$ 376,342				\$ 94,086	\$ 470,428
1172793	TRANSPO			Education & Training (1/2 of 1% allocation)	Transit Planning		2013	Z800		5307	\$ 8,000				\$ 2,000	\$ 10,000
1172794	TRANSPO			Passenger Facility Rehabilitation	Transit Facilities		2013	Z500		5307	\$ 500,000				\$ 125,000	\$ 625,000
1172795	TRANSPO			Maintenance/Shop Equipment	Transit Misc Equipment		2013	Z300		5307	\$ 80,000				\$ 20,000	\$ 100,000
1172796	TRANSPO			Education & Training (1/2 of 1% allocation)	Transit Planning		2014	Z800		5307	\$ 24,000				\$ 6,000	\$ 30,000
1172797	TRANSPO			Safety Equipment	Transit Operating		2014	Z100		5307	\$ 41,600				\$ 10,400	\$ 52,000
1172798	TRANSPO			Rehabilitate Bus Terminal	Transit Facilities		2014	Z500		5307	\$ 110,742				\$ 27,686	\$ 138,428
1172799	TRANSPO			Replace Fixed Route Vehicles	Transit Purchase Vehicles		2014	Z200		5307	\$ 800,000				\$ 200,000	\$ 1,000,000
1172800	TRANSPO			Tire Lease	Transit Operating		2015	Z100		5307	\$ 80,000				\$ 20,000	\$ 100,000
1172801	TRANSPO			Replacement of 2003 Gillig Buses	Transit Purchase Vehicles		2015	Z200		5307	\$ 1,331,610				\$ 332,903	\$ 1,664,513



U.S. Department
of Transportation
**Federal Highway
Administration**

Indiana Division

575 North Pennsylvania Street, Room 254
Indianapolis, Indiana 46204

August 2, 2011

In Reply Refer To:
HDA-IN

Mr. Jim Stark
Deputy Commissioner
Planning and Project Management
100 North Senate Avenue
Indianapolis, IN 46204

Dear Mr. Stark:

We have completed our review of Amendment #12-02 to the FY 2012-2015 Indiana Statewide Transportation Improvement Program (STIP) as transmitted by INDOT's letter dated via e-mail August 1, 2011. FHWA approves it for inclusion into the STIP.

If you have any questions, please call Joyce Newland, Planning Program Manager, at (317) 226-5353 or e-mail at joyce.newland@dot.gov.

Sincerely,

RF Robert F. Tally Jr., P.E.
Division Administrator

Attachment

cc: transmitted by e-mail
Jeanette Wilson, INDOT



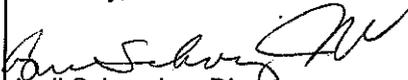
August 1, 2011
 Federal Highway Administration
 Attention: Ms. Joyce Newland
 575 N. Pennsylvania Street, Room 254
 Indianapolis, IN 46204

SUBJECT: 2012-2015 Amendment 12-02

Dear Mr. Tally:
 INDOT is requesting the projects listed be amended into the FY 2012-FY 2015 STIP.
 If you have any questions, please contact me.

TOTAL \$ 114,422,707

Sincerely,


 April Schwering, Director
 Intermediate Range Planning

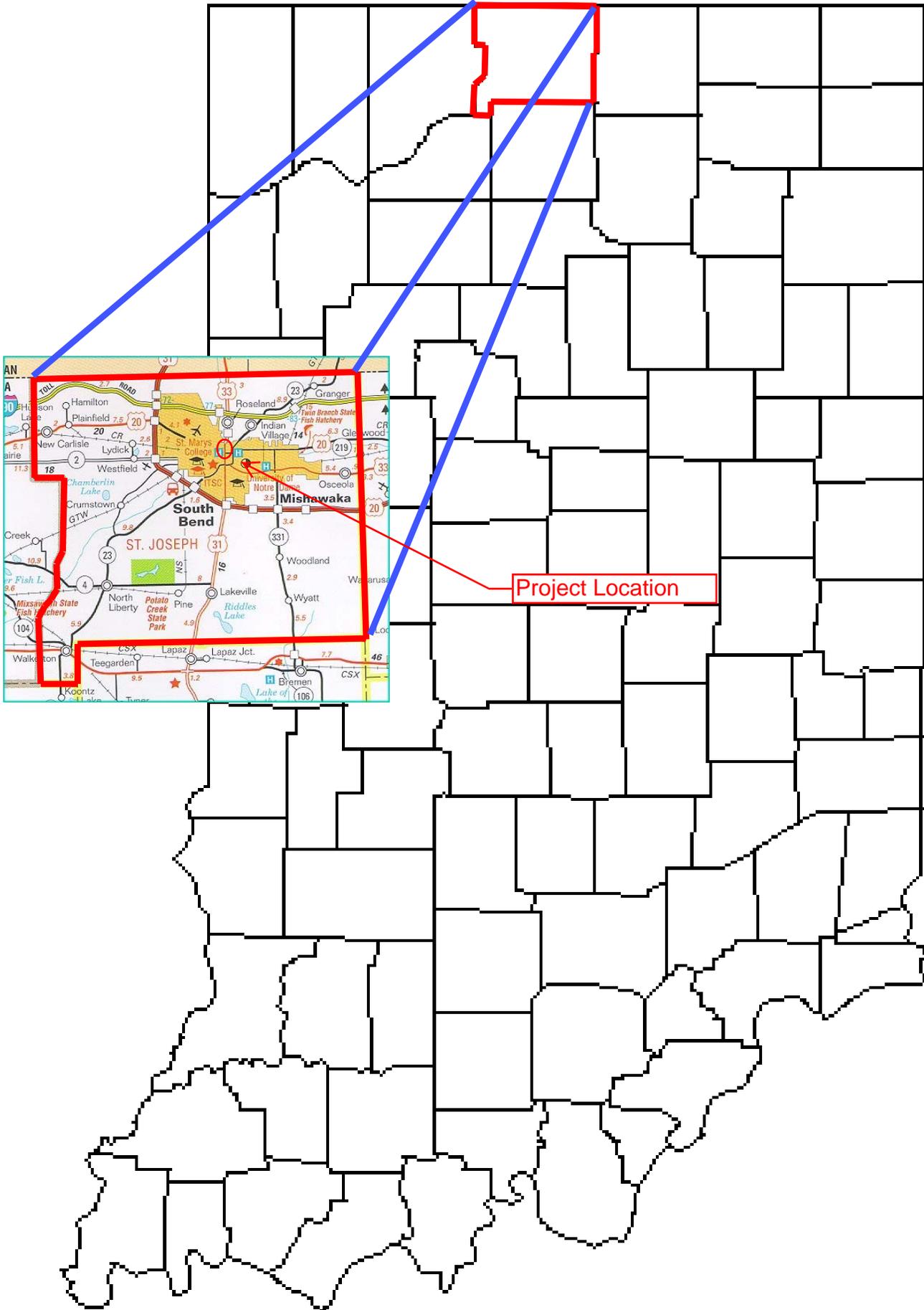
DES	Route Type	Number	Location	County	Work Type	Work Category	Phase	Phase Year	Amount	Funding	Remarks
1005378	Local	IR 1001	CR 950 N (CR #74) at NS near Town of Springport	Henry	Railroad Protection	Central Office Safety Project	CN	2012	\$265,882.00	Local Safety	CN not listed in 2012 INSTIP
1005375	Local	1001	CR 200 W (CR #133) at CSX West of Connersville	Fayette	Railroad Protection	Central Office Safety Project	CN	2012	202,704.00	STP	CN not listed in 2012 INSTIP
1005377	Local	1001	CR 450 S(CR #36) at NS South of Hartford City	Blackford	Railroad Protection	Central Office Safety Project	CN	2012	\$293,417.00	Local Safety	CN not listed in 2012 INSTIP
1006240	US	52	4.43 Mi E of E Junction with SR 1	Franklin	Small Structure Replacement	District Small Structure Project	PE	2012	\$ 75,000	STP	Not in TIP/STIP
0500083	Local		Bridge Replacement, Other Construction on IR1006, Bridge #17 over McCarty Ditch on CR 500 W	Gibson	Bridge Replacement, Other Construction	Local Bridge Project	CN	2012	\$ 1,122,200	BR	Letting is 10/13/2011. Award amt is \$720,000
0710980	US	150	656 ft W of CR 225 to 0.28 mi W of Jct SR 37	Orange	Road Reconstruction (3R/4R Standards)	Major Pavement Project (Non-NHS)	PE	2012	\$ 28,265	STP	Adding PE to change design from Metric to English
1173154	SR	930	Various locations within the Fort Wayne District	Allen	Other Type Project (Miscellaneous)	Planning/Feasibility/Corridor Study Project	PE	2012	\$ 80,000	STP	new project

DES	Route Type	Number	Location	County	Work Type	Work Category	Phase	Phase Year	Amount	Funding	Remarks
1173265	Local	1006	Bridge 105 on Becks Mill Road	Washington	Bridge Maintenance And Repair	Local Bridge Project	PE	2012	\$ 115,000	STP	Project not listed in STIP
1173263	Local	MS0	Along the Madison railroad incline in Madison	Jefferson	Enhancement	Local Transportation Enhancement Project	PE	2012	\$ 55,000	STP	Project not listed in STIP
1173253	US	50	US 50 @ I-65 interchange	Jackson	Enhancement	Local Transportation Enhancement Project	PE	2012	\$ 121,000	STP	Project not listed in STIP
0710945	SR	19	From 0.36 Miles S of SR 14 to SR 25	Kosciusko & Fulton Co.	HMA Overlay, Functional	District Pavement Project (Non-I)	CN	2012	\$ 2,600,000	STP	This project does not show up in MACOG's TIP. It does show up in the new draft STIP document.
1173149	SR	933	over St. Joseph River, 1.59 mi N of SR 23	St. Joseph	Bridge Replacement, Concrete	District Bridge Project (Replacement)	PE	2012	\$ 450,000	STP	Amend into current TIP/STIP
1173234	Local	Muncie	Prairie Creek Reservoir Trail (Phase 1) to develop and construct 0.85 mile of trail, a trailhead, an ATV area and trail amenities.	Delaware	Other Type Project (Miscellaneous)	Recreational Trails Program (DNR)	PE	2012	\$ 50,000	RTP	Project amended into the 2012-2015 DMMPC TIP on July 20, 2011 by Resolution # 11-09.
1173234	Local	Muncie	Prairie Creek Reservoir Trail (Phase 1) to develop and construct 0.85 mile of trail, a trailhead, an ATV area and trail amenities.	Delaware	Other Type Project (Miscellaneous)	Recreational Trails Program (DNR)	RW	2012	\$ 6,500	RTP	Project amended into the 2012-2015 DMMPC TIP on July 20, 2011 by Resolution # 11-09.
1173234	Local	Muncie	Prairie Creek Reservoir Trail (Phase 1) to develop and construct 0.85 mile of trail, a trailhead, an ATV area and trail amenities.	Delaware	Other Type Project (Miscellaneous)	Recreational Trails Program (DNR)	CN	2012	\$ 187,500	RTP	Project amended into the 2012-2015 DMMPC TIP on July 20, 2011 by Resolution # 11-09.

Appendix B

Graphics

General Project Information - Aerial Map
SR 933, St. Joseph County, Indiana
1173149, Bridge Repair



General Project Information - Aerial Map

SR 933, St. Joseph County, Indiana

1173149, Bridge Repair



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:10,000

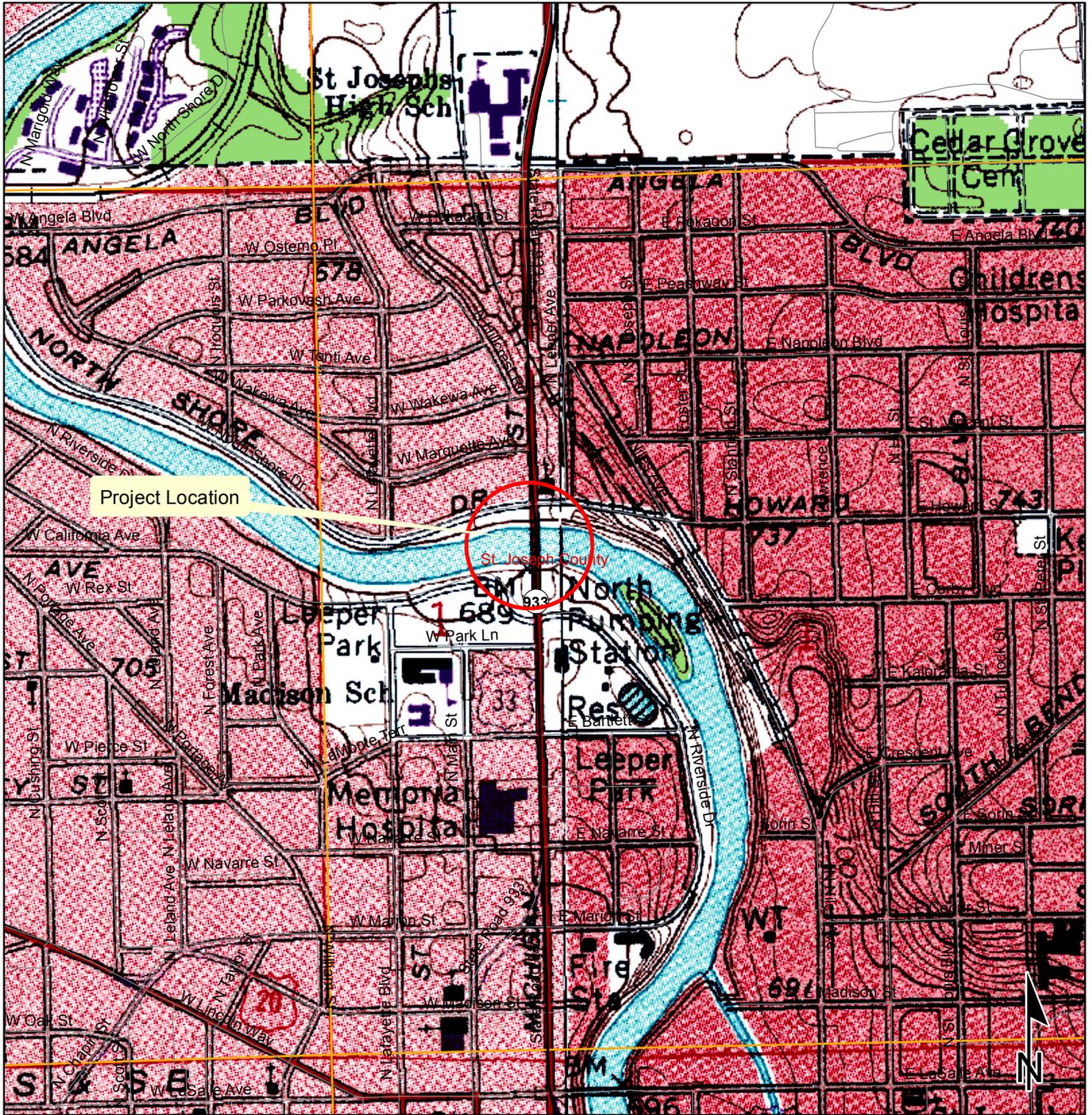


B-2

General Project Information - Topo Map

SR 933, St. Joseph County, Indiana

1173149, Bridge Repair



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:10,000 Miles

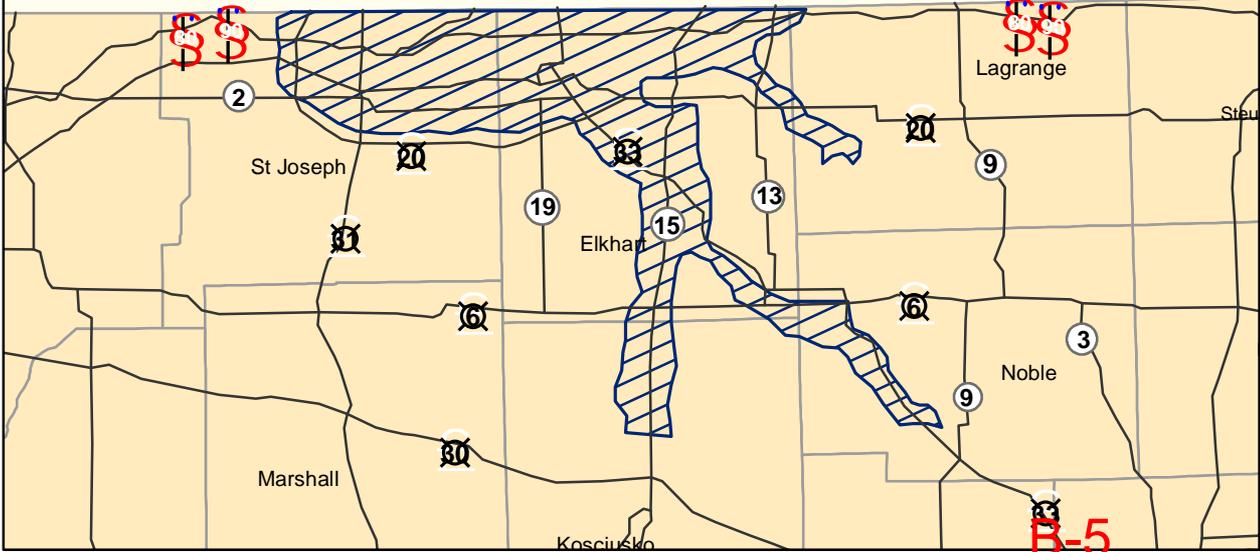




 Potential Karst Features Region

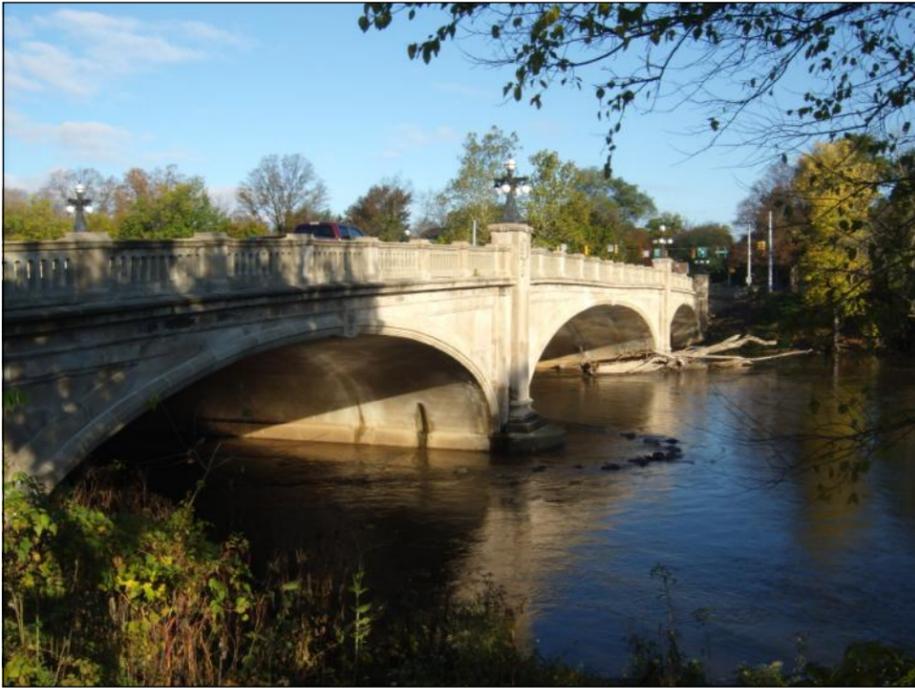


Project Location



B-5

General Project Information - Photographs
SR 933, St. Joseph County, Indiana
Des No 1173149, Bridge Rehabilitation



Picture 1: SR 933 Bridge looking northwest.



Picture 2: SR 933 Bridge looking north.



Picture 3: SR 933 Bridge looking south.



Picture 4: St. Joseph River looking east from SR 933 Bridge.



Picture 5: St. Joseph River looking west from SR 933 Bridge.

PROJECT	DESIGNATION
BR-230-6(002)	1173149
CONTRACT	BRIDGE FILE
	(933)31-71-3690C

STRUCTURE	TYPE	SPAN AND SKEW	OVER	± STATION
(933)31-71-3690C	REINFORCED CONCRETE ARCH	84'-0" 120'-0" 84'-0" NO SKEW	ST. JOSEPH RIVER	16+16 "C"

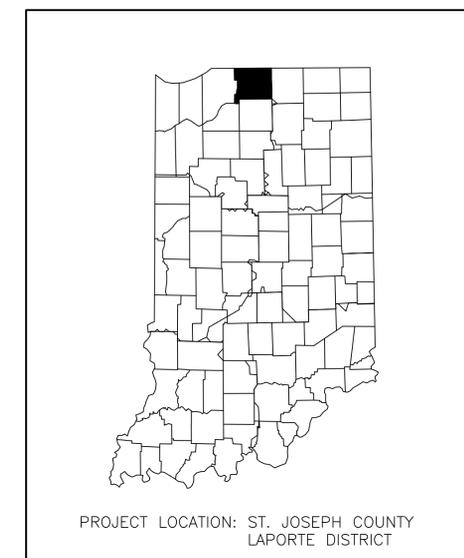
INDIANA DEPARTMENT OF TRANSPORTATION



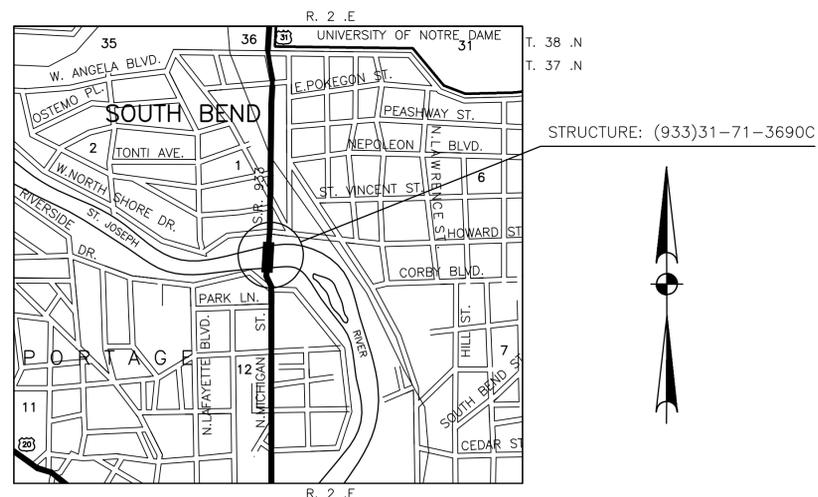
BRIDGE PLANS FOR SPANS OVER 20 FEET PROJECT NO. BR-230-6(002)

MISCELLANEOUS MAINTENANCE AND IMPROVEMENTS ON STRUCTURE: (933)31-71-3690C, S.R. 933 (MICHIGAN STREET) OVER ST. JOSEPH RIVER LOCATED IN SECTION 1, T.37 N., R.2 E., PORTAGE TOWNSHIP, ST. JOSEPH COUNTY, INDIANA.

TRAFFIC DATA		
A.A.D.T. (2001)	25910	V.P.D.
A.A.D.T. (2032)	49370	V.P.D.
D.H.V. (2032)	4937	V.P.H.
DIRECTIONAL DISTRIBUTION	50	%
TRUCKS	5	% A.A.D.T.
		% D.H.V.
EQUIVALENT SINGLE AXLE LOADS		
DESIGN DATA		
DESIGN SPEED	35	M.P.H.
PROJECT DESIGN CRITERIA	3R NON-FREEWAY	
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL	
RURAL/URBAN	URBAN	
TERRAIN	LEVEL	
ACCESS CONTROL	NONE	



LATITUDE: 41°41'17"N LONGITUDE: 86°15'02"W



LOCATION IN ST. JOSEPH COUNTY
Not to Scale

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2012
TO BE USED WITH THESE PLANS

U:\15298\Pro\Development\Design\Drawings\5298B701.dwg Donald Sheetz Plot: 12/19/2011 2:39 PM Save: 12/14/2011 1:33 PM

BFS
Butler Fairman Seufert
CIVIL ENGINEERS

Headquarters
8450 WESTFIELD BLVD., SUITE 300
INDIANAPOLIS, IN 46240-8302
TEL 317-713-4616
FAX 317-713-4616
www.BFSEngr.com

Branch Locations
FORT WAYNE 260-459-1532
JEFFERSONVILLE 812-285-0590
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

FEDERAL HIGHWAY ADMINISTRATION
U.S. DEPT. OF TRANSPORTATION
APPROVED: _____
DATE _____
DIVISION ADMINISTRATOR

PLANS
PREPARED BY: Butler, Fairman & Seufert (317) 713-4615
PHONE NUMBER
CERTIFIED BY: _____ DATE _____
APPROVED FOR LETTING: _____ DATE _____
INDIANA DEPARTMENT OF TRANSPORTATION

CONTRACT	FILE
	(933)31-71-3690C
PROJECT	DESIGNATION
	1173149
BR-230-6(002)	SHEET
	1 OF 27

BFS NO. 5298

INDEX

SHEET NUMBER	SUBJECT
1	TITLE SHEET
2	INDEX SHEET
3-4	TRAFFIC MAINTENANCE DETAILS
5	GENERAL PLAN
6	TYPICAL SECTIONS
7	MISCELLANEOUS DETAILS
8-13	CRACK MAPPING AND FRP STRIPS
14-19	RAILING DETAILS SHOWING REMOVALS AND RAILING POST REHABILITATION
20-25	RAILING DETAILS SHOWING RECONSTRUCTION
26	RAILING DETAILS
27	BRIDGE SUMMARY

UTILITIES

*GAS: Nipsco
1039 E. Pennsylvania Ave.
South Bend, In 46618
(574) 284-2277*

*ELECTRIC: American Electric Power
2929 W. Lathrop St.
South Bend, In 46628
(574) 283-1819*

*SANITARY SEWER/WATER: City Of South Bend Div.
Of Public Works
Engineering Dept.
224 N. Main St.
South Bend, In 46601
(574) 235-9251*

*TELEPHONE: SBC
307 S. Main St.
South Bend, In 46307
(574) 237-8380*

*Sprint Communications
5600 N. River Rd., Suite 500
Rosemont, IL 60018
(847) 318-3192*

NOTE: There may be other utilities other than these that are listed that were not notified by underground Indiana, and no evidence was discovered as to their identity.

REVISIONS

SHEET NO.	DATE	REVISED

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Butler Fairman Soufer
CIVIL ENGINEERS

Headquarters:
8450 WESTFIELD BLVD., SUITE 300
INDIANAPOLIS, IN 46240-8302
TEL 317-713-4615
FAX 317-713-4616
www.BFSeng.com

Branch Locations:
FORT WAYNE 260-458-1532
JEFFERSONVILLE 812-286-0990
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

RECOMMENDED FOR APPROVAL: _____ DATE _____

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: M. EICHENAUER CHECKED: M. EICHENAUER

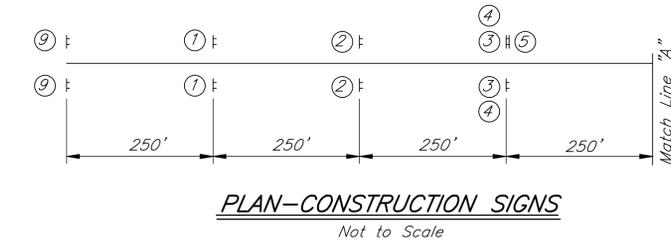
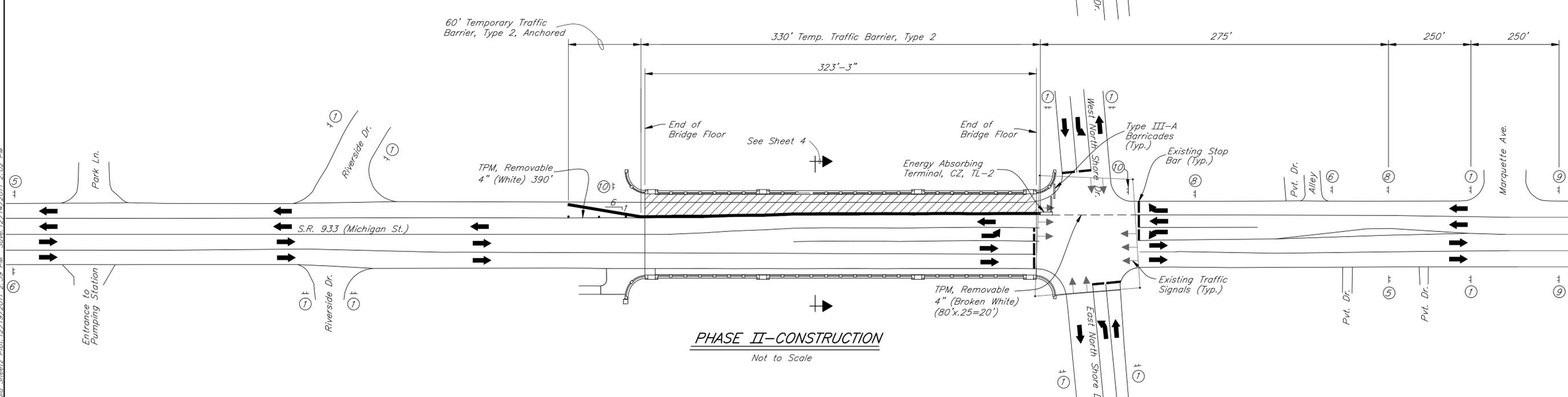
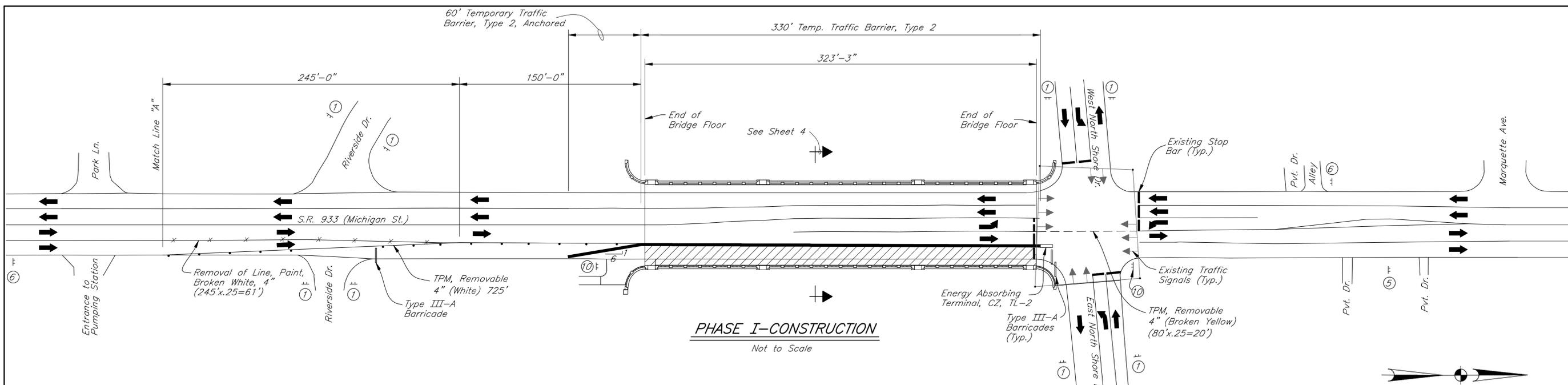
INDIANA DEPARTMENT OF TRANSPORTATION

INDEX SHEET

HORIZONTAL SCALE	FILE
NONE	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
NONE	1173149
SURVEY BOOK	SHEET
	2 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

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- SIGN LEGEND**
- ① XW20-1 Road Construction Ahead
 - ② XW20-5(R) Right Lane Closed Ahead
 - ③ XW4-2-A(L) Merge
 - ④ XW13-1-A Speed Advisory
 - ⑤ XG20-2 End Construction
 - ⑥ XG20-5 Lane Closure Notice Sign
 - ⑦ XW1-4-A(L) Reverse Curve
 - ⑧ R3-7 Right Lane Must Turn Right
 - ⑨ XG20-7a Speeding/Reckless Driving Sign
 - ⑩ R9-9 Sidewalk Closed

- LEGEND**
- TPM=Temporary Pavement Marking (Type, Color and Width as Noted)
 - Indicates Drum with Type "C" Steady Burning Light (Not a Pay Item) (Typ.)
 - Temporary Traffic Barrier, Type 2
 - * * * Indicates Line Removal
 - ▨ Construction Area

NOTES
 Signs shall be placed as shown on the Plans or as directed by the Engineer.
 All Construction Signs to have Low Intensity Flashing Yellow Light, Type "A".

BFS
 Butler Fairman Soufer
 CIVIL ENGINEERS

Headquarters
 8450 WESTFIELD BLVD., SUITE 300
 INDIANAPOLIS, IN 46240-8302
 TEL 317-713-4615
 FAX 317-713-4616
 www.BFSeng.com

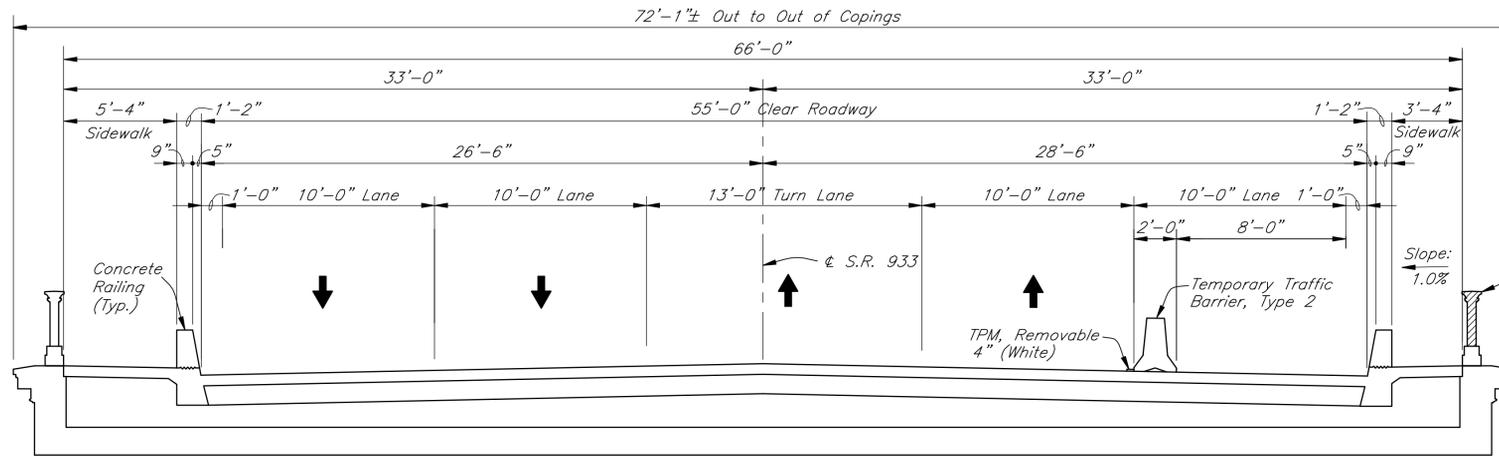
Branch Locations
 FORT WAYNE 260-459-1532
 JEFFERSONVILLE 812-286-0590
 LAFAYETTE 765-423-5602
 MERRILLVILLE 219-769-2333
 PLAINFIELD 317-839-3242

RECOMMENDED FOR APPROVAL:		DESIGN ENGINEER		DATE	
DESIGNED: D. SHEETZ	DRAWN: D. SHEETZ				
CHECKED: M. EICHENAUER	CHECKED: M. EICHENAUER				

INDIANA
 DEPARTMENT OF TRANSPORTATION

TRAFFIC MAINTENANCE DETAILS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	3 OF 27
CONTRACT	PROJECT
	BR-230-6(002)



* Remove Concrete Railing Panels (Cost to be included in the Lump Sum item "Present Structure, Remove Portions") (See Sheets 14 thru 19)

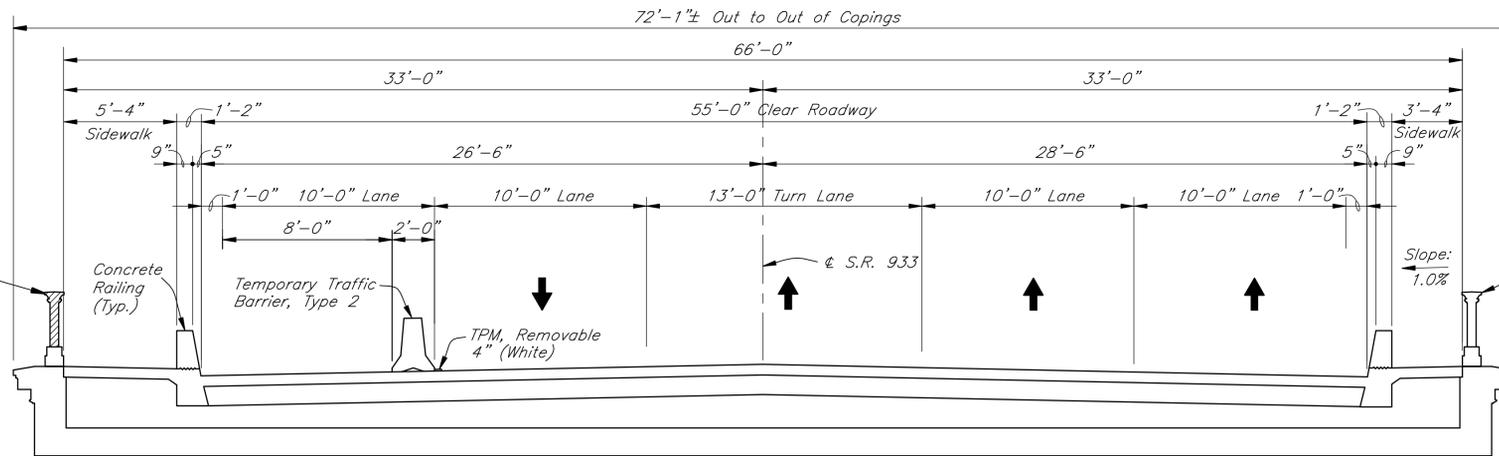
* All Major Posts are to remain in place
All Minor Posts are to have Stone Removed and Reset/Replumbed on New Mortar Bed unless noted otherwise

PHASE I

Scale: 1/4" = 1'-0"

Note: Hatched Areas Indicate Portions to be Removed.

ESTIMATED QUANTITIES		
PAY ITEM	UNIT	TOTAL
Maintaining Traffic	L.S.	1
Temporary Traffic Barrier, Type 2	LFT.	330
Temporary Traffic Barrier, Type 2, Anchored	LFT.	60
Construction Signs, Type A	EA.	20
Construction Signs, Type B	EA.	1
Construction Signs, Type C	EA.	2
Temporary Pavement Marking, Removable, 4" (White)	LFT.	1115
Temporary Pavement Marking, Removable, 4" (Broken White)	LFT.	40
Removal of Line, Paint, Broken, 4"	LFT.	61
Line, Multi-component, Broken, White, 4"	LFT.	61
Standard Barricade, Type III-A	LFT.	36
Energy Absorbing Terminal, CZ, TL-2	EA.	1



* Remove Concrete Railing Panels (Cost to be included in the Lump Sum item "Present Structure, Remove Portions") (See Sheets 14 thru 19)

* New Concrete Railing Panels (See Sheets 20 thru 26)

PHASE II

Scale: 1/4" = 1'-0"

L:\5298\ProDevelopment\Design\Drawings\5298B12.dwg Donald Sheets Plot: 12/19/2011 2:39 PM Save: 12/14/2011 2:31 PM

BFS
Butler Fairman Soufer
civil engineers

Headquarters
8450 WESTFIELD BLVD., SUITE 300
INDIANAPOLIS, IN 46240-8302
TEL 317-713-4615
FAX 317-713-4616
www.BFSeng.com

Branch Locations
FORT WAYNE 260-459-1532
JEFFERSONVILLE 812-286-0590
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

RECOMMENDED FOR APPROVAL: _____
DESIGN ENGINEER DATE

DESIGNED: S. WEINTRAUT DRAWN: D. SHEETZ

CHECKED: M. EICHENAUER CHECKED: M. EICHENAUER

INDIANA DEPARTMENT OF TRANSPORTATION

TRAFFIC MAINTENANCE DETAILS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	4 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

STRUCTURE IS BUILT ON 5-100' VERTICAL CURVES

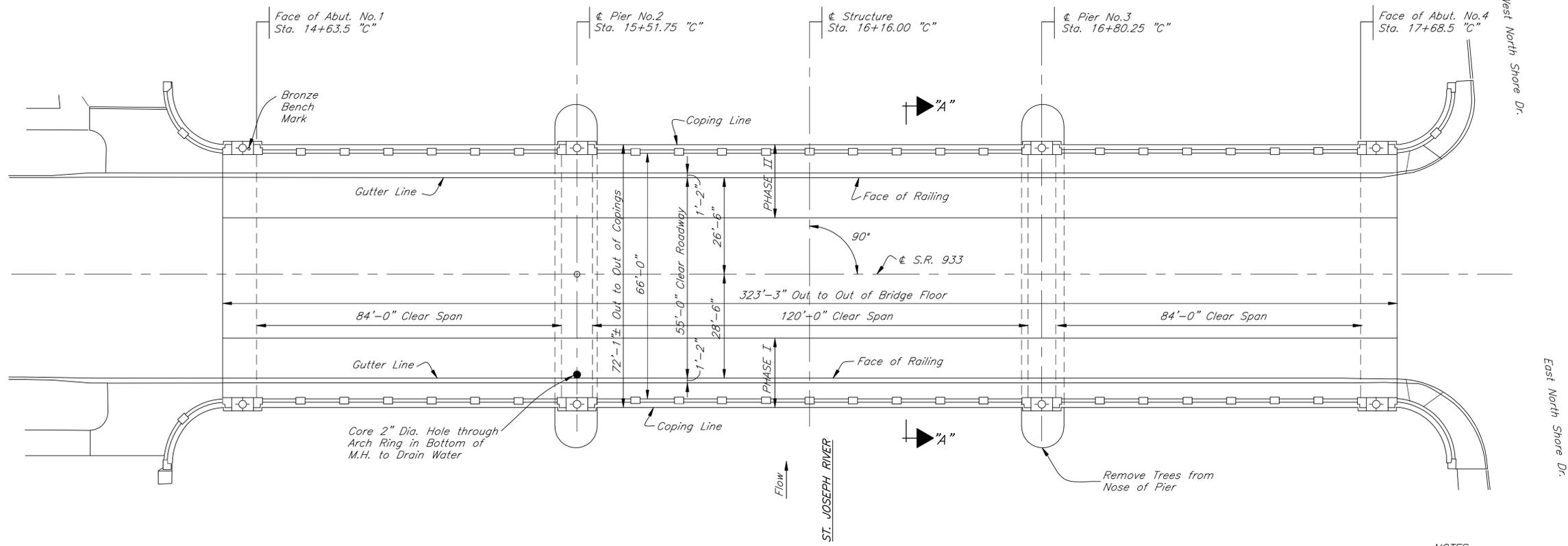
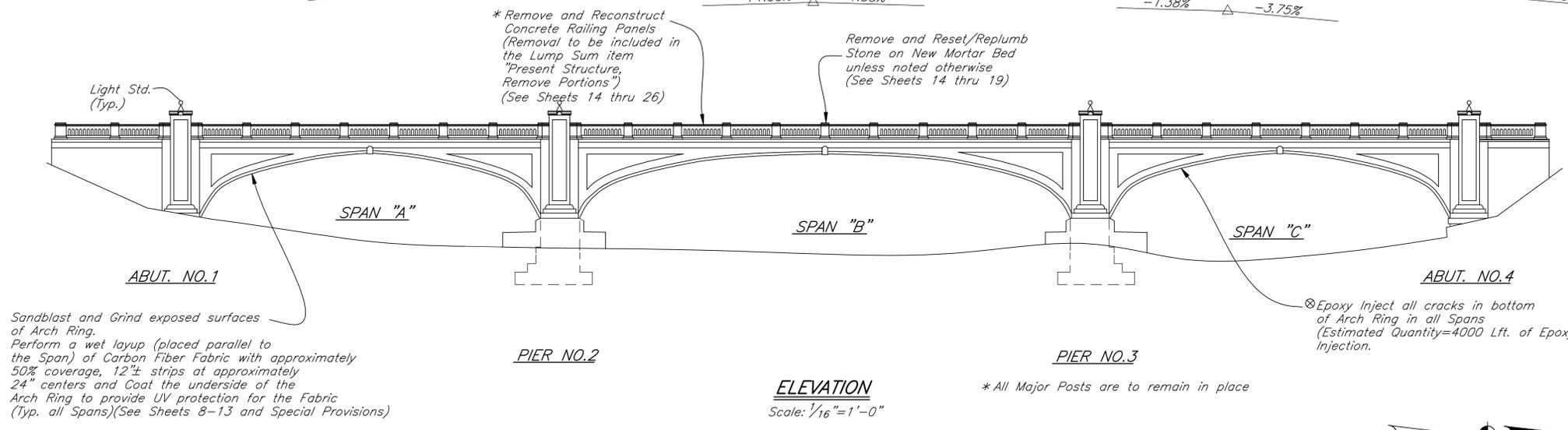
P.I. Sta. 14+16.00 "C"
El. 683.62
V.C.=100'
+2.45% +4.40%

P.I. Sta. 15+16.00 "C"
El. 688.02
V.C.=100'
+4.40% +1.38%

P.I. Sta. 16+16.00 "C"
El. 689.40
V.C.=100'
+1.38% -1.38%

P.I. Sta. 17+16.00 "C"
El. 688.02
V.C.=100'
-1.38% -3.75%

P.I. Sta. 18+16.00
El. 684.27
V.C.=100'
-3.75% -1.80%



PLAN
Scale: 1/16"=1'-0"

NOTES
See Sheet 6 for Section "A-A" and General Notes.
See Sheet 7 for Spandrel Wall Repairs.

REINFORCED CONCRETE ARCH BRIDGE
3 SPANS: 84'-0", 120'-0" AND 84'-0", NO SKEW, 55'-0" CLEAR ROADWAY ON STATE ROAD 933 OVER ST. JOSEPH RIVER.

L: 152981 Pro Development | Design | Drawings | 15298821.dwg Donald Sheetz Plot: 12/19/2011 2:39 PM Save: 12/15/2011 7:45 AM

BFS
Butler Fairman Soufer
civil engineers

Headquarters
8450 WESTFIELD BLVD., SUITE 300
INDIANAPOLIS, IN 46240-8302
TEL 317-713-4615
FAX 317-713-4616
www.BFSeng.com

Branch Locations
FORT WAYNE 260-459-1532
JEFFERSONVILLE 812-286-0590
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER	DATE
DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ	
CHECKED:	CHECKED:	

INDIANA
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	5 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

GENERAL NOTES

Plans for the structure as originally built are not available.
 Rehabilitation Plans, Contract B-27194 are available upon request from INDOT.

Inspection Reports completed in 2002 and 2011 are available upon request from INDOT.

Portions of present structure shall be removed.

Where new work is to be fitted to old work, the Contractor shall check all dimensions and conditions in the field and report any errors or discrepancies to the Engineer and assume responsibility for their correctness and the fit of the new part to the old.

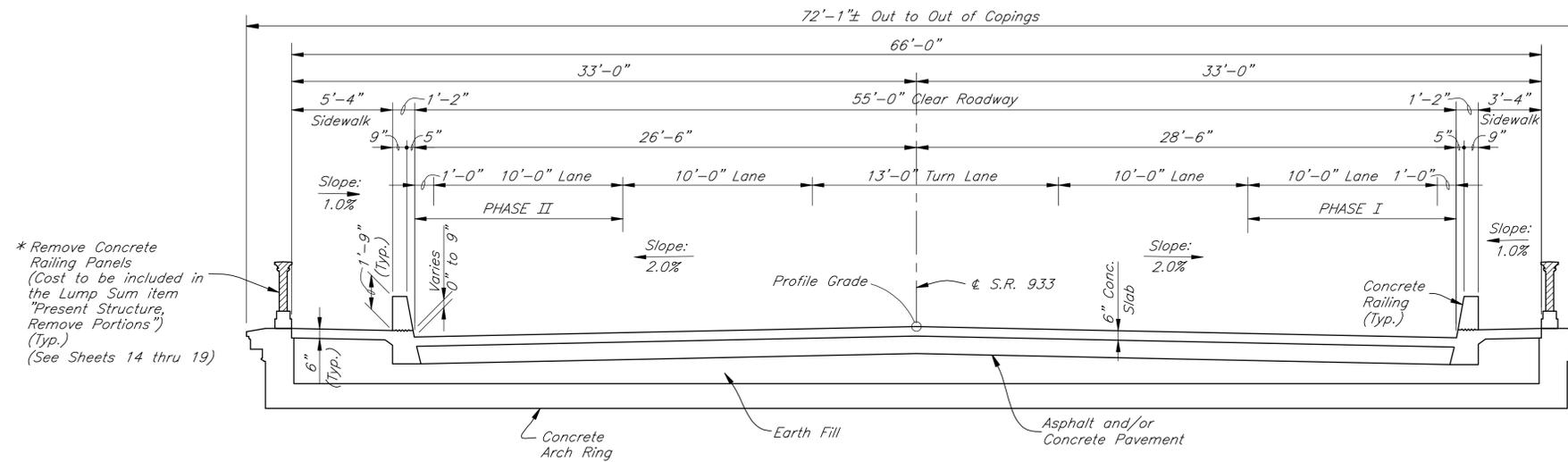
Epoxy coated reinforcing steel shall be required in various portions of the structure as shown.

Reinforcing steel covering shall be 2" in all areas unless noted.

Reinforcing steel shall be A.S.T.M. A615, Grade 60.

Concrete shall be Class C in railings.

Chamfer exposed corners of concrete 1/2" unless noted.



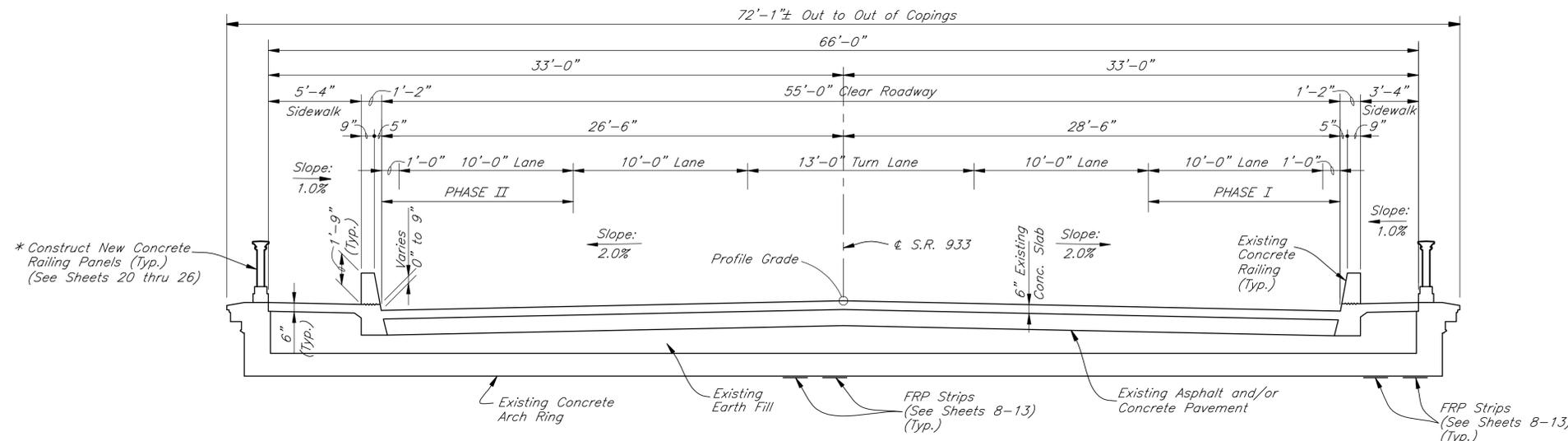
* Remove Concrete Railing Panels (Cost to be included in the Lump Sum item "Present Structure, Remove Portions") (Typ.) (See Sheets 14 thru 19)

SECTION "A-A"
EXISTING

Note: Hatched Areas Indicate Portions to be Removed.

Scale: 1/4" = 1'-0"

* All Major Posts are to remain in place
 All Minor Posts are to have Stone Removed and Reset/Replumbed on New Mortar Bed unless noted otherwise



* Construct New Concrete Railing Panels (Typ.) (See Sheets 20 thru 26)

SECTION "A-A"
PROPOSED

Scale: 1/4" = 1'-0"

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CIVIL ENGINEERS

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Branch Locations
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JEFFERSONVILLE 812-286-0590
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

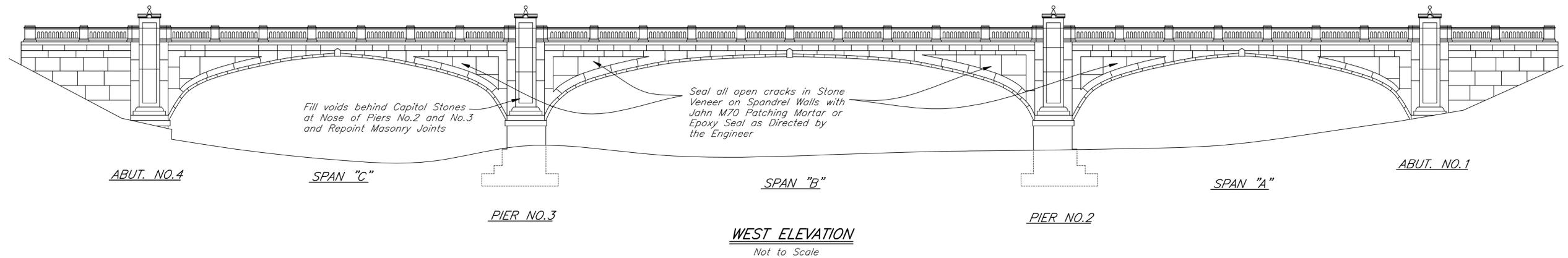
RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER	DATE
DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ	
CHECKED:	CHECKED:	

INDIANA
DEPARTMENT OF TRANSPORTATION

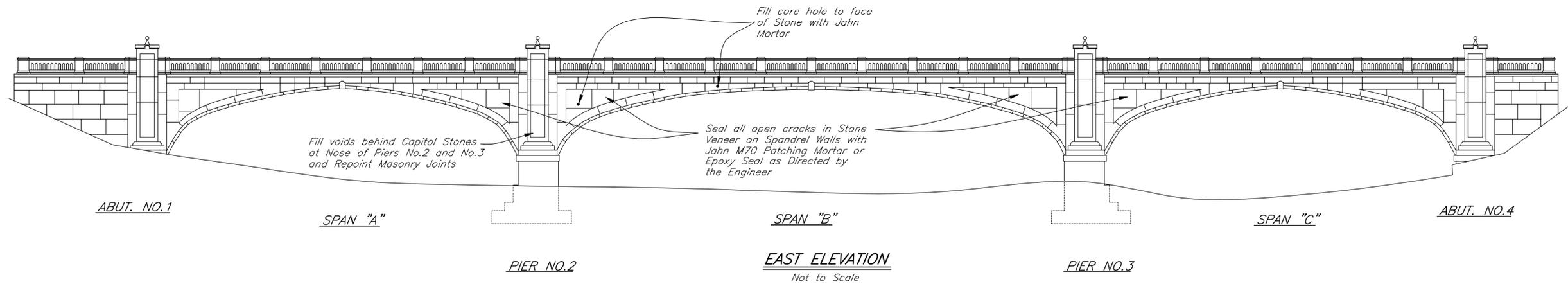
GENERAL PLAN

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	6 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298



Note: Repoint Masonry Joints on Spandrel Walls as required by the Engineer.



L:\5298\ProDevelopment\Drawings\Drawings\5298B71.dwg Donald Sheetz Plot: 12/19/2011 2:39 PM Save: 12/19/2011 2:12 PM

BFS
Butler Fairman Soufer
civil engineers

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TEL 317-713-4615
FAX 317-713-4616
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Branch Locations
FORT WAYNE 260-459-1532
JEFFERSONVILLE 812-285-0590
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER	DATE
DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ	
CHECKED:	CHECKED:	

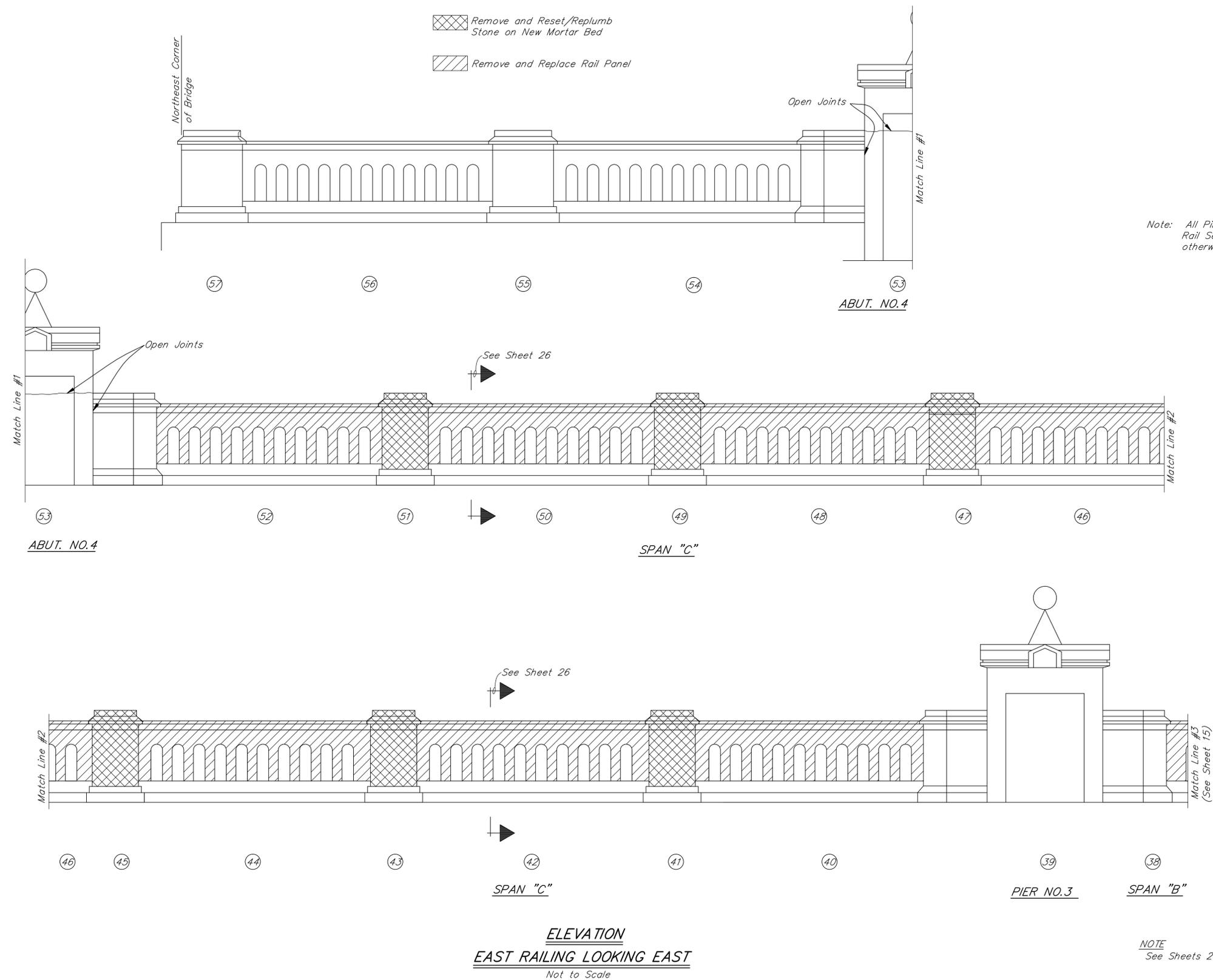
INDIANA
DEPARTMENT OF TRANSPORTATION

MISCELLANEOUS DETAILS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	7 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

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ELEVATION
EAST RAILING LOOKING EAST
 Not to Scale

NOTE
 See Sheets 20 thru 25 for Reconstruction Details.

BFS
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civil engineers

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 INDIANAPOLIS, IN 46240-8302
 TEL 317-713-4615
 FAX 317-713-4616
 www.BFSeng.com

Branch Locations
 FORT WAYNE 260-459-1532
 JEFFERSONVILLE 812-285-0590
 LAFAYETTE 765-423-5602
 MERRILLVILLE 219-769-2333
 PLAINFIELD 317-839-3242

RECOMMENDED FOR APPROVAL:	
DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ
CHECKED: _____	CHECKED: _____

INDIANA
 DEPARTMENT OF TRANSPORTATION
RAILING DETAILS-PHASE I
SHOWING REMOVALS AND
RAILING POST REHABILITATION

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	14 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

Appendix C

Early Coordination



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

LaPorte District
315 East Boyd Boulevard, P.O. Box 429
LaPorte, Indiana 46350, 46352 (219) 362-6125 FAX: (219) 325-7516

Mitchell E. Daniels, Jr., Governor
Michael Reed, Commissioner

December 9, 2011

Re: Des. No. 1173149
SR 933, over St. Joseph River, 1.59 miles North of SR 23
Bridge Rehabilitation

To Whom It May Concern:

The Indiana Department of Transportation intends to proceed with the aforementioned project. This letter is part of the early coordination phase of the environmental review process, in which we are requesting comments from your area of expertise regarding any possible environmental effects associated with this project. **Please use the above designation number and description in your reply.** We will incorporate your comments into a study of this project's environmental impacts.

This project is located within the limits of the city of South Bend on SR 933 over the St. Joseph River, 1.59 miles north of SR 23 in Section 2, Township 37 North, Range 2 East on the U.S.G.S. South Bend West Quadrangle all within Portage Township, St. Joseph County, Indiana. The project is located within the Indiana Department of Transportation's (INDOT's) La Porte District and lies within the Michiana Area Council of Governments (MACOG) Metropolitan Planning Organization (MPO).

This structure, a three span filled-spandrel arch bridge, number 933-31-71-03690-B, was constructed in 1914 and rehabilitated in 1977 and again in 2006. The existing SR 933 Bridge over the St. Joseph River built with a 0° skew with clear bridge width at 55 ft and total bridge width at 72 ft. The travel way on the bridge consists of four 10 ft lanes and two 2 ft shoulders. This section of SR 933 is functionally classified as an "Urban-Principal Arterial" route and is not on the National Highway System. The current land use in the vicinity of the project is residential to the north, and a public park to the south of the structure.

The need for this project is based on deterioration of the structure. The railing on the east side of the bridge is cracked, and it is more pronounced in the center span. The railing on the east side is also cracked. Several sections of railing are leaning. The concrete deck slab that was placed on the bridge as part of a previous rehabilitation has numerous longitudinal and transverse cracks. The underside of the arch ring is cracked, but no seepage or delamination was present around the cracks. The purpose of this project is to correct the above deficiencies.

Two alternatives are being considered, the do nothing and the rehabilitation alternative. The preferred alternative is currently the rehabilitation alternative. The preferred alternative would entail removing and replacing several of the bridge railings, removing and reinstalling all post stones (column and capital stone), and sealing the cracks on many of the posts. The cracks in the concrete pavement would be sealed. Epoxy would be injected into the cracks in the bottom of the arch rings, and the exposed surface would be sandblasted in

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preparation for the application of carbon fiber reinforcement. No approach work is involved with this alternative.

The preferred alternative will not require the acquisition of new permanent right-of-way and no temporary right-of-way will be required. No changes in alignment will take place as a result of this project. No in stream work is anticipated. A wetland and waters determination will be completed to identify water resources within the project area. The INDOT Cultural Resources section will investigate the area for historic resources for compliance with Section 106.

Please respond with your comments on any environmental impacts that may be incurred as a result of this project so that an environmental report can be prepared. We also welcome your related opinions and other input for consideration in the preparation of the environmental document. If we do not receive your response within **thirty (30) days**, it will be assumed that your agency or organization feels that there will be no significant effects as a result of this project or you do not wish to offer opinions concerning this project. However, should you find that an extension to respond is required; a reasonable amount of time will be granted upon request. If you have any questions regarding this matter, please contact Travis Mast at (219) 325-7509.

Thank you in advance for your input.

Attachments: Aerial Maps
T topographic Map
 Photographs

Sincerely,

Travis Mast
Environmental Scientist
LaPorte District, INDOT



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

LaPorte District
315 East Boyd Boulevard, P.O. Box 429
LaPorte, Indiana 46350, 46352 (219) 362-6125 FAX: (219) 325-7516

Mitchell E. Daniels, Jr., Governor
Michael B. Cline, Commissioner

The following agencies received Early Coordination Letters:

U.S. Fish and Wildlife Service
P.O. Box 2616
Chesterton, IN 46304

Field Environmental Officer
Chicago Regional Office
US Department of Housing & Urban
Development
Metcalf Fed. Bldg.
77 W. Jackson Blvd. RM 2401
Chicago, IL 60604

Regional Environmental Coordinator
Midwest Regional Office
National Park Service
601 Riverfront Drive
Omaha, Nebraska 68102

Federal Highway Administration
Federal Office Building Room 254
575 North Pennsylvania Street
Indianapolis, Indiana 46204

MACOG Executive Director
1120 County-City Building
227 West Jefferson Blvd
South Bend, IN 46601

Manager, Public Hearings
Indiana Department of Transportation
100 N. Senate Avenue
Indianapolis, IN 46204

Manager - Aviation Section
Indiana Department of Transportation
Room N901, IGC North
100 North Senate Avenue
Indianapolis, Indiana 46204
(Attachment)

State Conservationist
Natural Resource Conservation Service
6013 Lakeside Boulevard
Indianapolis, Indiana 46278
(Attachment)

Environmental Coordinator
Indiana Department of Natural Resources
Division of Fish and Wildlife
Room W264, IGC South
402 West Washington Street
Indianapolis, IN 46204
environmentalreview@dnr.in.gov

Section Head
Environmental Geology Section
(Attachment)
email
IGSenvir@indiana.edu

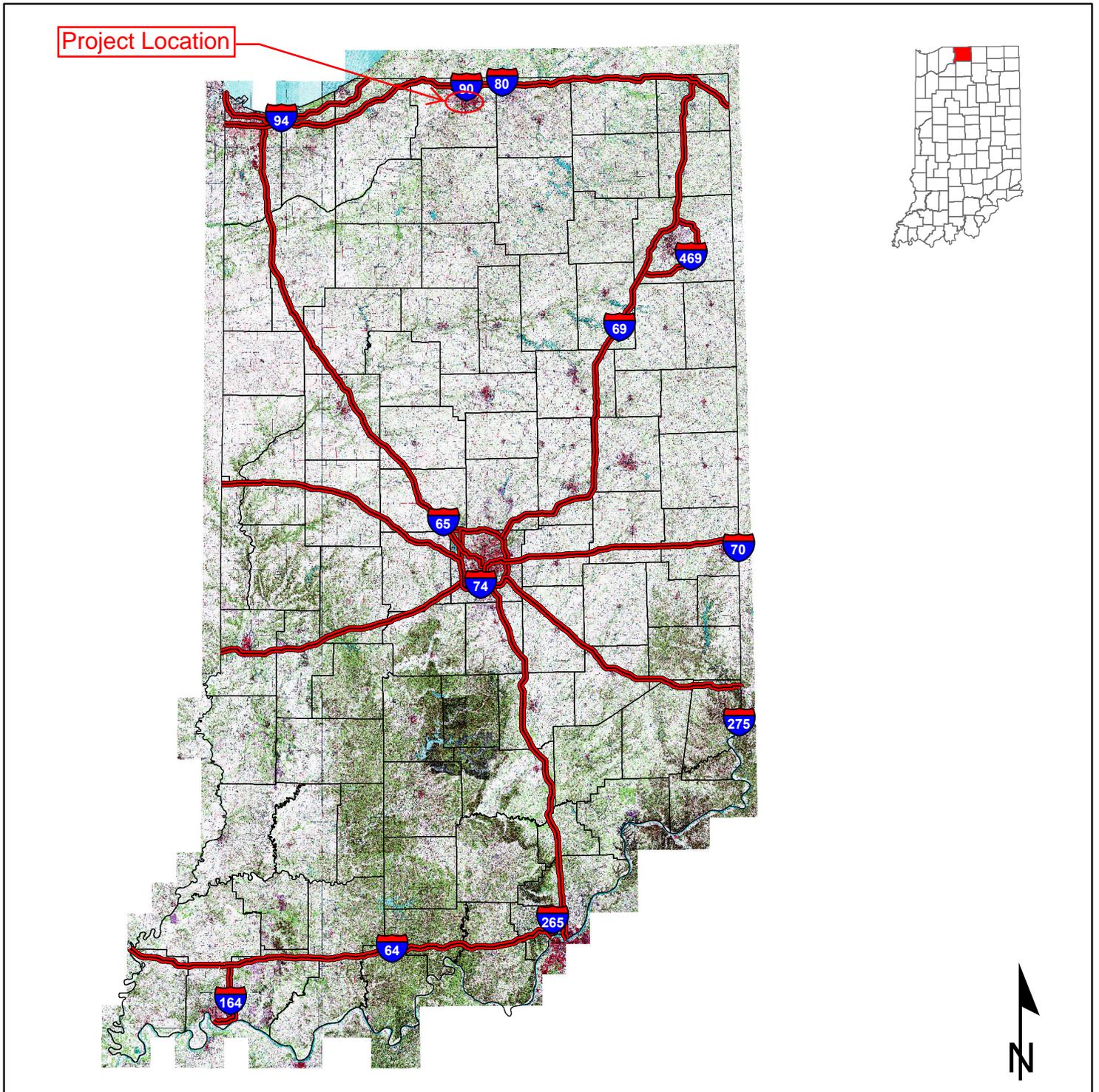
Indiana Department of Environmental
Management
Automatic Website Early Coordination
http://www.in.gov/idem/enviroreview/hwy_earlyenviroreview.html

Chief, Groundwater Section
Indiana Department of Environmental
Management
100 N. Senate Avenue
Indianapolis, IN 46204
(Attachment)
jsulliva@idem.in.gov

Early Coordination - State Map

SR 933, St. Joseph County, Indiana

Des No 1173149, Bridge Rehabilitation



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

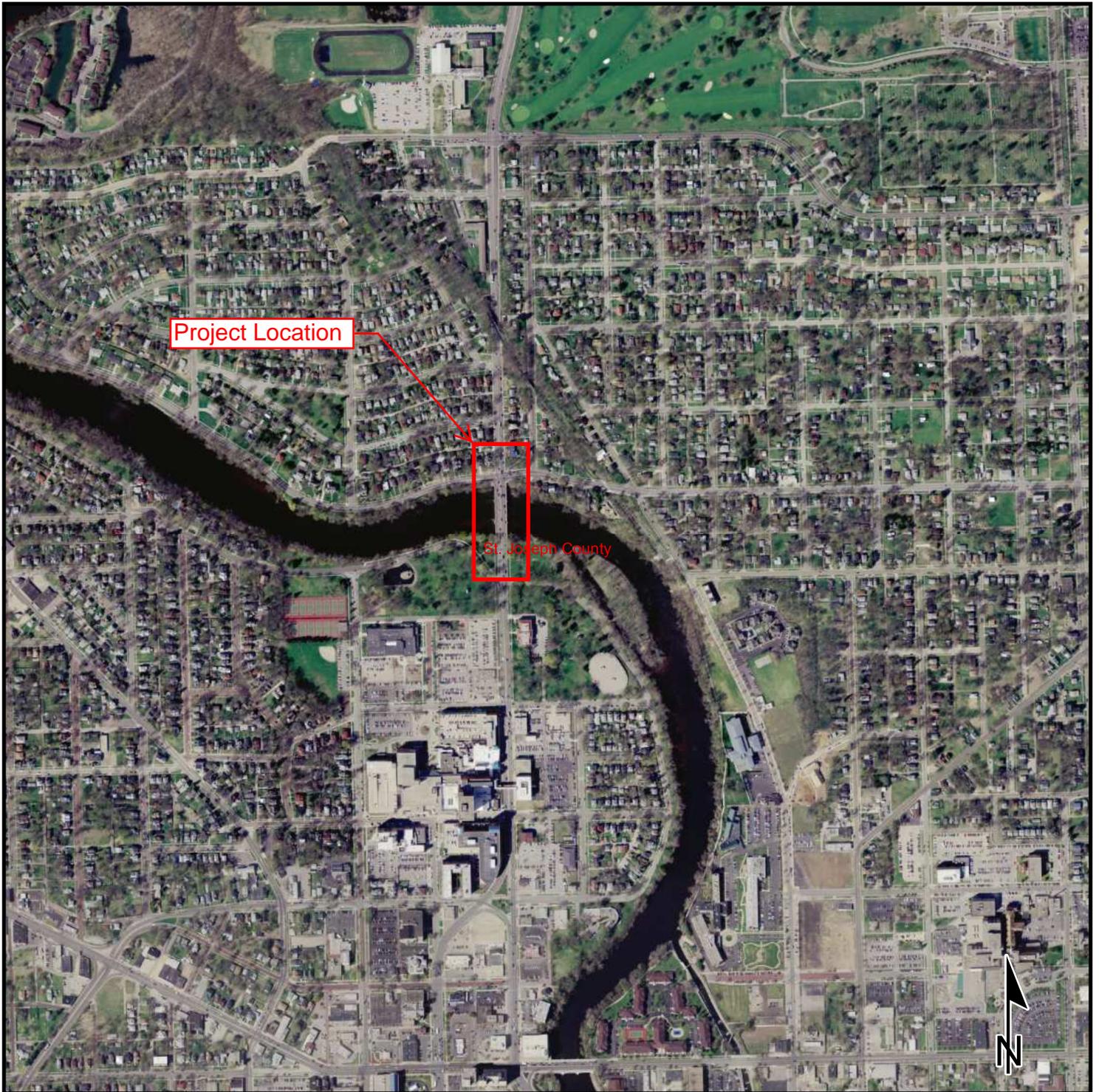
Scale 1:2,500,000 Miles



Early Coordination - Aerial Map

SR 933, St. Joseph County, Indiana

Des No 1173149, Bridge Rehabilitation



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:10,000 Miles

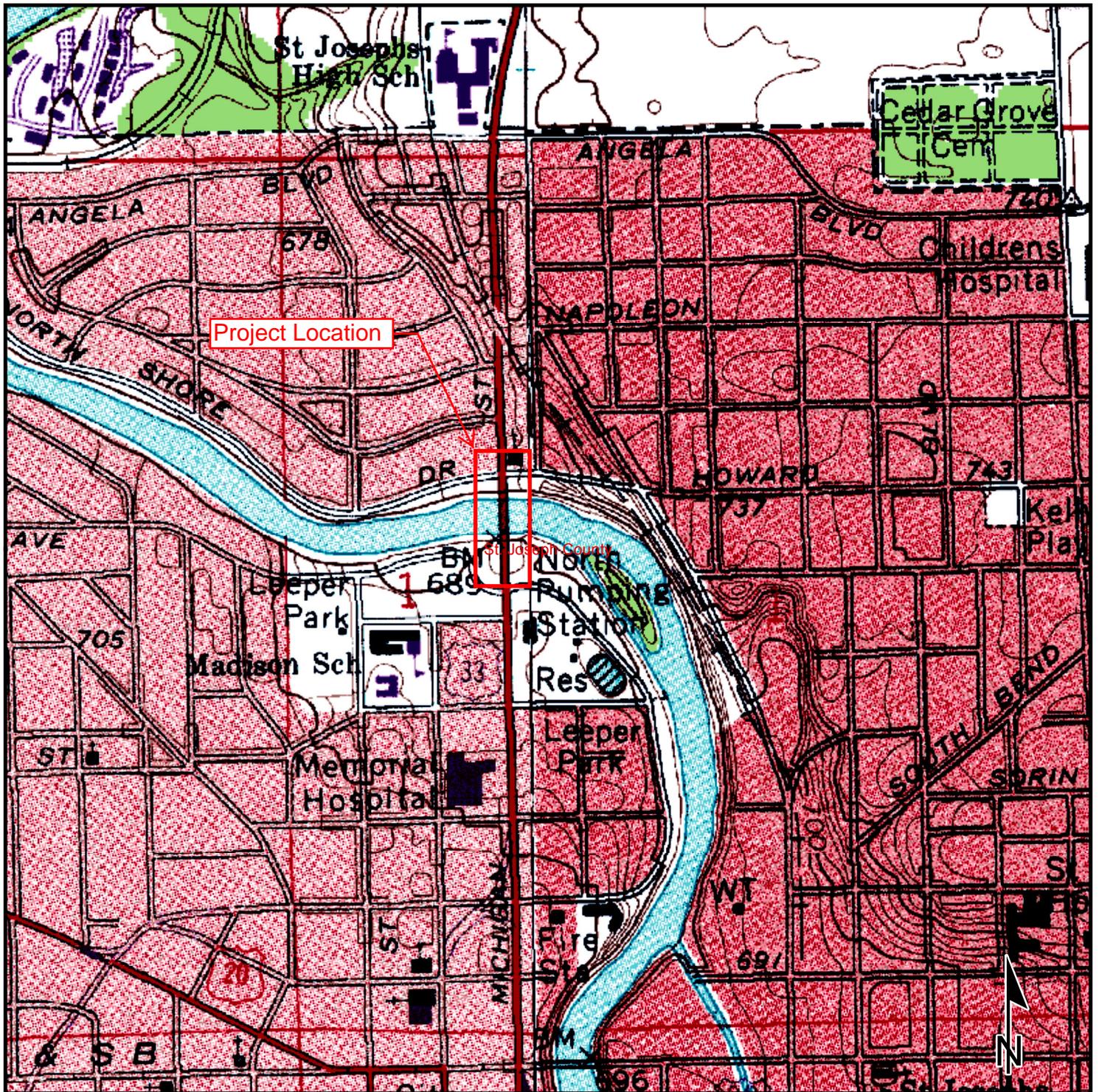
	Section Town and Range		Interstate
	County Boundary		State Route
	Local Road		US Route

C-5

Early Coordination - Topo Map

SR 933, St. Joseph County, Indiana

Des No 1173149, Bridge Rehabilitation



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

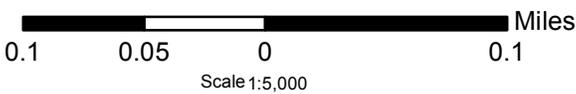
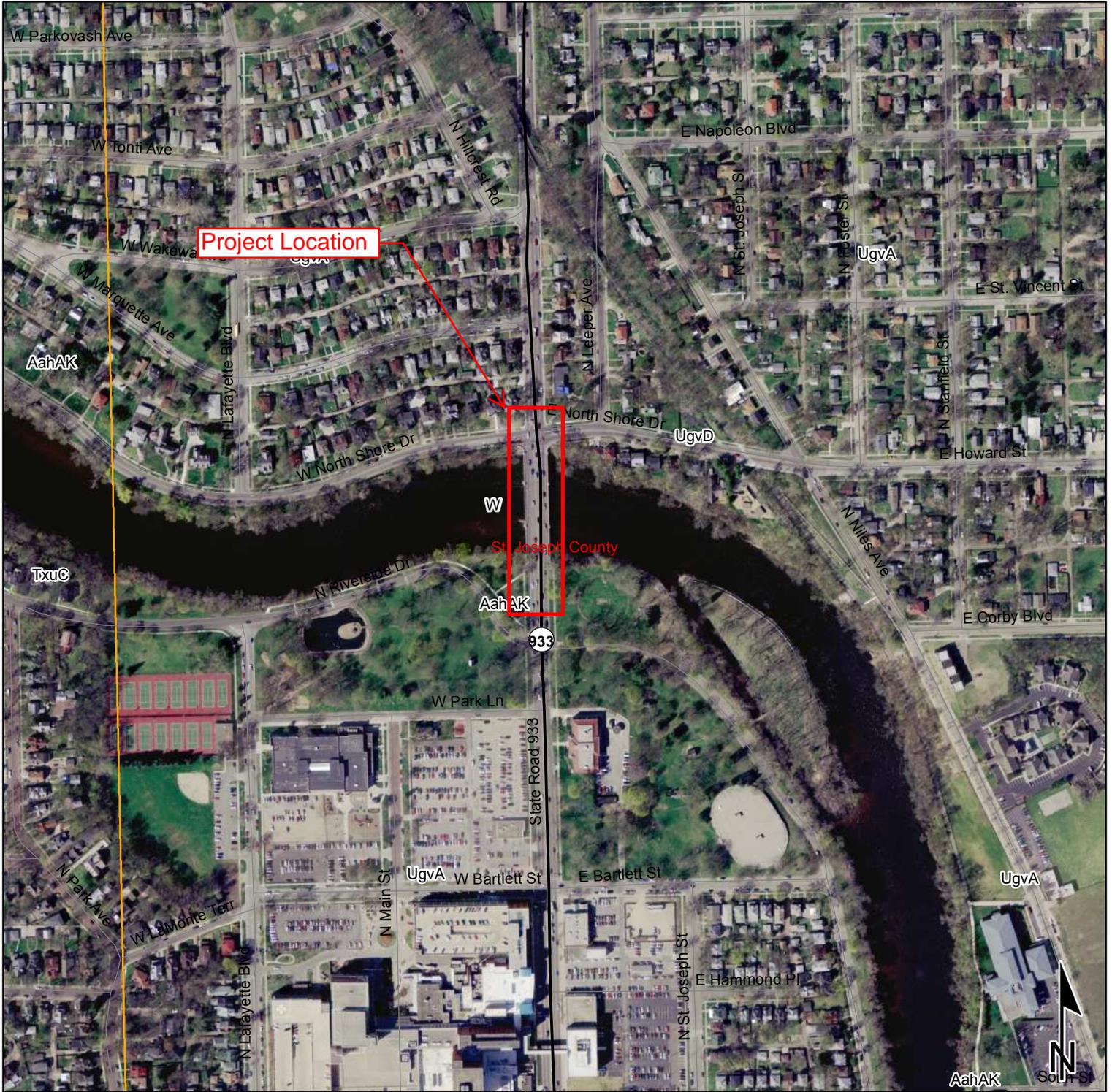
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:10,000 Miles



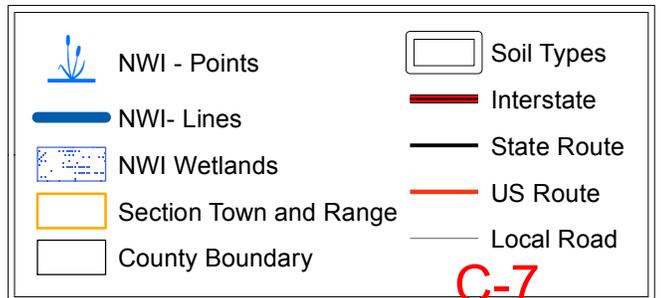
C-6

Early Coordination - Hydric Soils SR 933, St. Joseph County, Indiana Des No 1173149, Bridge Rehabilitation



Sources: **Non Orthophotography**
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

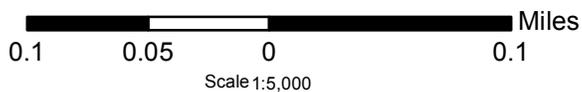
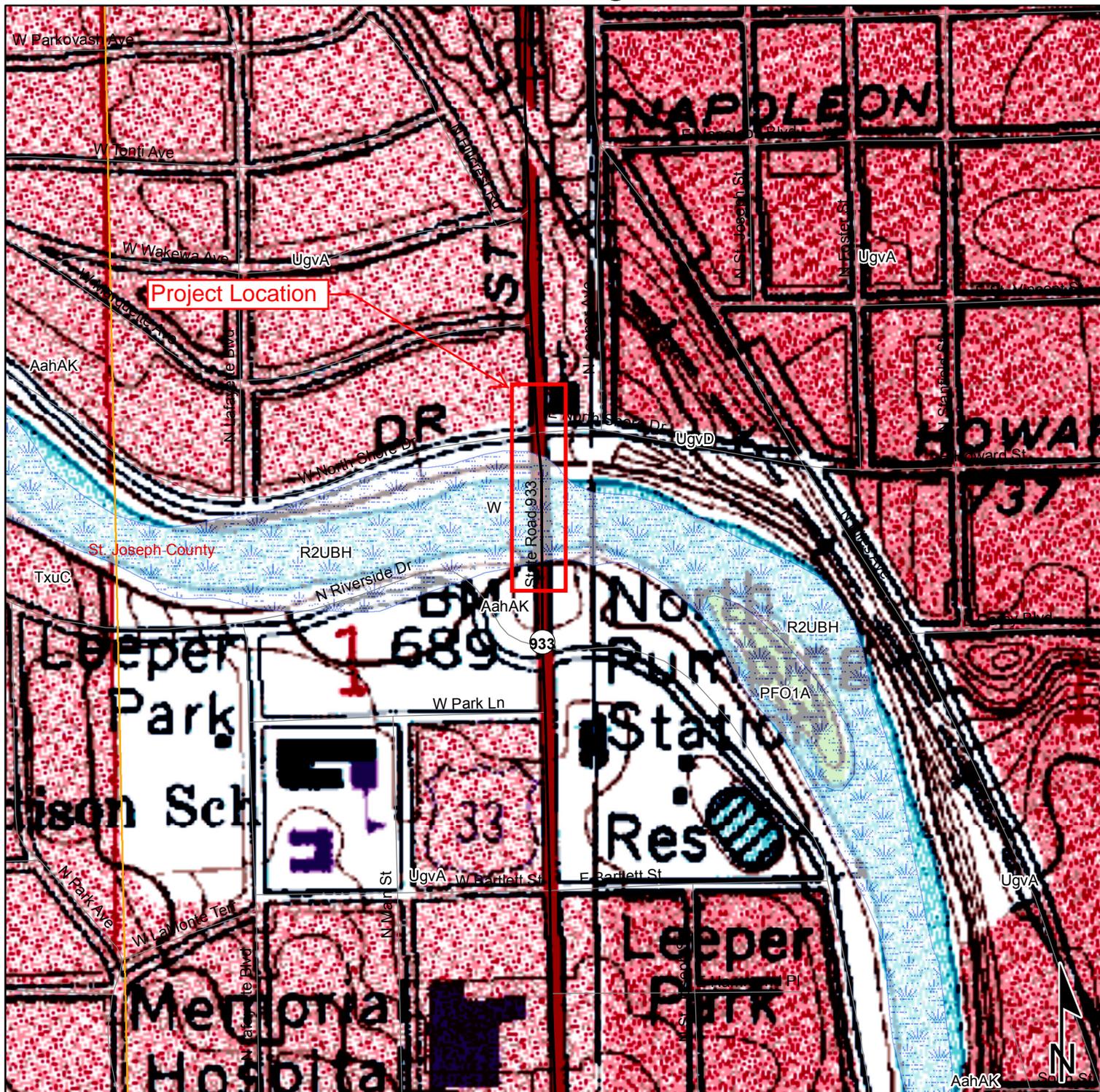
This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Early Coordination - NWI

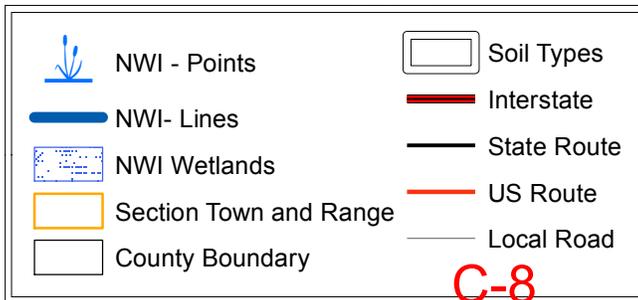
SR 933, St. Joseph County, Indiana

Des No 1173149, Bridge Rehabilitation



Sources: **Non Orthophotography**
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Early Coordination - Photographs
SR 933, St. Joseph County, Indiana
Des No 1173149, Bridge Rehabilitation



Picture 1: SR 933 Bridge looking northwest.



Picture 2: SR 933 Bridge looking north.



Picture 3: SR 933 Bridge looking south.



Picture 4: St. Joseph River looking east from SR 933 Bridge.



Picture 5: St. Joseph River looking west from SR 933 Bridge.



United States Department of the Interior

Fish and Wildlife Service



Bloomington Field Office (ES)
620 South Walker Street
Bloomington, IN 47403-2121
Phone: (812) 334-4261 Fax: (812) 334-4273

December 9, 2011

Ms. Kristi D. Hamilton
Butler, Fairman and Seufert, Inc.
8450 Westfield Boulevard, Suite 300
Indianapolis, Indiana 46240-5920

Project No.: Des. 1173149
Project: SR 933/Michigan Street Bridge – Likely Rehabilitation of Historic Bridge
Location: South Bend, St. Joseph County

Dear Ms. Hamilton:

This responds to your letter dated November 30, 2011, requesting our comments on the aforementioned project.

These comments have been prepared under the authority of the Fish and Wildlife Coordination Act (16 U.S.C. 661 et. seq.) and are consistent with the intent of the National Environmental Policy Act of 1969, the Endangered Species Act of 1973, and the U. S. Fish and Wildlife Service's Mitigation Policy.

The proposed project will have no effect on wetlands or other significant habitat types. Project impacts are expected to be minor in nature. Based on a review of the information you provided, the U.S. Fish and Wildlife Service has no objections to the project as currently proposed. This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act of 1973, as amended. However, should new information arise pertaining to project plans or a revised list be published, it will be necessary for the Federal agency to reinitiate consultation.

We appreciate the opportunity to comment at this early stage of project planning. If project plans change such that fish and wildlife habitat may be affected, please recoordinate with our office as soon as possible. If you have any questions, please contact Elizabeth McCloskey at (219) 983-9753 or elizabeth_mccloskey@fws.gov.

Sincerely yours,

for Scott E. Pruitt
Supervisor

cc: Federal Highway Administration, Indianapolis, IN
Christie Stanifer, Indiana Division of Water, Indianapolis, IN

December 27, 2011

Travis Mast
Environmental Manager
INDOT LaPorte District
315 East Boyd Blvd.
P. O. Box 429
LaPorte, IN 46350

RE: *Early Coordination Review of DES. NO. 1173149—SR 933, Over St. Joseph River; 1.59 miles North of SR 23, St. Joseph County, IN*

Dear Mr. Mast:

The Michiana Area Council of Governments has reviewed the above-mentioned project under the Early Coordination process. The preferred project activities consist of removing and replacing several of the bridge railings, removing and reinstalling all post stones (column and capital stone), and sealing the cracks on most of the posts. The cracks in the concrete pavement would be sealed. Epoxy would be injected into the cracks in the bottom of the arch rings, and the exposed surface would be sandblasted in preparation for the application of carbon fiber reinforcement.

This project is located over the St. Joseph River, Lake Michigan Basin, which is designated a salmonid stream within this reach. For this reason, all precautions should be in place preventing any contaminants or construction residues from entering the river.

Activities in the river, including equipment staging, may require permits from the Indiana Department of Natural Resources because of the designation. Activities during spawning season in salmonid streams are restricted, with specific dates becoming a part of the permit. Equipment staged in-stream for a long period of time should be placed such as to not deflect water energy and exacerbate erosion or bank sloughing at the construction location.

Should the Indiana Department of Transportation discover the need to reinforce the bridge support structures, it is recommended that washed river rock be used rather than limestone rip-rap to absorb the river energy against the bridge supports.

If you have any further questions, please contact me at 574-287-1829 or sseanor@macog.com.

Sincerely,

Sandra M. Seanor
Executive Director

F:\ABC\MPO\WORKPLAN\2012\w211tiptp\INDOT\Early LRP Coord Maps\LaPorte LRP Dist Maps\L12km1tmast.docx

Should the applicant need to obtain any environmental permits in association with this proposed project, please be mindful that IC 13-15-8 requires that they notify all adjoining property owners and/or occupants within ten days of your submittal of each permit application. Applicants seeking multiple permits, may still meet the notification requirement with a single notice if all required permit applications are submitted with the same ten day period.

Please note that this letter does not constitutes a permit, license, endorsement, or any other form of approval on the part of either the Indiana Department of Environmental Management or any other Indiana state agency.

Should you have any questions relating to the content or recommendations of this letter, or if you have additional questions about whether a more complete environmental review of your project should be conducted, please feel free to contact Brad Baughn at (317) 234-3386, BBAughn@idem.in.gov.

Sincerely,



Thomas W. Easterly
Commissioner

Signature(s) of the Applicant

I acknowledge that I am seeking grant monies, a bond issuance, or other public funding mechanism to cover some portion of the cost of the public works, infrastructure, or community development project as described herein, which I am working (possibly with others) to complete.

Project Description

Two alternatives are being considered, the do nothing and the rehabilitation alternative. The preferred alternative is currently the rehabilitation alternative. The preferred alternative would entail removing and replacing several of the bridge railings, removing and reinstalling all post stones (column and capital stone), and sealing the cracks on many of the posts. The cracks in the concrete pavement would be sealed. Epoxy would be injected into the cracks in the bottom of the arch rings, and the exposed surface would be sandblasted in preparation for the application of carbon fiber reinforcement. No approach work is involved with this alternative. The preferred alternative will not require the acquisition of new permanent right-of-way and no temporary right-of-way will be required. No changes in alignment will take place as a result of this project. No in stream work is anticipated. A wetland and waters determination will be completed to identify water resources within the project area. The INDOT Cultural Resources section will investigate the area for historic resources for compliance with Section 106.

With my signature, I do hereby affirm that I have read the letter from the Indiana Department of Environmental Management that appears directly above. In addition, I understand that in order to complete the project in which I am interested, with a minimum impact to the environment, I must consider all the issues addressed in the aforementioned letter, and further, that I must obtain any required permits.

Dated Signature of the Public Owner
Contact/Responsible Elected Official _____

Dated Signature of the Project
Planner/Consultant Contact Person  12-9-2011

Travis Mast



INDIANA DEPARTMENT OF ENVIRONMENTAL MANAGEMENT

We Protect Hoosiers and Our Environment.

Mitchell E. Daniels Jr.
Governor

Thomas W. Easterly
Commissioner

100 North Senate Avenue
Indianapolis, Indiana 46204
(317) 232-8603
Toll Free (800) 451-6027
www.idem.IN.gov

December 12, 2011

66-33
Mr. Travis Mast
INDOT
315 East Boyd Boulevard
LaPorte, Indiana 46350

Dear Mr. Mast:

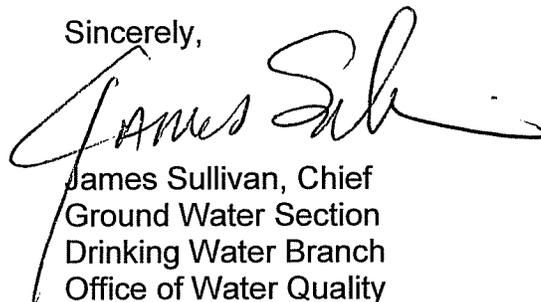
RE: Wellhead Protection Area Proximity
Determination
State Road 933 Over St. Joseph River,
South Bend, Indiana, St. Joseph County

Upon review of the above referenced site, it has been determined that the site is **located** within a Wellhead Protection Area. Be aware that this project is within the St. Joseph Aquifer System, an EPA designated sole source aquifer system. Contact Bill Spaulding at Spaulding.William@epamail.epa.gov for more information/guidance.

This information is accurate to the best of our knowledge. However, there are in some cases, a few factors that could impact the accuracy of this determination. For example, some Wellhead Protection Area Delineations have not been submitted or many not have been approved by this office. In these cases, we use a 3,000 foot fixed radius buffer to make the proximity determination. To find the status of a Public Water Supply System's Wellhead Protection Area Delineation, please visit our tracking database at <http://www.in.gov/idem/4289.htm>.

If you have any additional questions, please feel free to contact me at the address above or at (317) 234-7476.

Sincerely,



James Sullivan, Chief
Ground Water Section
Drinking Water Branch
Office of Water Quality

JS:gml



UNITED STATES ENVIRONMENTAL PROTECTION AGENCY

REGION 5

77 WEST JACKSON BOULEVARD
CHICAGO, IL 60604-3590

December 12, 2011

Travis Mast
IN DOT LaPorte District
315 East Boyd Boulevard
LaPorte, IN 46350

Re: Sole Source Aquifer Review
SR 933 Bridge Rehabilitation – Des. No: 1173149
St. Joseph County, IN

Dear Mr. Mast:

I have reviewed the information you sent regarding the above referenced project. As described, the SR 933 Bridge Rehabilitation project mentioned does not pose substantial threats to the St. Joseph Sole Source Aquifer System, a Sole Source Aquifer designated under the authority of the Safe Drinking Water Act, Section 1424(e). Adequate design and monitoring plans should be followed to ensure the protection of the aquifer. Unless future developments change the status of the proposal, no modifications or further review under the Sole Source Aquifer Program should be necessary.

As always, we suggest that during construction and operation appropriate safeguards and best management practices for storm water are in place to ensure that ground water is not endangered. Such precautions would include notifying general contractors that the site is sensitive, securing adequate precautions for fueling/servicing large equipment, and developing contingency plans to handle the release of any hazardous materials.

Thank you for your cooperation. If you have any further questions please call me at (312) 886-9262.

Sincerely,

A handwritten signature in black ink, appearing to read "William Spaulding".

William Spaulding
Ground Water and Drinking Water Branch

United States Department of Agriculture



Natural Resources Conservation Service
6013 Lakeside Blvd.
Indianapolis, IN 46278

December 28, 2011

Des 1173149

Travis Mast
Environmental Scientist
Indiana Department of Transportation
LaPorte District
315 East Boyd Blvd.
P.O. Box 429
LaPorte, IN 46350

Dear Mr. Mast:

The proposed project to rehabilitate the bridge on SR 933 over St. Joseph River in St. Joseph County, Indiana as stated in your letter received December 14, 2011, will not cause a conversion of prime farmland.

If you need additional information, please contact Lisa Bolton at 317-290-3200, extension 342.

Sincerely,

Michael A. Cox

JANE E. HARDISTY
State Conservationist

Enclosures

FARMLAND CONVERSION IMPACT RATING
FOR CORRIDOR TYPE PROJECTS

PART I (To be completed by Federal Agency)		3. Date of Land Evaluation Request 12/9/11	4. Sheet 1 of _____
1. Name of Project SR 933 Bridge Rehabilitation		5. Federal Agency Involved FHWA and INDOT	
2. Type of Project		6. County and State St. Joseph County, Indiana	
PART II (To be completed by NRCS)		1. Date Request Received by NRCS 12-14-11	2. Person Completing Form Lisa Bolten
3. Does the corridor contain prime, unique statewide or local important farmland? (If no, the FPPA does not apply - Do not complete additional parts of this form). YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>		4. Acres Irrigated Average Farm Size	
5. Major Crop(s)	6. Farmable Land in Government Jurisdiction Acres: _____ %	7. Amount of Farmland As Defined in FPPA Acres: _____ %	
8. Name Of Land Evaluation System Used	9. Name of Local Site Assessment System	10. Date Land Evaluation Returned by NRCS 12-28-11	

PART III (To be completed by Federal Agency)	Alternative Corridor For Segment _____			
	Corridor A	Corridor B	Corridor C	Corridor D
A. Total Acres To Be Converted Directly	0			
B. Total Acres To Be Converted Indirectly, Or To Receive Services	0			
C. Total Acres In Corridor	0	0	0	0

PART IV (To be completed by NRCS) Land Evaluation Information
A. Total Acres Prime And Unique Farmland
B. Total Acres Statewide And Local Important Farmland
C. Percentage Of Farmland in County Or Local Govt. Unit To Be Converted
D. Percentage Of Farmland in Govt. Jurisdiction With Same Or Higher Relative Value

PART V (To be completed by NRCS) Land Evaluation Information Criterion Relative value of Farmland to Be Serviced or Converted (Scale of 0 - 100 Points)

PART VI (To be completed by Federal Agency) Corridor Assessment Criteria (These criteria are explained in 7 CFR 658.5(c))	Maximum Points				
1. Area in Nonurban Use	15	0			
2. Perimeter in Nonurban Use	10	0			
3. Percent Of Corridor Being Farmed	20	0			
4. Protection Provided By State And Local Government	20	0			
5. Size of Present Farm Unit Compared To Average	10	0			
6. Creation Of Nonfarmable Farmland	25	0			
7. Availability Of Farm Support Services	5	0			
8. On-Farm Investments	20	0			
9. Effects Of Conversion On Farm Support Services	25	0			
10. Compatibility With Existing Agricultural Use	10	0			
TOTAL CORRIDOR ASSESSMENT POINTS	160	0	0	0	0

PART VII (To be completed by Federal Agency)					
Relative Value Of Farmland (From Part V)	100				
Total Corridor Assessment (From Part VI above or a local site assessment)	160	0	0	0	0
TOTAL POINTS (Total of above 2 lines)	260	0	0	0	0

1. Corridor Selected: A	2. Total Acres of Farmlands to be Converted by Project: 0	3. Date Of Selection: 12/8/11	4. Was A Local Site Assessment Used? YES <input type="checkbox"/> NO <input checked="" type="checkbox"/>
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5. Reason For Selection:
Corridor A includes the preferred alternative which will not require right-of-way acquisition and therefore will not result in the conversion of farm ground.

Signature of Person Completing this Part: _____ DATE **12-9-2011**

NOTE: Complete a form for each segment with more than one Alternate Corridor

Project No. _____ Des. No. 1173149

Project Description: SR 933, over St. Joseph River, 1.59 miles North of SR 23 Bridge Rehabilitation, St. Joseph County, Indiana

Name of Organization requesting early coordination:

INDIANA DEPARTMENT OF TRANSPORTATION - LaPorte District

QUESTIONNAIRE FOR THE INDIANA GEOLOGICAL SURVEY

- 1) Do unusual and/or problem () geographic, () geological, () geophysical, or () topographic features exist within the project limits? Describe:
No

- 2) Have existing or potential mineral resources been identified in this area? Describe:
No

- 3) Are there any active or abandoned mineral resources extraction sites located nearby? Describe: No

This information was furnished by:

Name: Robin Rupp Title: Geologist
Address: 611 North Walnut Grove, Bloomington, IN 47405
Phone: 812-855-7428 Date: January 12, 2012

**Questionnaire for the Indiana Department of Transportation,
Office of Aviation**

Project No: _____ **Des/Bridge No:** 1173149 _____

Project Description:

SR 933, over St. Joseph River, 1.59 miles North of SR 23
South Bend, St. Joseph County, Indiana

Requested By:

Travis Mast LaPorte District INDOT

Are there any existing or proposed airports within or near the project limits? YES

If yes, describe any potential conflicts with air traffic during or after the construction of the project.

The Michiana Regional Airport is located approximately
14,400' Northwest of the project.

If any permanent structures or equipment utilized for
the project penetrates the 100:1 slope from the airport FAA
form 7460 (Notice of Proposed construction or alteration) must
be filed. For assistance contact Marcus Dial, INDOT Office of
Aviation, 317-232-1494.

This information was furnished by:

Name: James W. Kinder
Title: Chief Airport Inspector – INDOT Office of Aviation
Date: December 15, 2011

Mast, Travis

From: Clark, Rickie
Sent: Wednesday, December 28, 2011 10:14 AM
To: Mast, Travis
Subject: DES# 1173149 SR 933 over St. Joseph River Bridge Rehabilitation (Early Coordination)

Good Morning Travis,

I received an early coordination notification letter for the project listed above. The reason we're requesting to be notified at the early coordination stage is to try to develop public involvement plans and raise public awareness of transportation improvement projects earlier in the project development phase when it makes sense to engage the public/stakeholders. For larger projects (EIS, EA) INDOT/LPA's do a great job engaging the public. For smaller jobs (CE) there may be other opportunities to engage the public prior to the hearings phase, so I wanted to send the following templates for consideration as this project develops and to also let you know that my office is available to help with any public outreach efforts you may wish to use as this project develops.

The templates may be helpful in documenting any public involvement activities implemented during project development or perhaps encourage discussion in identifying any public involvement needs for the project. The public involvement plan could be as simple as using a more detailed Notice of Survey with additional contact info, and the normal Public Hearings phase.....or a detailed Notice of Survey, a media release/advisory, the Public Hearings phase, in addition to Sec. 106 or something like that.

My office is available to provide support and/or resources to bolster any public involvement activities you may wish to implement or just discuss. I think the key here is to not necessarily require action but to document that there was some level of thought given towards public involvement at the early stages of development.

I appreciate the time and opportunity to comment.

Rickie Clark, INDOT Office of Public Involvement
(317) 232-6601



Community Context PublicInvolvementS
Audit Public... pecialistsPI...

Appendix D

Section 106 Studies



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N758
Indianapolis, Indiana 46204

PHONE: (317) 232-5533
FAX: (317) 232-0238

Mitchell E. Daniels, Jr., Governor
Michael B. Cline, Commissioner

21 February, 2012

Mr. Robert E. Carter, Jr.
State Historic Preservation Officer
Indiana Department of Natural Resources
Division of Historic Preservation and Archaeology
402 W. Washington Street, W274
Indianapolis, IN 46204

RE: Route No: SR 933/Michigan Avenue
 Des. No: 1173149
 Description: Bridge project over the St. Joseph River
 City: City of South Bend, Portage Township, St. Joseph County; (DHPA#12724)

Dear Mr. Carter:

On February 21, 2012, the Indiana Department of Transportation (INDOT) has signed, on behalf of the Federal Highway Administration (FHWA), a final determination of "no adverse effect" for this project. In accordance with 36 CFR 800.5(c), you and the parties that responded affirmatively to the Section 106 consulting party invitation are being provided the documentation for this finding, specified in Sec. 800.11(e). Also, as specified in 36 CFR 800.5(c), consulting parties have 30 days from receipt of this documentation to review and comment on the finding.

Failure to respond within 30 days from receipt of the finding shall be considered agreement with the finding. If you have any questions regarding this matter, please feel free to contact Ms. Susan R. Branigin of this section at (317) 234-0142. Thank you in advance for your input.

Sincerely,

A handwritten signature in blue ink, appearing to read 'Staffan D. Peterson'.

Staffan D. Peterson, Ph.D.
Manager, Cultural Resources Office
INDOT Environmental Services

SDP/SRB/srb
Enclosures

cc: ES project file

K-Todd Behling, INDOT-LaPorte District Project Manager
Travis Mast, INDOT LaPorte District Environmental Manager
Stephen Weintraut, Butler, Fairman & Seufert Project Manager
Catherine Hostetler, Director, South Bend and St. Joseph County Historic Preservation Commission



Division of Historic Preservation & Archaeology • 402 W. Washington Street, W274 • Indianapolis, IN 46204-2739
Phone 317-232-1646 • Fax 317-232-0693 • dhpa@dnr.IN.gov



March 23, 2012

Staffan Peterson, Ph.D.
Cultural Resources Manager
Environmental Services
Indiana Department of Transportation
100 North Senate Avenue, Room N642
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: Notification of INDOT's finding of "no adverse effect" and 800.11 documentation regarding the SR 933 Bridge carrying Michigan Street over the St. Joseph River (Designation # 1173149; DHPA #12724)

Dear Dr. Peterson:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated February 21, 2012 and received on February 23, 2012 for the above indicated project in South Bend, St. Joseph County, Indiana.

As previously indicated, we do not believe the characteristics that qualify the identified historic properties for inclusion in the National Register of Historic Places will be diminished as a result of this project. In regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the proposed project area.

Therefore, we concur with the INDOT's February 21, 2012 finding, on behalf of the FHWA, that there are no historic buildings, structures, districts, objects, or archaeological resources within the area of potential effects that will be adversely affected by the above indicated project.

Please keep in mind this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. Additionally, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and 29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about archaeological issues please contact Wade Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. If you have questions about buildings or structures please contact Chad Slider at (317) 234-5366 or cslider@dnr.IN.gov.

Very truly yours,

James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:CWS:cws

emc: Staffan D. Peterson, Ph.D., INDOT
Mary Kennedy, INDOT
Shaun Miller, INDOT
Susan Branigin, INDOT
Melany Prather, INDOT

**FEDERAL HIGHWAY ADMINISTRATION's
SECTION 4(f) COMPLIANCE REQUIREMENTS (for historic properties) AND
SECTION 106 FINDINGS AND DETERMINATIONS
AREA OF POTENTIAL EFFECT
ELIGIBILITY DETERMINATIONS
EFFECT FINDING
SR 933/Michigan Street
Bridge Project
Over the St. Joseph River
South Bend, Portage Township, St. Joseph County
DES. NO. 1173149
FEDERAL PROJECT NO.**

AREA OF POTENTIAL EFFECT
(Pursuant to 36 CFR Section 800.4(a)(1))

The project is located on SR 933/Michigan Street over the St. Joseph River in the City of South Bend, Portage Township, St. Joseph County. The area of potential effect (APE) includes those areas of existing and proposed right-of-way and incidental construction, including immediately adjacent properties. The preferred alternative for this project will include removal and replacement of the existing bridge railing panels between Abutment 1 and Abutment 4, the performance of minor repairs to the railing posts, and major repairs to the arch ring. All work will be restricted to the existing bridge structure; no new right-of-way will be acquired.

ELIGIBILITY DETERMINATIONS
(Pursuant to 36 CFR 800.4(c)(2))

The APE contains three NR- listed resources: **1)** Leeper Park Historic District (#201-598-34001-34018; listed 2000 under Criterion C: Landscape Architecture); **2)** Samuel Leeper, Jr. House (#201-598-37018; listed 1985 under Criterion C: Architecture); **3)** North Pumping Station (site #201-598-37014); listed 1997 under Criterion C; Architecture). The APE also contains four resources recommended eligible for listing in the National Register of Historic Places: **1)** Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018; "Outstanding" resource in the NR-listed LPHD. It is eligible under Criterion C: Landscape Architecture; **2)** Harter Heights Historic District (HHHD)/#201-597/598-35001-277); **3)** Northshore Triangle Historic District (NSTHD)/#201-598-36001-290); **4)** West North Shore Historic District (WNSHD)/#201-598-37001-020). The HHHD, NSTHD and WNSHD are NR-eligible under Criterion A: Exploration/Settlement and Community Planning/Development and under Criterion C: Architecture.

EFFECT FINDING
(Pursuant to 36 CFR 800.4(d)(1))

INDOT, acting on FHWA's behalf, has determined a "No Adverse Effect" finding is appropriate for this undertaking because the project will not introduce any new visual, atmospheric or audible elements that would alter any of the characteristics that qualify the Leeper Park Historic District (#201-598-34001-34018), Samuel Leeper, Jr. House (#201-598-37018), or North Pumping Station (site #201-598-37014) for listing in the National Register of Historic Places.

The project will also will not introduce any new visual, atmospheric or audible elements that would alter any of those characteristics or qualities that qualify the following resources, recommended eligible for National Register-listing: **1)** Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018); **2)** Harter Heights Historic District (HHHD)/#201-597/598-35001-277); **3)** Northshore Triangle Historic District (NSTHD)/#201-598-36001-290); **4)** West North Shore Historic District (WNSHD)/#201-598-37001-020).

In addition, per "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridge PA), the project scope activities conducted as part of Des. #1173149 shall adhere to the Secretary of the Interior's Standards for Rehabilitation, and will not introduce negative impacts as defined in 36 CFR §800.5(a)(ii) to the NR-eligible Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018).

SECTION 4(f) COMPLIANCE REQUIREMENTS (for historic properties)

Leeper Park Historic District (LPHD; #201-598-34001-018), bounded roughly by the St. Joseph River on the north and east, by Park Lane and Bartlett Street to the south and the east property lines of properties to the west)--This undertaking will not convert

property from *Leeper Park Historic District LPHD, a designed-landscape park bounded roughly by the St. Joseph River on the north and east, by Park Lane and Bartlett Street to the south and the east property lines of properties to the west*), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."

The North Pumping Station (#201-598-34014; 830 N. Michigan Avenue) --This undertaking will not convert property from *The North Pumping Station (#201-598-34014; 830 N. Michigan Avenue)*, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."

Samuel Leeper House (#201-598-37018; 113 North Shore Drive)--This undertaking will not convert property from *Samuel Leeper House (#201-598-37018; 113 North Shore Drive)*, a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."

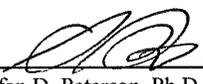
Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018)--This resource is used for transportation purposes. This undertaking will have a "No Adverse Effect" on *Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018)*, a Section 4(f) historic property. INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."

Harter Heights Historic District (HHHD); #201-597-598-35001-277), roughly bounded by *Angela Boulevard to the north, Michigan Street to the east, Stanfield Street to the west and Corby Boulevard to the south*--This undertaking will not convert property from *Harter Heights Historic District (HHHD); #201-597-598-35001-277*), roughly bounded by *Angela Boulevard to the north, Michigan Street to the east, Stanfield Street to the west and Corby Boulevard to the south*), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."

West North Shore Historic District (WNSHD; #201-598-37001-020), roughly bounded by the *St. Joseph River to the south, by Michigan Street to the west, Iroquois Street to the east and West North Shore Drive to the north*--This undertaking will not convert property from *West North Shore Historic District (WNSHD; #201-598-37001-020)* roughly bounded by the *St. Joseph River to the south, by Michigan Street to the west, Iroquois Street to the east and West North Shore Drive to the north*), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."

Northshore Triangle Historic District (NTHD; #201-498-36001-290) roughly bounded by *Angela Boulevard to the north, Marquette Avenue to the south, railroad tracks and Michigan Street to the east and Iroquois Street to the east*--This undertaking will not convert property from *Northshore Triangle Historic District (NTHD; #201-498-36001-290)* roughly bounded by *Angela Boulevard to the north, Marquette Avenue to the south, railroad tracks and Michigan Street to the east and Iroquois Street to the east*), a Section 4(f) historic property, to a transportation use; INDOT, acting on FHWA's behalf, has determined the appropriate Section 106 finding is "No Adverse Effect"; therefore, no Section 4(f) evaluation is required. INDOT respectfully requests the Indiana State Historic Preservation Officer provide written concurrence with the Section 106 determination of "No Adverse Effect."

Consulting parties will be provided a copy of the findings and determinations in accordance with INDOT and FHWA's Section 106 procedures. Comments will be accepted for 30 days upon receipt of the findings.


Staffan D. Peterson, Ph.D., for FHWA
Manager, Cultural Resources Office
INDOT Environmental Services

2/21/2012
Approved Date

**FEDERAL HIGHWAY ADMINISTRATION
DOCUMENTATION OF SECTION 106 FINDING OF
NO ADVERSE EFFECT
SUBMITTED TO THE STATE HISTORIC PRESERVATION OFFICER
PURSUANT TO 36 CFR Section 800.5(c)
SR 933/Michigan Street
Bridge Project
Over the St. Joseph River
South Bend, Portage Township, St. Joseph County
DES. NO. 1173149
FEDERAL PROJECT NO.**

1. DESCRIPTION OF UNDERTAKING

The proposed project is located on the SR 933/Michigan Street Bridge over the St. Joseph River in South Bend, Portage Township, St. Joseph County. The project is in the Indiana Department of Transportation (INDOT) LaPorte District. Within the proposed project limits, SR 933 is functionally classified as an Urban Other Principal Arterial route and is not on the National Highway System. The federal involvement in the project is funding received from the Federal Highway Administration (FHWA).

The existing structure, Bridge No. (933) 31-71-03690-B/NBI No. 11046), is a 3-span (84 feet X 120 feet X 84 feet) reinforced concrete arch. It was constructed by the City of South Bend in 1914-1915 as part of Leeper Park, and is locally known as the Leeper Bridge. The SR 933/Michigan Street/Leeper Bridge was repaired in 1977 and 2006.

The SR 933/Michigan Street/Leeper Bridge over the St. Joseph River (Bridge No. (933)31-71-3690-B/NBI No. 11046) is an "Outstanding" resource in the Leeper Park Historic District (LPHD), which was listed in the National Register of Historic Places (NR) in May, 2000. The bridge is also individually NR-eligible, in part, for its engineering significance. The bridge was classified as "Select" by the 2010 Indiana Department of Transportation (INDOT) Historic Bridge Inventory. "Select" bridges are defined in the Historic Bridge PA as those structures that are excellent examples of a specific bridge type and that are most suitable for preservation. The SR 933/Michigan Street/Leeper Bridge also possesses Local Landmark status as conferred by the City of South Bend.

A bridge inspection conducted in the summer of 2011 revealed structural deficiencies; the proposed project was intended to address the following areas of deterioration as noted in the inspection report:

- Spot elevations of the top of the bridge deck and the underside of the arch rings of the east and west copings at center span revealed that the contours on the underside of Span B's arch ring indicate that the elevation of the arch's crown is approximately 6 inches higher on the bridge's west side than on the east. This difference in elevation is probably due to the settlement of the foundation under the east side of Pier 3;
- Transverse cracking and leaching are present on the underside of the Span B arch ring;
- In Span A, the arch segments east and west of the longitudinal centerline of the bridge are approximately the same, and transverse cracks cover approximately the middle 25% of the span;
- In Span B, the arch segments west of the bridge's longitudinal centerline exhibit transverse cracks that cover approximately 30% of the span, and this area is shifted slightly towards Pier 3. This shift toward Pier 3 is most likely due to the settlement of the east side of this pier;
- The east side of the longitudinal construction joint between the 16-foot, 11-inch and 10-foot, 3-inch segments has shifted down nearly 3 inches and has shifted horizontally to the south by 2 inches. This deflection is due to the settlement of the east side of Pier 3. Many of the cracks in the patched areas are shrinkage cracks in the patching mortar, since most of them do not extend into the existing concrete;
- In Span C, the arch segments west of the longitudinal centerline of the bridge have transverse cracks in that cover approximately the middle 15% of the span;
- In Span C, the arch segments east of the longitudinal centerline of the bridge have transverse cracks that cover approximately 50% of the span, and this area is significantly shifted towards Pier 3. This shift towards Pier 3 is most likely due to the settlement of the east side of this pier;

- Cracks were observed in the new pavement. These appear to be aligned with cracks or joints in the previous pavement layer;
- Existing cracking observed on the east and west sides of the bridge railing may date to 1977. Cracking, with vertical and horizontal deflection is present on both sides of the bridge with the east side rail being much more severe than the west side. The railing appears to be buckling due to high compressive forces. This compression is most likely produced by vertical deflection downward of the arch ring, which was a result of Pier 3 settlement. Another factor that may have aggravated the cracking distress was the shrinkage of the new concrete pavement slab that was tied to the top of the spandrel wall in order to stabilize the outward movement of the wall.

The spandrel wall concrete in the exposed back side of the bridge's SW corner wing wall is of poor quality and exhibits lack of consolidation;

The preferred alternative for this project will include removal and replacement of the existing bridge railing panels between Abutment 1 and Abutment 4, the performance of minor repairs to the railing posts, and major repairs to the arch ring. All work will be restricted to the existing bridge structure; no new right-of-way will be acquired. The scope of work for the proposed preferred alternative (rehabilitation) is as follows:

- Remove and replace the existing bridge railing panels from between abutment 1 (major post number 5 east and 62 west) and abutment 4 (major post 53 east and 110 west). All major and minor posts are to remain in place;
- Repair railing posts as follows: **A)** Remove and reinstall with new mortar joints all post stones (column and capital stone) above base stone between abutments 1 and 4. All posts which are not currently vertical shall be plumbed as part of this work; **B)** Post 27 east column stone replaced; **C)** Post 31 east should have crack epoxy sealed; **D)** Post 33 east should have crack epoxy sealed; **E)** Post 98 west should have crack epoxy sealed; **F)** Post 100 west should have crack epoxy sealed;
- All cracks in the existing concrete pavement will be sealed by INDOT LaPorte District maintenance;
- All spandrel walls will be repaired as follows: **A)** Seal all open cracks in stone veneer with Jahn M70 patching mortar; **B)** Fill all core holes with Jahn M70 patching mortar; **C)** Fill void behind capital stones at nose of piers and re-point joint;

The arch rings will be repaired as follows: **A)** Epoxy-inject all cracks in the bottom of the arch rings; **B)** Perform impact echo survey on bottom of arch rings after epoxy-injection of exposed transverse cracks to determine if there are any areas of horizontal delamination that have not been filled with epoxy; **C)** Epoxy-inject all horizontally delaminated area that have not been already sealed with epoxy; **D)** Perform another impact echo survey to determine if any horizontally delaminated areas remain unfilled. Repeat this process until no unfilled delaminations are present in the arch rings; **E)** Sandblast and grind exposed surface of arch ring in preparation for application of carbon fiber reinforcement (FRP); **F)** Perform a wet lay-up (placed parallel to the span) of carbon fiber fabric (Tyfo Sch-41 or equal) with 50% coverage, 12-inch strips at 24-inch centers. This fabric will increase the strength of the arch ring, and because of the 12-inch space between fabric layers, INDOT will still be able to monitor the transverse cracks and visually inspect 50% of the original concrete surface; **G)** Coat the underside of the arch ring to provide UV protection for the fabric.

Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridge PA), the Federal Highway Administration—Indiana Division will "...satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges..." through the Project Development Process (PDP) of the Historic Bridge PA. The SR 933/Michigan Street/Leeper Bridge over the St. Joseph River has been classified as a "Select" bridge by the INDOT Historic Bridge Inventory. "Select" bridges are defined in the Historic Bridge PA as those structures that are excellent examples of a specific bridge type and that are most suitable for preservation. The Federal Highway Administration—Indiana Division will satisfy its Section 106 responsibilities for other resources located in the project area through coordination/consultation with consulting parties.

Land use in the proposed project area is urban/residential. The APE has been determined as the existing and proposed right-of-way (R/W) and the area immediately surrounding it, including incidental construction, and it takes into account the properties that might experience physical and/or visual impacts from the project. (See APE map in *Appendix B*.) The approval of the APE, by INDOT, acting on FHWA's behalf, is included in this document

2. EFFORTS TO IDENTIFY HISTORIC PROPERTIES

The State and National Register listings for St. Joseph County were checked. The following NR-listed resources were located within the proposed project area: **1) Leeper Park Historic District/#201-598-34001-018, of which the subject structure, Bridge No. (933)31-71-3690-B/NBI No. 11046 #201-598-34018) is a contributing resource); 2) North Pumping Station/#201-598-34014; 3) Samuel Leeper, Jr. House/#201-598-37018.**

The City of South Bend was surveyed in 2005 for the City of South Bend Historic Sites and Structures Inventory (Portage Township; South Bend Scattered Sites (SBSS); Leeper Park Historic District (LPHD)/#201-598-34001-34018); Harter Heights Historic District (HHHD)/#201-597/598-35001-277); Northshore Triangle Historic District (NSTHD)/#201-598-36001-290); West North Shore Historic District (WNSHD)/#201-598-37001-020. No surveyed Portage Township resources were located in the proposed project area.

The following surveyed SBSS resources were located in the project area: **1) SBSS #201-597-10019** (Mason House; 140 E. North Shore Drive; ca.-1917 Prairie; rated “Notable”); **2) SBSS #201-597-10020** (O’Hara House; 134 E. North Shore Drive; ca.-1928 English Cottage; rated “Notable”); **3) SBSS #201-597-10021** (Taylor House; 128 E. North Shore Drive; ca.-1928 Dutch Colonial Revival; rated Contributing”); **4) SBSS #201-597-10022** (Mattes House; 122 E. North Shore Drive; ca.-1907 Dutch Colonial Revival; rated “Notable”). (See *Appendices* section of this report for table of SBSS resources located in the project APE.)

All surveyed LPHD resources were located in the proposed project area, with the following exceptions: **1) #201-598-34012**; ca.-1970 Recreation Building; rated “Non-contributing”); **2) #201-598 34113** (ca.-1970 Tennis Courts; rated “Non-contributing”). One resource located in the LPHD is individually NR-listed: **North Pumping Station/#201-598-34014**. (See *Appendices* section of this report for table of LPHD resources located in the project APE.)

The following surveyed HHHD resources were located in the proposed project area: **1) #201-597-598-35141** (First Unitarian Church; 101 East North Shore Drive; ca.-1955 International; rated “Notable”); **2) #201-597-598-35142** (Mattes House; 121 East North Shore Drive; ca.-1921 Colonial Revival; rated “Outstanding”); **3) #201-597-598-35143** (Van Dien House; 127 East North Shore Drive; ca.-1921 Colonial Revival; rated “Outstanding”); **4) #201-597-598-35144** (Ca.-1950 Contemporary house; 137 East North Shore Drive; rated “Outstanding”); **5) #201-597-598-35155** (Kruger House; 1020 Michigan Street; ca.-1920 Dutch Colonial Revival; rated “Notable”); **6) #201-597-598-35156** (Gilbert House; 1022 Michigan Street; ca.-1908 dormer-front bungalow; rated “Notable”); **7) #201-597-598-35157** (ca.-1908 Dutch Colonial Revival house; 1026 Michigan Street; rated “Contributing”). (See *Appendices* section of this report for table of HHHD resources located in the project APE.)

The following surveyed NSTHD resources were located in the proposed project area: **1) #201-598-36234** (Judge Slick House; 106 Marquette Avenue; ca.-1911 American Four Square/Prairie; rated “Outstanding”); **2) #201-598-36235** (Woodward House; 102 Marquette Avenue; ca.-1902 Queen Anne; rated “Outstanding”). (See *Appendices* section of this report for table of NSTHD resources located in the project APE.)

The following surveyed WNSHD resources were located in the proposed project area: **1) #201-598-37009** (Doloh House; 229 North Shore Drive; ca.-1910 Tudor Revival; rated “Outstanding”); **2) #201-598-37010** (Hoover House; 221 North Shore Drive; ca.-Mission Revival; rated “Outstanding”); **3) #201-598-37011** (Bryan House; 219 North Shore Drive; ca.-1925 Italian Renaissance Revival; rated “Outstanding”); **4) #201-598-37012** (Carson House; 215 North Shore Drive; ca.-1905 American Four Square; rated “Notable”); **5) #201-598-37013** (Neff House; 209 North Shore Drive; ca.-1905 Colonial Revival; rated “Outstanding”); **6) #201-598-37014** (Rider House; 201 North Shore Drive; ca.-1906 Neoclassical; rated “Notable”); **7) #201-598-37015** (Miler House; 129 North Shore Drive; ca.-1905 Neoclassical; rated “Notable”); **8) #201-598-37016** Mack House (123 North Shore Drive; ca.-1903 American Four Square; rated “Notable”); **9) #201-598-37017** (Rutherford House; 117 North Shore Drive; ca.-1904 Free Classic; rated “Notable”); **10) #201-598-37018** Samuel Leeper, Jr. House; 113 North Shore Drive; ca.-1888 Gable-front/Italianate; rated “Outstanding”; NR-listed); **11) #201-598-37019** (Lent House; 107 North Shore Drive; ca.-1903 Neoclassical; rated “Outstanding”); **12) #201-598-37020** (Judge Slick House; 103 North Shore Drive; ca.-1906 Free Classic; rated “Notable”). (See *Appendices* section of this report for table of WNSHD resources located in the project APE.)

A historic property report (HPR); Branigin, December, 2011) was prepared and concluded that the subject structure, SR 933/Michigan Street/Leeper Bridge (Bridge No. (933)31-71-3690-B/NBI No. 11046 #201-598-34018) remained NR-eligible. In addition, the report recommended that the following, previously named NR-listed resources remained eligible for listing in the National Register: **1) Leeper Park Historic District (LPHD); #201-598-34001-018, in which the subject structure, Bridge No. (933)31-71-3690-B/NBI No. 11046 #201-598-34018) is an “Outstanding” resource); 2) North Pumping Station/#201-598-34014; 3) Samuel Leeper, Jr. House/#201-598-37018.**

The HPR also recommended that the following historic districts located in the APE are NR-eligible under Criterion A: Exploration/Settlement and Community Planning/Development and under Criterion C: Architecture: **1) Harter Heights Historic**

District (HHHD)/#201-597/598-35001-277); 2) Northshore Triangle Historic District (NSTHD)/#201-598-36001-290); 3) West North Shore Historic District (WNSHD)/#201-598-37001-020).

With regard to archaeological concerns, an archaeological short report (Miller, October, 2011) was prepared and submitted to the office of the Indiana State Historic Preservation Officer (SHPO) for review. The report recommended project clearance, based upon the author's opinion that "...the project area does not have the potential to contain archaeological resources and no further work is recommended..."

On December 21, 2011, the following parties were invited to be Section 106 consulting parties and to aid in identification of historic properties: SHPO; Indiana Landmarks/Northern Regional Office; St. Joseph County Historian; City of South Bend; Historic Michigan Road Byway Committee; Center for History/South Bend; Hannah Lindahl Children's Museum; Michiana Jewish Historical Society; Mishawaka Penn-Harris Public Library; South Bend Area Genealogical Society; Historic Preservation Commission of South Bend & St. Joseph County; South Bend Heritage Foundation, Inc.; Downtown South Bend, Inc; Indiana Lincoln Highway Association, Inc.; Michiana Area Council of Governments (MACOG); Indiana Historic Spans Task Force; Dr. James L. Cooper, Ph.D.

The following parties responded to the December 21 mailing with none expressing any objections about the project and with none providing comments about additional historic properties not already identified in the HPR: Historic Michigan Road Byway Committee (letter dated December 26, 2011); the South Bend Historic Preservation Commission (SBHPC) (letter dated December 29, 2011); Indiana Landmarks Northern Regional Office (INL-NRO) (email dated January 20, 2012); and the South Bend Area Genealogical Society (letter dated January 23, 2012). (See correspondence in Appendix D and see Section 6 for more information).

The SHPO responded to the December 21 mailing in a letter dated 20 January, 2012 (DHPA #12724), stating in part the following: "...In regard to buildings and structures, we concur with INDOT's assessment that the Mason House at 140 East North Shore Drive (Indiana Historic Sites and Structures Interim Report—St. Joseph County site #201-597-10019), the O'Hara House at 134 East North Shore Drive (site #201-597-10020) and the Mattes House at 122 East North Shore Drive (site #201-597-10021) are not eligible for inclusion in the National Register of Historic Places.

Additionally, we concur with INDOT's assessment that the following properties are eligible for inclusion in the National Register of Historic Places: 1) West North Shore District [sic] (sites #201-598-37001-37020) for its collection of late 19th and early 20th century Revival [sic] residential architecture along with its association to the early planning and development of South Bend; 2) Harter Heights Historic District (sites #201-597-35001-35277) for its association with early planning and development of South Bend. The district also has strong architectural integrity and many of the properties were designed by local architectural firms; 3) North[shore] Triangle Historic District (sites #201-598-36001-36290) for its association with early planning and development of South Bend. Also the properties within the district embody a high level of architectural integrity and it includes several properties that were designed by Ernest Young.

Furthermore, we agree that the following properties were listed in the National Register of Historic Places: 1) Leeper Park Historic District (sites #201-598-34001-34018) listed 06-15-2000 (HPR states 1998); 2) Samuel Leeper, Jr. House (site #201-598-37018) listed 03-21-1985; 3) North Pumping Station (site #201-598-37014) listed 01-02-1997 (HPR states 1996)/

Also we agree that St. Joseph County Bridge No. 933 (NBI No. 11046, site #201-598-34018) is an "Outstanding" contributing resource within the Leeper Park Historic District and is the subject of the proposed project. We note that the cover letter and HPR state this bridge is individually eligible as a contributing resource to the Leeper Park Historic District. Since the criteria for contributing resources and individually eligible resources differs, we assumed that the intention was to state that the bridge is a contributing resource to the district and it also meets the criteria to be considered individual [sic] eligible. However, we do agree that the Indiana Historic Bridges Inventory [sic] states that St. Joseph County Bridge No. 933 is a 'Contributing resource in a listed historic district. Therefore this bridge will not be reevaluated as part of this inventory project.' We also concur that the Indiana Historic Bridges Inventory [sic] determined St. Joseph County Bridge No. 933 to be a Select bridge."

With regard to archaeological resources, the SHPO's 20 January, 2012 letter (DHPA #12724) stated the following in part: "...Based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the proposed project area. However, this identification is subject to the project activities remaining in areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the National Register of Historic Places in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered..." (See letter in Appendix D of this document).

The SHPO's 20 January, 2012 letter also asked questions related to the project's scope and impacts, which are discussed in Section 6 of this document. No other invited consulting party responded to the December 21, 2011 letter.

3. DESCRIBE AFFECTED PROPERTIES

Leeper Park Historic District (LPHD; #201-598-34001-018) is a designed-landscape park bounded roughly by the St. Joseph River on the north and east, by Park Lane and Bartlett Street to the south and the east property lines of properties to the west. The park was designed by City Beautiful Movement landscape architect George Kessler in a 1912 plan commissioned by the City of South Bend. The LPHD was listed in the National Register in 2000 under Criterion C: Landscape Architecture.

The North Pumping Station (#201-598-34014; 830 N. Michigan Avenue) is a two-story brick public works building constructed ca.-1912 by the City of South Bend in the Beaux Arts architectural style. This resource is also an “Outstanding” resource in the NR-listed LPHD. Designed by the South Bend architectural firm of Freyermuth and Maurer, the North Pumping Station was listed in the National Register in 1997 under Criterion C: Architecture.

Samuel Leeper House (#201-598-37018; 113 North Shore Drive) is a two-story brick house constructed ca-1888 in the Italianate and Queen Anne architectural styles. It was listed in the National Register in 1985 under Criterion C: Architecture.

Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018) is a 3-span, reinforced concrete arch structure over the St. Joseph River. It was constructed by the City of South Bend ca.-1914-15 as part of the 1912 George Kessler-designed plan for Leeper Park. The Leeper Bridge is an outstanding resource in the NR-listed Leeper Park Historic District; it is NR-eligible under Criterion C: Landscape Architecture and Engineering.

Harter Heights Historic District (HHHD); #201-597-598-35001-277) is a residential historic district roughly bounded by Angela Boulevard to the north, Michigan Street to the east, Stanfield Street to the west and Corby Boulevard to the south. Platted in 1916 on land originally owned by South Bend’s founder, homes in this planned neighborhood/community exhibit a variety of architectural styles. The HHHD is recommended NR-eligible under Criterion A: Exploration/Settlement and Community Planning and Development and under Criterion C: Architecture.

West North Shore Historic District (WNSHD; #201-598-37001-020) is a residential historic district roughly bounded by the St. Joseph River to the south, by Michigan Street to the west, Iroquois Street to the east and West North Shore Drive to the north. Platted in 1903 on land originally owned by South Bend’s founder, this planned subdivision was laid out with setback requirement and deed restrictions. It contains a variety of 19th and 20th century architectural styles, and is recommended NR-eligible under Criterion A: Exploration/Settlement and Community Planning and Development and under Criterion C: Architecture.

Northshore Triangle Historic District (NTHD; #201-498-36001-290) is a residential historic district roughly bounded by Angela Boulevard to the north, Marquette Avenue to the south, railroad tracks and Michigan Street to the east and Iroquois Street to the east. Located on land originally owned by South Bend’s founder, this neighborhood began with the 1866 land divisions and sales of lots. The neighborhood contains a variety of architectural styles and is recommended for NR-listing under Criterion A: Exploration/Settlement and Community Planning and Development and under Criterion C: Architecture.

4. DESCRIBE THE UNDERTAKING’S EFFECT ON HISTORIC PROPERTIES

The preferred alternative for this project will include removal and in-kind replacement of the existing bridge railing panels between Abutment 1 and Abutment 4, the performance of minor repairs to the railing posts, and major repairs to the arch ring. All work will be restricted to the existing bridge structure; no new right-of-way will be acquired. Because the subject structure (Leeper Bridge; *Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018*) is a “Select” bridge under INDOT’s Historic Bridge PA, its treatment must follow the Secretary of Interior’s Standards for Rehabilitation. The detailed scope of work for the proposed preferred alternative (rehabilitation) was outlined in Section 1 of this document.

Other than the physical impacts to the subject bridge itself, *Leeper Park Historic District (LPHD; #201-598-34001-018; NR-listed 2000)* will also be physically impacted through the alterations to a contributing resource within it (the bridge). The other historic properties within the APE [*North Pumping Station (#201-598-34014; 830 N. Michigan Avenue); Samuel Leeper House (#201-598-37018; 113 North Shore Drive); Harter Heights Historic District (#201-597-598-35001-277); West North Shore Historic District (#201-598-37001-020); and Northshore Triangle Historic District (#201-498-36001-290)*] will only be visually impacted by the project.

5. EXPLAIN APPLICATION OF CRITERIA OF ADVERSE EFFECT—INCLUDE CONDITIONS OR FUTURE ACTIONS TO AVOID, MINIMIZE OR MITIGATE ADVERSE EFFECTS

The finding “No Adverse Effect” is appropriate because the project will not alter any of the characteristics that qualify the *Leeper Park Historic District (LPHD; #201-598-34001-018; NR-listed 2000)*; *North Pumping Station (#201-598-34014; 830 N. Michigan Avenue; individually NR-listed 1997)*; *Samuel Leeper House (#201-598-37018; 113 North Shore Drive; NR-listed 1985)*; *Harter Heights Historic District (HHHD); #201-597-598-35001-277)*; *West North Shore Historic District (WNSHD; #201-598-37001-020)*; or *Northshore Triangle Historic District (NTHD; #201-498-36001-290)* for National Register listing or eligibility.

Per 36 CFR § 800.5(a)(2), examples of adverse effect include, but are not limited to:

- (i) Physical destruction of or damage to all or part of the property;
- (ii) Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines;
- (iii) Removal of the property from its historic location;
- (iv) Change of the character of the property’s use or of physical features within the property’s setting that contribute to its historic significance;
- (v) Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features;
- (vi) Neglect of a property which causes its deterioration, except where such neglect and deterioration are recognized qualities of a property of religious and cultural significance to an Indian tribe or Native Hawaiian organization; and
- (vii) Transfer, lease, or sale of property out of Federal ownership or control without adequate and legally enforceable restrictions or conditions to ensure long-term preservation of the property’s historic significance.

Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the resources are not likely to be as described in 36 CFR § 800.5(a)(i), (iii), (iv), (vi) or (vii). The application of the criteria of adverse effect, focusing on 36 CFR § 800.5(a)(ii) and (v), for each property is as follows:

Leeper Park Historic District (LPHD; #201-598-34001-018; NR-listed 2000); The subject structure (*Leeper Bridge; Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018*), is an “Outstanding” resource in the LPHD. Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the LPHD are most similar to those described in 36 CFR § 800.5(a)(v): “*Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features...*” Project activities will involve in-kind repairs to the subject structure and will not significantly alter the bridge’s physical appearance. In addition, all Des. #1173149 project activities will be performed on the “Select” bridge per the Secretary of Interior’s Standards for Rehabilitation. The project will therefore not introduce visual, atmospheric or audible elements, as defined in 36 CFR §800.5(a), that would diminish the significant historic features of the Leeper Park Historic District.

The North Pumping Station (#201-598-34014; 830 N. Michigan Avenue; individually NR-listed 1997; “Outstanding” resource in the NR-listed LPHD); Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the North Pumping Station are most similar to those described in 36 CFR § 800.5(a)(v): “*Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features...*” Project activities will involve in-kind repairs to the subject structure and will not significantly alter the bridge’s physical appearance. In addition, all Des. #1173149 project activities will be performed on the “Select” bridge per the Secretary of Interior’s Standards for Rehabilitation. Project activities will not significantly alter the appearance of the subject structure, and will not introduce visual, atmospheric or audible elements that would diminish the significant historic features of the North Pumping Station. Des. #1173149 will therefore not introduce negative impacts as defined in 36 CFR §800.5 to the NR-listed North Pumping Station, which is located within the proposed project’s APE;

Samuel Leeper House (#201-598-37018; 113 North Shore Drive; NR-listed 1985); Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the Samuel Leeper House are most similar to those described in 36 CFR § 800.5(a)(v): “*Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features...*” Project activities will involve in-kind repairs to the subject structure and will not significantly alter the bridge’s physical appearance. In addition, all Des. #1173149 project activities will be performed on the “Select” bridge per the Secretary of Interior’s Standards for Rehabilitation. Project activities will not significantly alter the appearance of the subject structure, and will not introduce visual, atmospheric or audible elements that would diminish the significant historic features of the Samuel Leeper House. Des. #1173149 will therefore not introduce negative impacts as defined in 36 CFR §800.5 to the NR-listed Samuel Leeper House, which is located within the proposed project’s APE;

Leeper Bridge (Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018); “Outstanding” resource in the NR-listed LPHD); Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the Leeper Bridge are most similar to those described in 36 CFR § 800.5(a)(ii): “...Alteration of a property, including restoration, rehabilitation, repair, maintenance, stabilization, hazardous material remediation, and provision of handicapped access, that is not consistent with the Secretary’s Standards for the Treatment of Historic Properties (36 CFR part 68) and applicable guidelines...” and 36 CFR § 800.5(a)(v): “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features...”

With regard to 36 CFR § 800.5(a)(v), project scope activities will involve in-kind repairs to the subject structure and will not significantly alter the bridge’s physical appearance. As such, project activities will not significantly alter the appearance of the Leeper Bridge, and will not introduce visual, atmospheric or audible elements that would diminish the bridge’s significant historic features. Des. #1173149 will therefore not introduce negative impacts as defined in 36 CFR §800.5 to the NR-eligible Leeper Bridge.

With regard to 36 CFR § 800.5(a)(ii), Item 2 of “Attachment B: Standard Treatment Approach for Historic Bridges” of the *Programmatic Agreement (PA) Regarding Management and Preservation of Indiana’s Historic Bridges* stipulates that any and all treatment of ‘Select’ bridges under the PA shall be made in compliance with the Secretary of Interior’s Standards for Rehabilitation. Project scope activities conducted as part of Des. #1173149 shall adhere to the Secretary of the Interior’s Standards for Rehabilitation, and will not significantly alter the appearance of the subject structure. Therefore, Des. #1173149 will not introduce negative impacts as defined in 36 CFR §800.5(a)(5) to the NR-eligible Leeper Bridge (*Bridge No. (933) 31-71-03690-B/NBI No. 11046; #201-598-34018*);

Harter Heights Historic District (HHHD; #201-597-598-35001-277; Recommended NR-eligible under Criterion A: Exploration/Settlement and Community Planning and Development and under Criterion C: Architecture); Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the Harter Heights Historic District are most similar to those described in 36 CFR § 800.5(a)(v): “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features...” Project activities will involve in-kind repairs to the subject structure and will not significantly alter the bridge’s physical appearance. In addition, all Des. #1173149 project activities will be performed on the “Select” bridge per the Secretary of Interior’s Standards for Rehabilitation. Project activities will not significantly alter the appearance of the subject structure, and will not introduce visual, atmospheric or audible elements that would diminish the significant historic features of the HHHD. Des. #1173149 will therefore not introduce negative impacts as defined in 36 CFR §800.5 to the NR-eligible HHHD, which is located within the proposed project’s APE;

West North Shore Historic District (WNSHD; #201-598-37001-020; Recommended NR-eligible under Criterion A: Exploration/Settlement and Community Planning and Development and under Criterion C: Architecture); Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the West North Shore Historic District are most similar to those described in 36 CFR § 800.5(a)(v): “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features...” Project activities will involve in-kind repairs to the subject structure and will not significantly alter the bridge’s physical appearance. In addition, all Des. #1173149 project activities will be performed on the “Select” bridge per the Secretary of Interior’s Standards for Rehabilitation. Project activities will not significantly alter the appearance of the subject structure, and will not introduce visual, atmospheric or audible elements that would diminish the significant historic features of the WNSHD. Des. #1173149 will therefore not introduce negative impacts as defined in 36 CFR §800.5 to the NR-eligible WNSHD, which is located within the proposed project’s APE;

Northshore Triangle Historic District (NTHD; #201-498-36001-290; Recommended for NR-listing under Criterion A: Exploration/Settlement and Community Planning and Development and under Criterion C: Architecture); Application of the criteria of adverse effect, as defined per 36 CFR § 800.5(a)(2), finds that the proposed project’s potential effects upon the Northshore Triangle Historic District are most similar to those described in 36 CFR § 800.5(a)(v): “Introduction of visual, atmospheric or audible elements that diminish the integrity of the property’s significant historic features...” Project activities will involve in-kind repairs to the subject structure and will not significantly alter the bridge’s physical appearance. In addition, all Des. #1173149 project activities will be performed on the “Select” bridge per the Secretary of Interior’s Standards for Rehabilitation. Project activities will not significantly alter the appearance of the subject structure, and will not introduce visual, atmospheric or audible elements that would diminish the significant historic features of the NTHD. Des. #1173149 will therefore not introduce negative impacts as defined in 36 CFR §800.5 to the NR-eligible NTHD, which is located within the proposed project’s APE.

6. SUMMARY OF CONSULTING PARTIES AND PUBLIC VIEWS

On December 21, 2011, the following parties were invited to be Section 106 consulting parties and to aid in identification of historic properties: SHPO; Indiana Landmarks/Northern Regional Office; St. Joseph County Historian; City of South Bend;

Historic Michigan Road Byway Committee; Center for History/South Bend; Hannah Lindahl Children's Museum; Michiana Jewish Historical Society; Mishawaka Penn-Harris Public Library; South Bend Area Genealogical Society; Historic Preservation Commission of South Bend & St. Joseph County; South Bend Heritage Foundation, Inc.; Downtown South Bend, Inc; Indiana Lincoln Highway Association, Inc.; Michiana Area Council of Governments (MACOG); Indiana Historic Spans Task Force; Dr. James L. Cooper, Ph.D.

The Historic Michigan Road Byway Committee responded in a letter dated December 26, 2011 and declined to participate as a consulting party. The letter stated the following in part: "...*The bridge and the area in which the project is located is an important historical resource with several historical resources as identified in your report. We were contacted as a potential consulting party; however, the Michigan Road Byway is not located in the APE of the project. The byway turns west on Lincolnway and continues west from the city on that route, therefore, we will decline to be a consulting part on this project...*" (See letter in *Appendix D.*)

The South Bend Historic Preservation Commission (SBHPC) responded in a letter dated December 29, 2011, stating in part the following: "*Thank you for the information on the proposed restorations, in-kind repairs and structural upgrades to the Leeper Park Bridge. The Historic Preservation Commission of South Bend and St. Joseph County wishes to be a consulting party on this project...The project as presented does not pose a negative impact on any of the built environment located in the APE...*" (See letter in *Appendix D.*)

The SBHPC's December 29 letter further noted that the NR-eligible Leeper Bridge is a Local Landmark and that the NR-listed Leeper Park is a Landscape Local Landmark. The SBHC indicated that it would be necessary for INDOT to obtain a Certificate of Appropriateness (COA) for work done to these local landmarks under Des. #1173149. Arrangements were made through the INDOT-LaPorte District to comply with this process. (See letter in *Appendix D.*)

Indiana Landmarks Northern Regional Office (INL-NRO) responded in an email dated January 20, 2012, stating in part the following: "...*Pertaining to the proposed rehabilitation of the SR 933 bridge (Designation 1173149) I do not have any comments or concerns pertaining to the proposed rehabilitation or its impact on nearby/adjacent historic resources...*" (See email in *Appendix D.*)

The SHPO responded to the December 21 mailing in a letter dated 20 January, 2012 (DHPA #12724), stating in part the following: "...*In regard to buildings and structures, we concur with INDOT's assessment that the Mason House at 140 East North Shore Drive (Indiana Historic Sites and Structures Interim Report—St. Joseph County site #201-597-10019), the O'Hara House at 134 East North Shore Drive (site #201-597-10020) and the Mattes House at 122 East North Shore Drive (site #201-597-10021) are not eligible for inclusion in the National Register of Historic Places.*

Additionally, we concur with INDOT's assessment that the following properties are eligible for inclusion in the National Register of Historic Places: 1) West North Shore District [sic] (sites #201-598-37001-37020) for its collection of late 19th and early 20th century Revival [sic] residential architecture along with its association to the early planning and development of South Bend; 2) Harter Heights Historic District (sites #201-59/598-35001-35277) for its association with early planning and development of South Bend. The district also has strong architectural integrity and many of the properties were designed by local architectural firms; 3) North Triangle [sic] Historic District (sites #201-598-36001-36290) for its association with early planning and development of South Bend. Also the properties within the district embody a high level of architectural integrity and it includes several properties that were designed by Ernest Young.

Furthermore, we agree that the following properties were listed in the National Register of Historic Places: 1) Leeper Park Historic District (sites #201-598-34001-34018) listed 06-15-2000 (HPR states 1998); 2) Samuel Leeper, Jr. House (site #201-598-37018) listed 03-21-1985; 3) North Pumping Station (site #201-598-37014) listed 01-02-1997 (HPR states 1996)/

Also we agree that St. Joseph County Bridge No. 933 (NBI No. 11046, site #201-598-34018) is an "Outstanding" contributing resource within the Leeper Park Historic District and is the subject of the proposed project. We note that the cover letter and HPR state this bridge is individually eligible as a contributing resource to the Leeper Park Historic District. Since the criteria for contributing resources and individually eligible resources differs, we assumed that the intention was to state that the bridge is a contributing resource to the district and it also meets the criteria to be considered individual [sic] eligible. However, we do agree that the Indiana Historic Bridges Inventory [sic] states that St. Joseph County Bridge No. 933 is a "Contributing resource in a listed historic district."...[sic] "Therefore this bridge will not be reevaluated as part of this inventory project." We also concur that the Indiana Historic Bridges Inventory [sic] determined St. Joseph County Bridge No. 933 to be a Select bridge."

*Based on the information provided to our office, we do not believe the characteristics that qualify West North Shore Historic District, Harter Heights Historic District, North Triangle Historic District [sic], Samuel Leeper, Jr. House or the North Pumping Station for inclusion in the National Register of Historic Places as a result of this project..." (See letter in *Appendix D.*)*

The SHPO's January 20 letter (DHPA #12724) also requested the following information from INDOT:

- *“..Please provide a more detailed account of the bridge’s condition supported by clear photographs...”*;
- *“...Please clarify the scope of work...Additionally, preliminary design plans were provided, though it is unclear if these are the 30% plans. Per the Historic Bridges PA, it is our understanding that rehabilitation plans will be provided to the IN SHPO when the design is approximately 30% complete, 60% complete, and when final design plans are complete...”*;

In addition, the SHPO’s January 20, 2012 stated the following with regard to photo-documentation: *“...Under Attachment B of the Historic Bridges PA and Appendix 1 of the Historic Bridges PA Project Development Process, if a Select bridge is to be rehabilitated, the bridge owner is to consult with the Indiana SHPO to determine if photographic documentation is needed. We believe that due to the significance of St. Joseph Bridge No. 933 and its integrity of design and materials, photographic documentation is needed. We recommend digital photographs be taken of the St. Joseph County Bridge No. 933, in accordance with the “State of Indiana, Indiana DNR-Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards...”* (See letter in Appendix D.)

With regard to archaeological resources, the SHPO’s 20 January, 2012 letter (DHPA #12724) stated the following in part: *“...Based on the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the proposed project area. However, this identification is subject to the project activities remaining in areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the National Register of Historic Places in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered...”* (See letter in Appendix D of this document).

The South Bend Area Genealogical Society responded in a letter dated January 23, 2012, stating in part the following: *“...Relative to your letter of December 21, 2011 and the proposed repairs to the Michigan Street/Leeper Bridge, our Society has reviewed your letter and attached documents and find no objections to the repair...”* (See letter in Appendix D.)

No other invited consulting party responded to the December 21, 2011 letter.

On February 1, 2012, INDOT responded to the SHPO’s January 20, 2012 (DHPA #12724) request for more information, stating in part the following:

- *“...Some indication of the condition of St. Joseph County Bridge No. 933 has been provided. However, please provide a more detailed account of the bridge’s condition supported by clear photographs...”*

As was discussed in a January 26, 2012 phone conversation conducted between DHPA and INDOT staff members, detailed, descriptive information from the bridge inspection report was included in the INDOT early coordination letter mailed to consulting parties on December 21, 2011. This information was and remains the most-specific and up-to-date available on the current conditions of the Leeper ridge and the inherent need for the project. In an effort to augment the previously provided project information, please find enclosed—also per the January 26, 2012 phone conversation—23 bridge inspection photographs that highlight areas of deterioration necessitating the project work as specified under Des. #1173149.

- *“...we note that in the cover letter’s description of the preferred alternative, bullet point number four (4) states that the ‘spandrel walls will be replaced as follows’ and that the subsequent language appears to be repairs instead. We also note that the HPR’s description of the preferred alternative does not include the repair/replacement of the spandrel walls. Please clarify the scope of work...”*

The INDOT LaPorte District and the project consultant have indicated that the provided scope of work contained in the December 21, 2011 early coordination packet was accurate. With reference to bullet point #4, the consultant has confirmed that the spandrel walls will be repaired and that cracks in stone veneer will be sealed with either Jahn mortar or epoxy. The remainder of the scope of work as provided in the December 21, 2011 mailing remains valid.

- *“...Additionally, preliminary design plans were provided, though it is unclear if these are 30% the plans. Per the Historic Bridges PA, it is our understanding that rehabilitation plans will be provided to the Indiana SHPO when the design is approximately 30% complete, 60% complete, and when the final plans are complete...”*

Given the aggressive schedule for this high priority project, the INDOT LaPorte District and the project consultant have provided INDOT-CRO with the final project design plans, which are enclosed in this mailing.

- *“...Under Attachment B of the Historic Bridges PA and Appendix 1 of the Historic Bridges PA Project Development Process, if a Select Bridge is to be rehabilitated, the bridge owner is to consult with the Indiana SHPO to determine if photographic documentation is needed. We believe that due to the significance of St. Joseph County Bridge No. 933 and its integrity of design and materials, photographic documentation is needed. We recommend digital photographs be taken of the St. Joseph County Bridge No. 933, in accordance with the “State of Indiana, Indiana DNR-Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards” (attached)...”*

INDOT-CRO respectfully proposes that required photographic documentation for this project not include the production of photographic documentation for this project not include the production of photographic prints. Rather, and as your office suggested in your January 18, 2011 letter for Des. #0800834 (SR 55 Bridge over Big Pine Creek (DHPA #9939), INDOT proposes that digital photographs/images taken of the SR 933 Bridge over the St. Joseph River (taken in accordance with the “State of Indiana Minimum Architectural Documentation Standards”) be placed into the Indiana Memory collection, with the cooperation of the Indiana State Library. The Indiana SHPO would be provided a compact disc of these images, which would be taken prior to commencement of any project activities as specified under Des. #1173149. Your January 18, 2011 letter (DHPA #9339) referenced a precedent for this documentation method in the case of the US 27 bridge over the Whitewater River in Richmond. INDOT prefers this method of documentation and dissemination of the photographs because it is likely that more people will have easier access to them through the Indiana Memory Project than if the prints are filed in one or both of our offices. (See INDOT letter in *Appendix D.*)

The SHPO responded to INDOT’s February 1, 2012 correspondence in a letter dated February 15, 2012 (DHPA #12724), stating in part the following: *“We agree with INDOT that rather than documenting the St. Joseph County Bridge No. 933 with photographic prints, we recommend that the Indiana Department of Transportation take digital photographs of the St. Joseph County Bridge No. 933, in accordance with the ‘State of Indiana Minimum Architectural Documentation Standards’ and, with the cooperation of the Indiana State Library, place the images into the Indiana Memory Project collection and also provide the SHPO with a compact disc of the images... We are satisfied with the final design plans for the rehabilitation of the SR 933 Bridge carrying Michigan Street over the St. Joseph River. Additionally, based on the information provided to our office, we do not believe that there will be any alterations to the characteristics of the Leeper Park Historic District qualifying it for inclusion in or eligibility for the National Register (see 36 C.F.R. §800.16[i])... (See letter in Appendix D.)*

The Historic Bridge PA requires that a public hearing be held for all projects involving a Select or Non-Select Bridge. Therefore, the public notice requirements regarding INDOT’s APE and “No Adverse Effect” finding will be fulfilled through a public hearing to be held in South Bend, Indiana in March 2012. A 30-day comment period will be given for public comment. This document will be revised, if necessary, after the public comment period has ended to reflect any substantive comments received.

APPENDIX

- A. List of Consulting Parties**
- B. Maps/Plans**
- C. Photos/Tables**
- D. Correspondence/Correspondence Attachments**
- E. Archaeological Report Conclusions**
- F. Historic Property Report (HPR) Conclusions**

Appendix A---List of Consulting Parties

Des. #1173149
 SR 933/Michigan Street
 Bridge Project
 1.59 mile north of SR 23, over the St. Joseph River
 South Bend, Portage Township, St. Joseph County
 Consulting Parties: Those who responded to early coordination are shaded.

Title1	First_Name	Last_Name	Title	Company_Name	Address_Line_1	Address_Line_2	Address_Line_3	City	State	ZIP_Code
Mr.	Robert E.	Carter, Jr.	State Historic Preservation Officer	Indiana Department of Natural Resources	Division of Historic Preservation and Archaeology	Indiana Government Center South, Rm. W274		Indianapolis	IN	46204
Mr.	Todd	Zeiger	Director	Indiana Landmarks/Northern R.O.		402 W. Washington St.		South Bend	IN	46601
Mr.	Kurt	Garner	Co-Chair	Historic Michigan Road Byway Committee	12954 6 th Rd.			Plymouth	IN	46563
Mr.	John	Kovach	St. Joseph County Historian		7982 Potato Hole Ct.			New Carlisle	IN	46552-9062
Ms.	Sandi	Seanor	Ex. Dir.	Michiana Area Council of Governments (MACOG)	227 W. Jefferson Blvd., Rm. 1120			South Bend	IN	46601
Mr.	Randy	Ray	Director	Center for History	808 W. Washington			South Bend	IN	46601-1439
Ms.	Peggy	Marker	Director	Hannah Lindahl Children's Museum	1402 S. Main St.			Mishawaka	IN	46544-5241
Mr.	Roger	Birdsell	Ex. Director	Michiana Jewish Historical Society	3202 Shalom Way			South Bend	IN	46615-2955
Mr.	Paul	Brandenburg	Chair	Indiana Historic Spans Task Force	5868 Croton Cir.			Indpls.	IN	46254
Dr.	James	L.	Cooper		629 Seminary St			Greencastle	IN	46135

Des. #1173149
 SR 933/Michigan Street
 Bridge Project
 1.59 mile north of SR 23, over the St. Joseph River
 South Bend, Portage Township, St. Joseph County
 Consulting Parties: Those who responded to early coordination are shaded

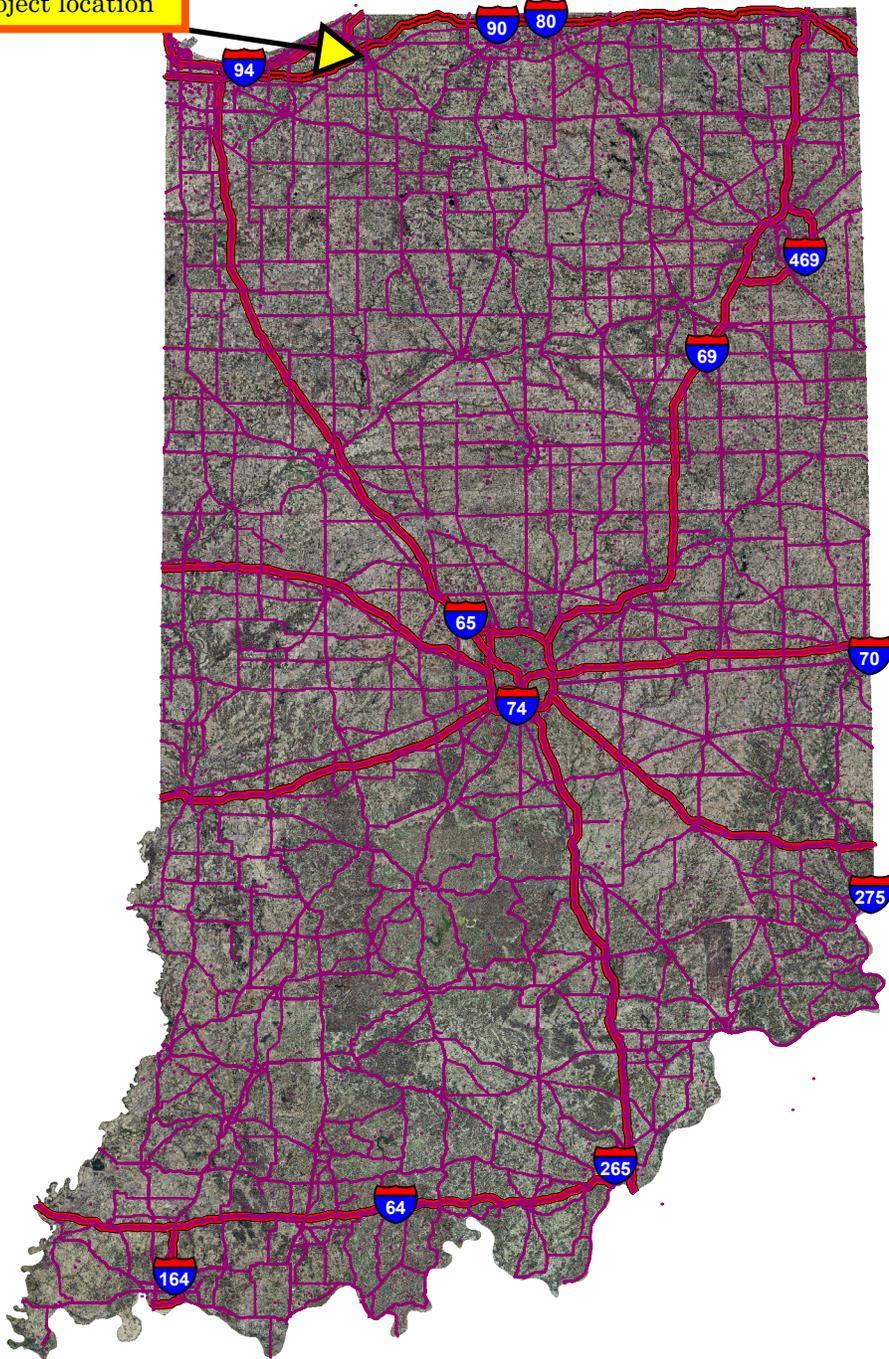
Title1	First_Name	Last_Name	Title	Company_Name	Address_Line_1	Address_Line_2	Address_Line_3	City	State	ZIP_Code
Director				Mishawaka-Penn-Harris Public Library	209 Lincolnway E			Mishawaka	IN	46544-2084
Hon.	Stephen	Luecke	Mayor	City of South Bend	227 W. Jefferson Blvd., Ste. 1400 N			South Bend	IN	46601
Mr.	William	Minish	President	South Bend Area Genealogical Society	209 Lincolnway E			Mishawaka	IN	46544-2014
Ms.	Catherine	Hostetler	Director	Historic Pres. Commission of South Bend & St. Joseph Co.	125 S. Lafayette Blvd.			South Bend	IN	46601
	St. Joseph County Comm.				227 W. Jefferson Blvd.			South Bend	IN	46601
Ms.	Jan	Shupert-Arick	President	IN Lincoln Hwy. Assoc., Inc.	402 W. Washington			South Bend	IN	46601
Mr.	Aaron	Perri	Ex. Dir.	Downtown South Bend, Inc.	205 N. Main St.			South Bend	IN	46601
Mr.	Marco	Mariani	Ex. Dir.	South Bend Heritage Fdn., Inc.	914 Lincolnway West.			South Bend	IN	46601

|

Appendix B---Maps/Plans

Des. #1173149
 Bridge Project
 SR 933/Michigan Street over St. Joseph River
 1.59 mile north of SR 23
 South Bend, St. Joseph County

Approximate project location



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

Scale 1:2,469,308 Miles
 30 15 0 30

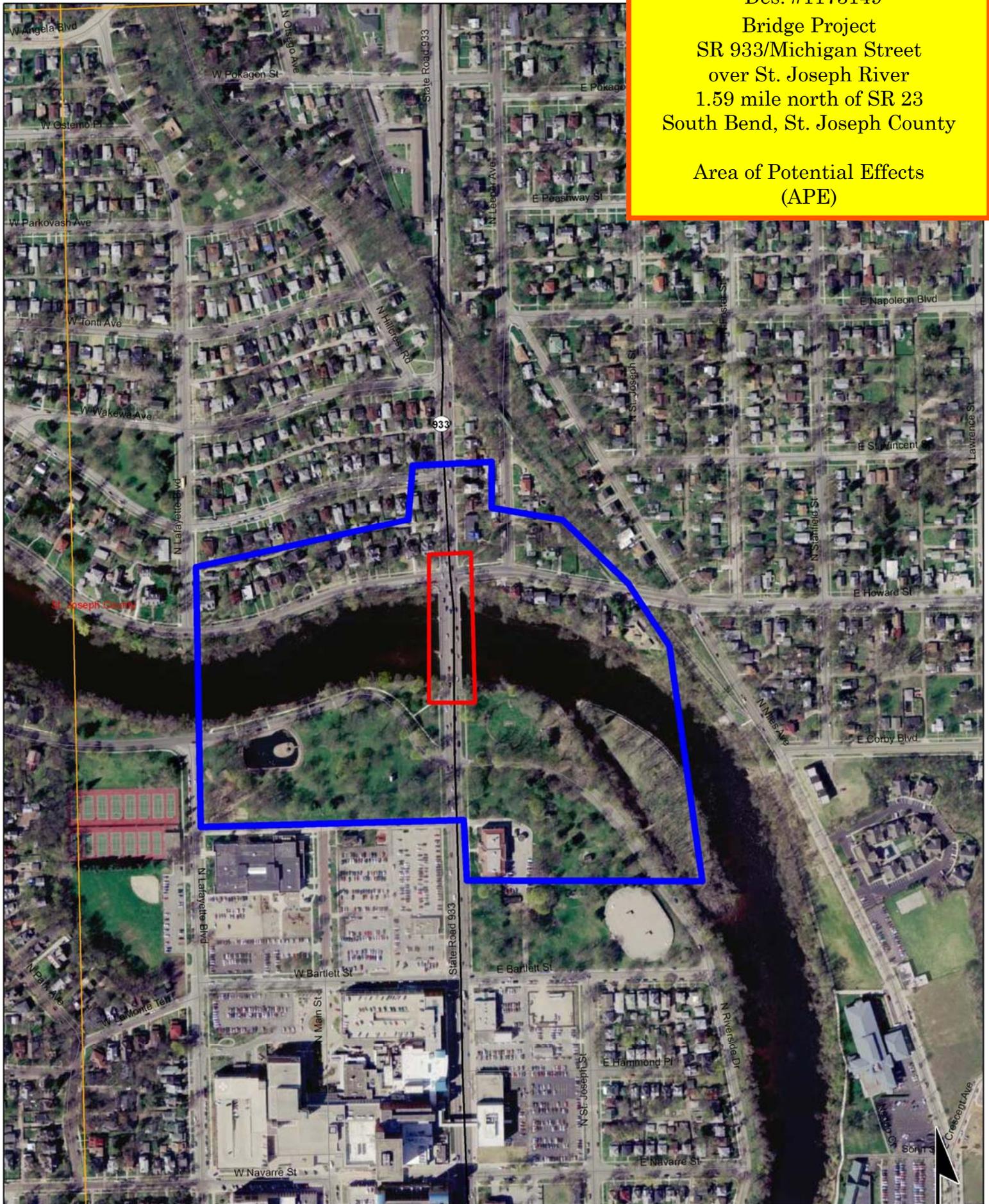
	Section Town and Range		Interstate
	County Boundary		State Route
	Local Road		US Route

B-2

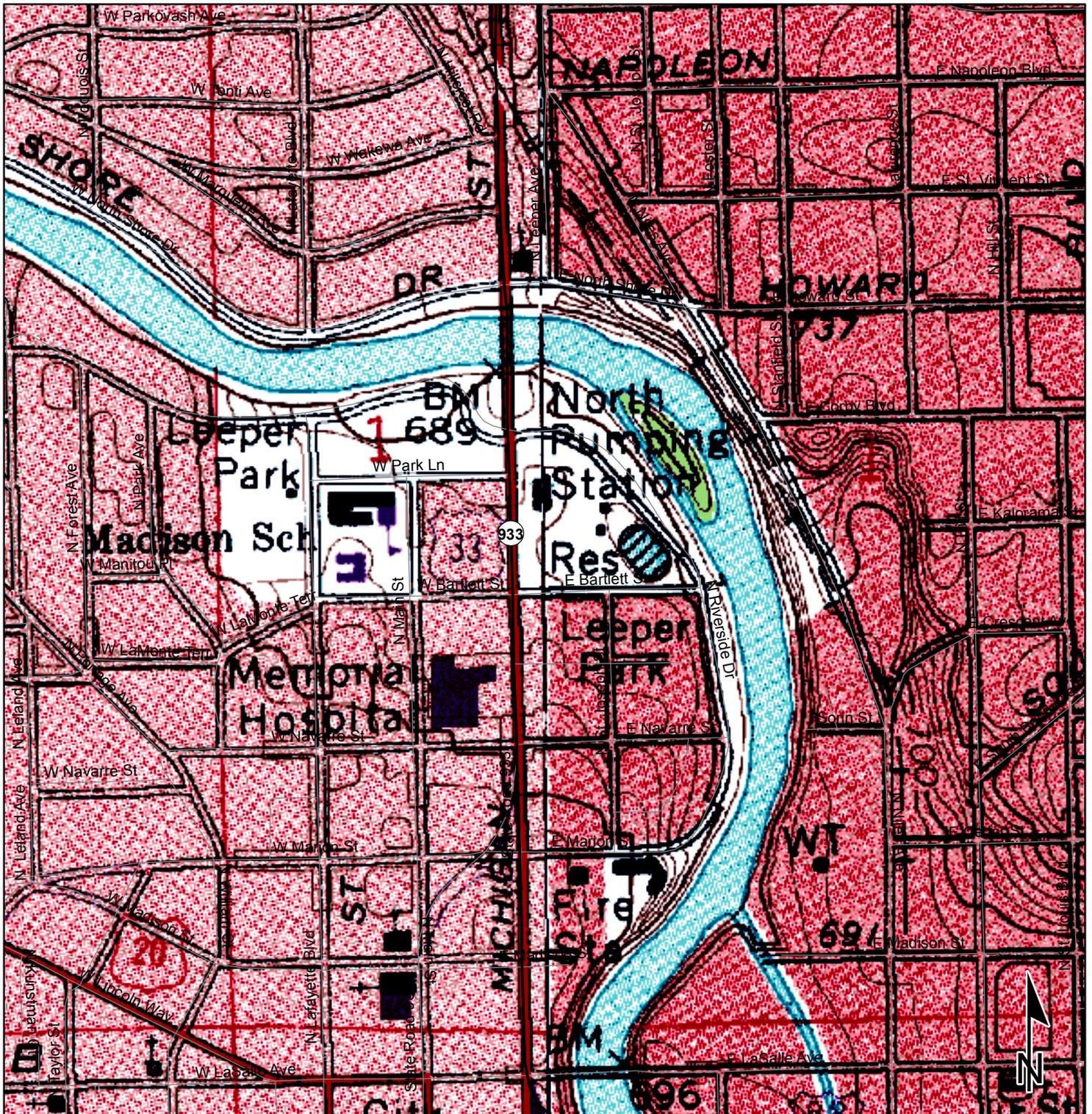
D-19

Des. #1173149
Bridge Project
SR 933/Michigan Street
over St. Joseph River
1.59 mile north of SR 23
South Bend, St. Joseph County

Area of Potential Effects
(APE)



1.59 mile north of SR 23
 SR 933/Michigan Street, South Bend, St. Joseph Co., IN
 Des. #1173149/Bridge Project over St. Joseph River



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources: Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

B-4

Scale 1:7,455 Miles
 0.1 0.05 0 0.1

	Section Town and Range		Interstate
	County Boundary		State Route
	Local Road		US Route

D-21

Des. #1173149
 SR 933/Michigan Street Bridge over
 St. Joseph River
 South Bend, Portage Township, Indiana

Final Design Plans

DESIGNATION
1173149
BRIDGE FILE
(933)31-71-3690C

STRUCTURE	TYPE	SPAN AND SKEW	OVER	± STATION
(933)31-71-3690C	REINFORCED CONCRETE ARCH	84'-0" 120'-0" 84'-0" NO SKEW	ST. JOSEPH RIVER	16+16 "C"

INDIANA DEPARTMENT OF TRANSPORTATION



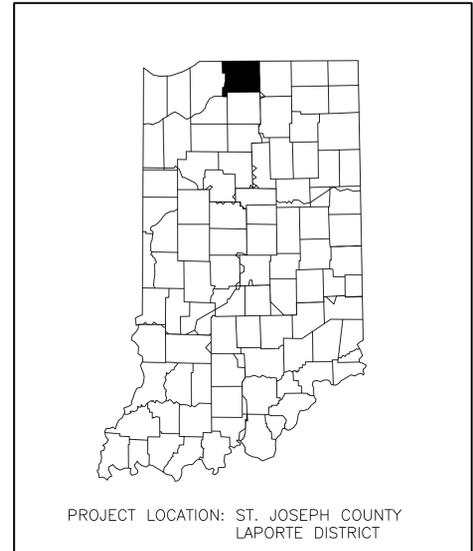
BRIDGE PLANS

FOR SPANS OVER 20 FEET

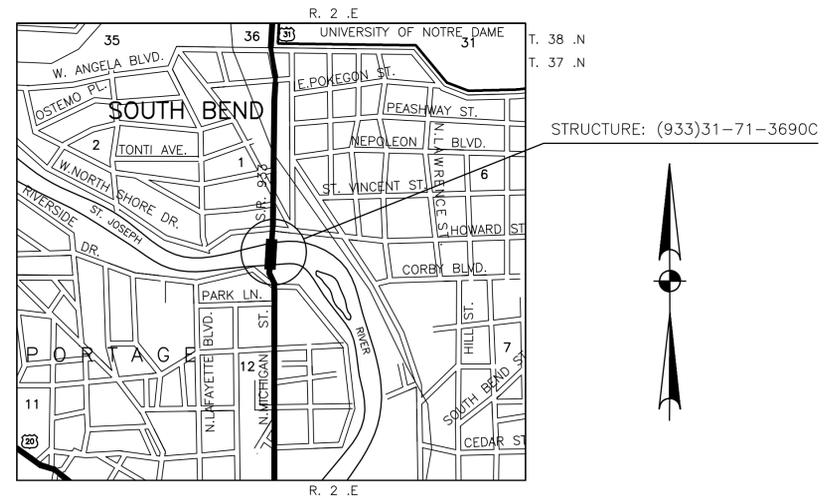
PROJECT NO. BR-230-6(002)

MISCELLANEOUS MAINTENANCE AND IMPROVEMENTS ON STRUCTURE: (933)31-71-3690C, S.R. 933 (MICHIGAN STREET) OVER ST. JOSEPH RIVER LOCATED IN SECTION 1, T.37 N., R.2 E., PORTAGE TOWNSHIP, ST. JOSEPH COUNTY, INDIANA.

TRAFFIC DATA		
A.A.D.T. (2001)	25910	V.P.D.
A.A.D.T. (2032)	49370	V.P.D.
D.H.V. (2032)	4937	V.P.H.
DIRECTIONAL DISTRIBUTION	50	%
TRUCKS	5	% A.A.D.T.
		% D.H.V.
EQUIVALENT SINGLE AXLE LOADS		
DESIGN DATA		
DESIGN SPEED	35	M.P.H.
PROJECT DESIGN CRITERIA	3R NON-FREEWAY	
FUNCTIONAL CLASSIFICATION	PRINCIPAL ARTERIAL	
RURAL/URBAN	URBAN	
TERRAIN	LEVEL	
ACCESS CONTROL	NONE	



LATITUDE: 41°41'17"N LONGITUDE: 86°15'02"W



LOCATION IN ST. JOSEPH COUNTY
Not to Scale

U:\15298\Pro\Development\Design\Drawings\5298B701.dwg Donald Sheetz Plot: 12/19/2011 2:39 PM Save: 12/14/2011 1:33 PM

INDIANA DEPARTMENT OF TRANSPORTATION
STANDARD SPECIFICATIONS DATED 2012
TO BE USED WITH THESE PLANS

BFS
Butler Fairman Seufert
CIVIL ENGINEERS

Headquarters
8450 WESTFIELD BLVD., SUITE 300
INDIANAPOLIS, IN 46240-8302
TEL 317-713-4616
FAX 317-713-4616
www.BFSEngr.com

Branch Locations
FORT WAYNE 260-459-1532
JEFFERSONVILLE 812-285-0590
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

FEDERAL HIGHWAY ADMINISTRATION
U.S. DEPT. OF TRANSPORTATION

APPROVED: _____ DATE _____

DIVISION ADMINISTRATOR

PLANS
PREPARED BY: Butler, Fairman & Seufert (317) 713-4615
PHONE NUMBER

CERTIFIED BY: _____ DATE _____

APPROVED FOR LETTING: _____ DATE _____

INDIANA DEPARTMENT OF TRANSPORTATION

B-5

FILE
(933)31-71-3690C
DESIGNATION
1173149
SHEET
1 OF 27
PROJECT
BR-230-6(002)

CONTRACT

BFS NO. 5298

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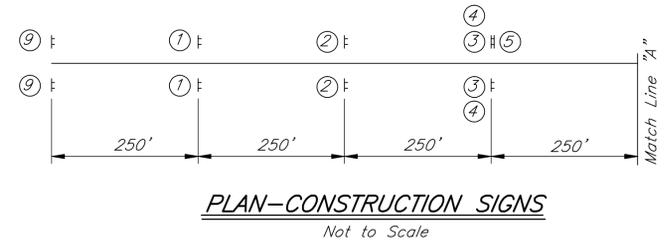
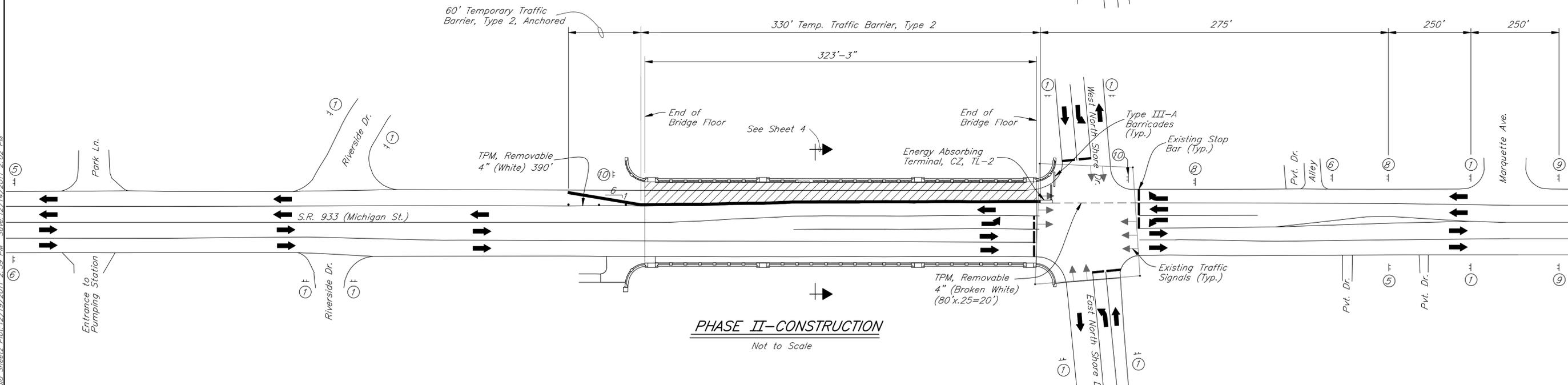
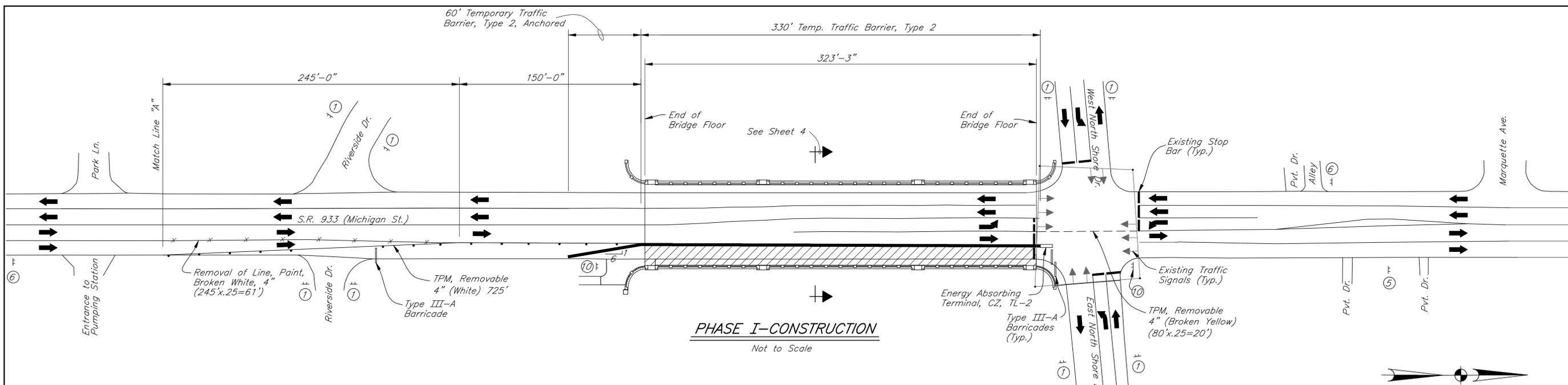
UTILITIES	
<i>GAS:</i>	<i>NipSCO 1039 E. Pennsylvania Ave. South Bend, In 46618 (574) 284-2277</i>
<i>ELECTRIC:</i>	<i>American Electric Power 2929 W. Lathrop St. South Bend, In 46628 (574) 283-1819</i>
<i>SANITARY SEWER/WATER:</i>	<i>City Of South Bend Div. Of Public Works Engineering Dept. 224 N. Main St. South Bend, In 46601 (574) 235-9251</i>
<i>TELEPHONE:</i>	<i>SBC 307 S. Main St. South Bend, In 46307 (574) 237-8380</i> <i>Sprint Communications 5600 N. River Rd., Suite 500 Rosemont, IL 60018 (847) 318-3192</i>
<i>NOTE: There may be other utilities other than these that are listed that were not notified by underground Indiana, and no evidence was discovered as to their identity.</i>	

INDEX	
SHEET NUMBER	SUBJECT
1	TITLE SHEET
2	INDEX SHEET
3-4	TRAFFIC MAINTENANCE DETAILS
5	GENERAL PLAN
6	TYPICAL SECTIONS
7	MISCELLANEOUS DETAILS
8-13	CRACK MAPPING AND FRP STRIPS
14-19	RAILING DETAILS SHOWING REMOVALS AND RAILING POST REHABILITATION
20-25	RAILING DETAILS SHOWING RECONSTRUCTION
26	RAILING DETAILS
27	BRIDGE SUMMARY

REVISIONS		
SHEET NO.	DATE	REVISED

 Butler Fairman Souffere <small>civilians</small>	<p style="text-align: center;">B-6</p>	RECOMMENDED FOR APPROVAL: _____	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	FILE
		DESIGNED: <u>D. SHEETZ</u> DRAWN: <u>D. SHEETZ</u>			NONE	(933)31-71-3690C
		CHECKED: <u>M. EICHENAUER</u> CHECKED: <u>M. EICHENAUER</u>	INDEX SHEET		VERTICAL SCALE	DESIGNATION
<small>Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSinc.com</small>	<small>Branch Locations FORT WAYNE 260-459-1532 JEFFERSONVILLE 812-285-0590 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242</small>			NONE	1173149	
					SURVEY BOOK	SHEET
					CONTRACT	PROJECT
						BR-230-6(002)

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PLAN-CONSTRUCTION SIGNS
Not to Scale

SIGN LEGEND

- ① XW20-1 Road Construction Ahead
- ② XW20-5(R) Right Lane Closed Ahead
- ③ XW4-2-A(L) Merge
- ④ XW13-1-A Speed Advisory
- ⑤ XG20-2 End Construction
- ⑥ XG20-5 Lane Closure Notice Sign
- ⑦ XW1-4-A(L) Reverse Curve
- ⑧ R3-7 Right Lane Must Turn Right
- ⑨ XG20-7a Speeding/Reckless Driving Sign
- ⑩ R9-9 Sidewalk Closed

LEGEND

- TPM=Temporary Pavement Marking (Type, Color and Width as Noted)
- Indicates Drum with Type "C" Steady Burning Light (Not a Pay Item) (Typ.)
- Temporary Traffic Barrier, Type 2
- * * * Indicates Line Removal
- ▨ Construction Area

NOTES
 Signs shall be placed as shown on the Plans or as directed by the Engineer.
 All Construction Signs to have Low Intensity Flashing Yellow Light, Type "A".

BFS
 Butler Fairman Souffert
 CIVIL ENGINEERS

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 8450 WESTFIELD BLVD., SUITE 300
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 TEL 317-713-4615
 FAX 317-713-4616
 www.BFSeng.com

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 JEFFERSONVILLE 812-286-0590
 LAFAYETTE 765-423-5602
 MERRILLVILLE 219-769-2333
 PLAINFIELD 317-839-3242

B-7

RECOMMENDED FOR APPROVAL: _____ DATE _____
 DESIGN ENGINEER

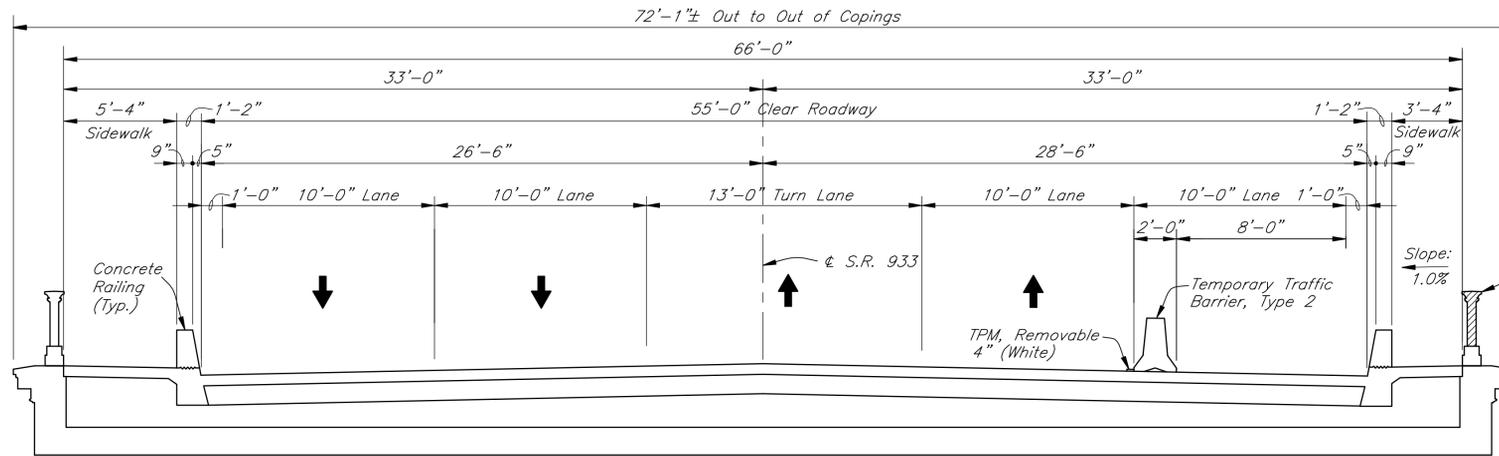
DESIGNED: D. SHEETZ DRAWN: D. SHEETZ
 CHECKED: M. EICHENAUER CHECKED: M. EICHENAUER

INDIANA DEPARTMENT OF TRANSPORTATION

TRAFFIC MAINTENANCE DETAILS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	3 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

SHEET NO. 5298



* Remove Concrete Railing Panels (Cost to be included in the Lump Sum item "Present Structure, Remove Portions") (See Sheets 14 thru 19)

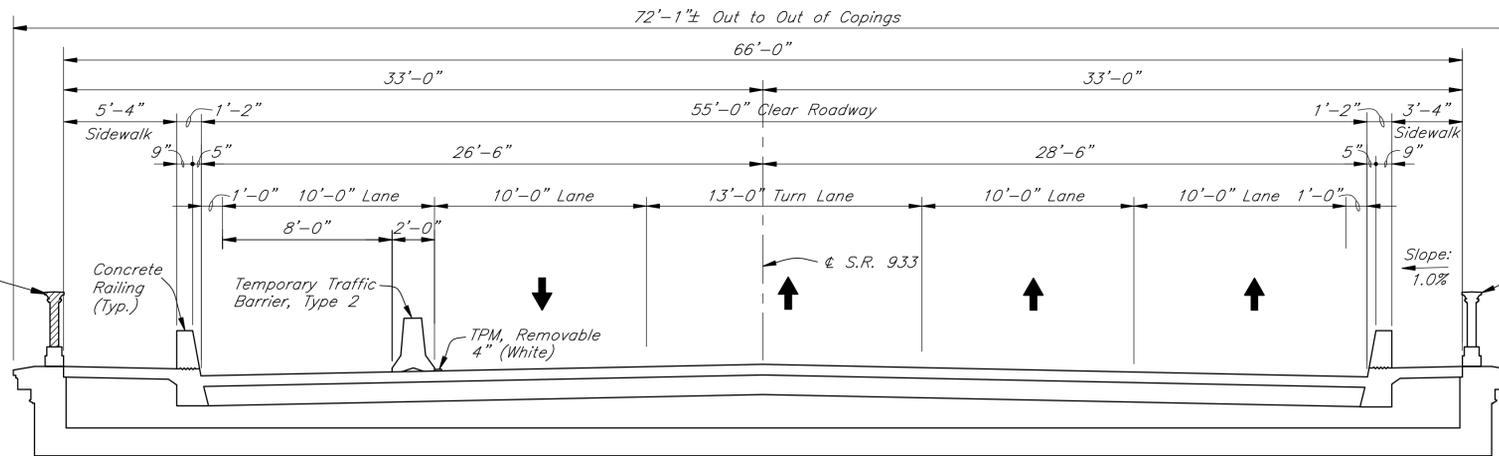
* All Major Posts are to remain in place
All Minor Posts are to have Stone Removed and Reset/Replumbed on New Mortar Bed unless noted otherwise

PHASE I

Scale: 1/4" = 1'-0"

Note: Hatched Areas Indicate Portions to be Removed.

ESTIMATED QUANTITIES		
PAY ITEM	UNIT	TOTAL
Maintaining Traffic	L.S.	1
Temporary Traffic Barrier, Type 2	LFT.	330
Temporary Traffic Barrier, Type 2, Anchored	LFT.	60
Construction Signs, Type A	EA.	20
Construction Signs, Type B	EA.	1
Construction Signs, Type C	EA.	2
Temporary Pavement Marking, Removable, 4" (White)	LFT.	1115
Temporary Pavement Marking, Removable, 4" (Broken White)	LFT.	40
Removal of Line, Paint, Broken, 4"	LFT.	61
Line, Multi-component, Broken, White, 4"	LFT.	61
Standard Barricade, Type III-A	LFT.	36
Energy Absorbing Terminal, CZ, TL-2	EA.	1



* Remove Concrete Railing Panels (Cost to be included in the Lump Sum item "Present Structure, Remove Portions") (See Sheets 14 thru 19)

* New Concrete Railing Panels (See Sheets 20 thru 26)

PHASE II

Scale: 1/4" = 1'-0"

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BFS
Butler Fairman Soufer
civil engineers

Headquarters
8450 WESTFIELD BLVD., SUITE 300
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FAX 317-713-4616
www.BFSeng.com

Branch Locations
FORT WAYNE 260-459-1532
JEFFERSONVILLE 812-286-0590
LAFAYETTE 765-423-5602
MERRILLVILLE 219-769-2333
PLAINFIELD 317-839-3242

B-8

RECOMMENDED FOR APPROVAL: _____
DESIGN ENGINEER DATE

DESIGNED: S. WEINTRAUT DRAWN: D. SHEETZ

CHECKED: M. EICHENAUER CHECKED: M. EICHENAUER

INDIANA DEPARTMENT OF TRANSPORTATION

TRAFFIC MAINTENANCE DETAILS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	4 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

STRUCTURE IS BUILT ON 5-100' VERTICAL CURVES

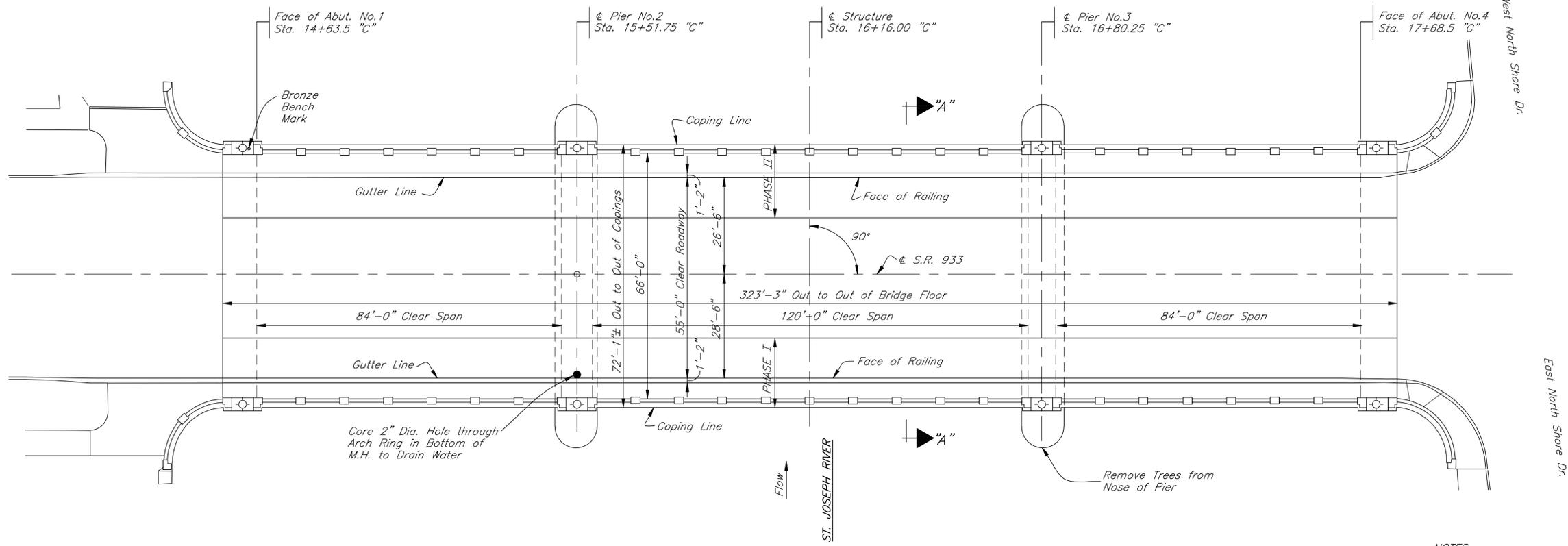
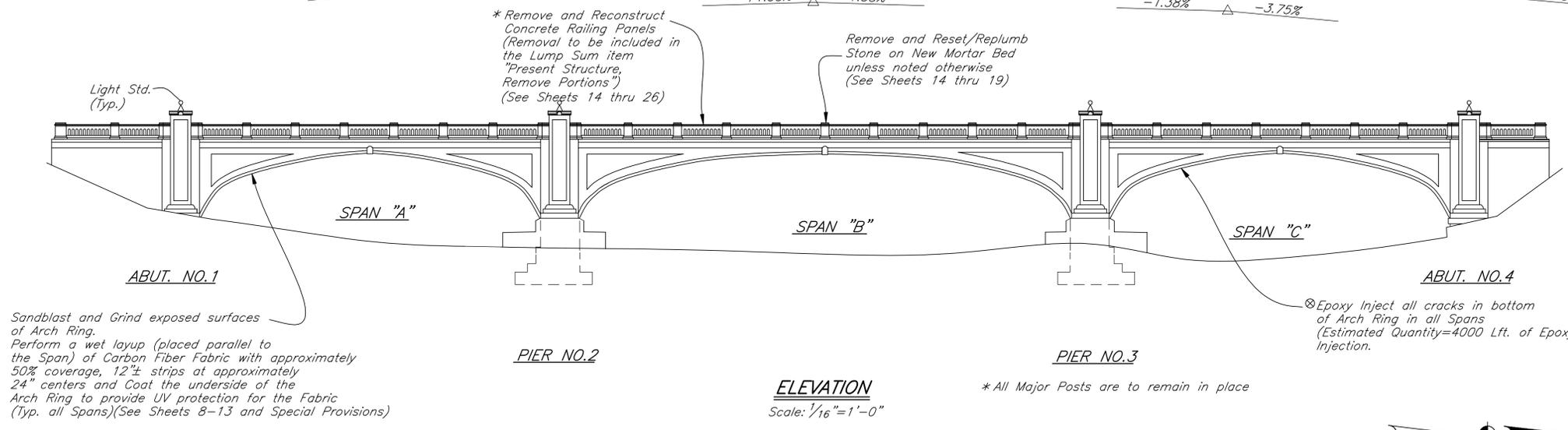
P.I. Sta. 14+16.00 "C"
El. 683.62
V.C.=100'
+2.45% +4.40%

P.I. Sta. 15+16.00 "C"
El. 688.02
V.C.=100'
+4.40% +1.38%

P.I. Sta. 16+16.00 "C"
El. 689.40
V.C.=100'
+1.38% -1.38%

P.I. Sta. 17+16.00 "C"
El. 688.02
V.C.=100'
-1.38% -3.75%

P.I. Sta. 18+16.00
El. 684.27
V.C.=100'
-3.75% -1.80%



NOTES
See Sheet 6 for Section "A-A" and General Notes.
See Sheet 7 for Spandrel Wall Repairs.

REINFORCED CONCRETE ARCH BRIDGE
3 SPANS: 84'-0", 120'-0" AND 84'-0", NO SKEW, 55'-0" CLEAR ROADWAY ON STATE ROAD 933 OVER ST. JOSEPH RIVER.

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GENERAL PLAN

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VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	5 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

GENERAL NOTES

Plans for the structure as originally built are not available. Rehabilitation Plans, Contract B-27194 are available upon request from INDOT.

Inspection Reports completed in 2002 and 2011 are available upon request from INDOT.

Portions of present structure shall be removed.

Where new work is to be fitted to old work, the Contractor shall check all dimensions and conditions in the field and report any errors or discrepancies to the Engineer and assume responsibility for their correctness and the fit of the new part to the old.

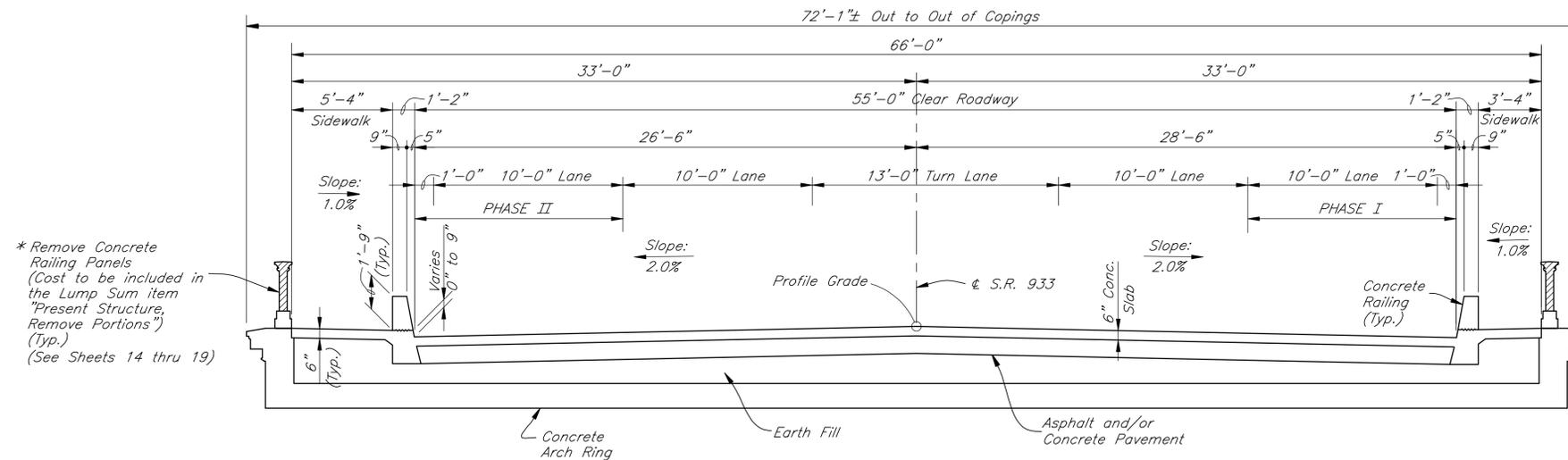
Epoxy coated reinforcing steel shall be required in various portions of the structure as shown.

Reinforcing steel covering shall be 2" in all areas unless noted.

Reinforcing steel shall be A.S.T.M. A615, Grade 60.

Concrete shall be Class C in railings.

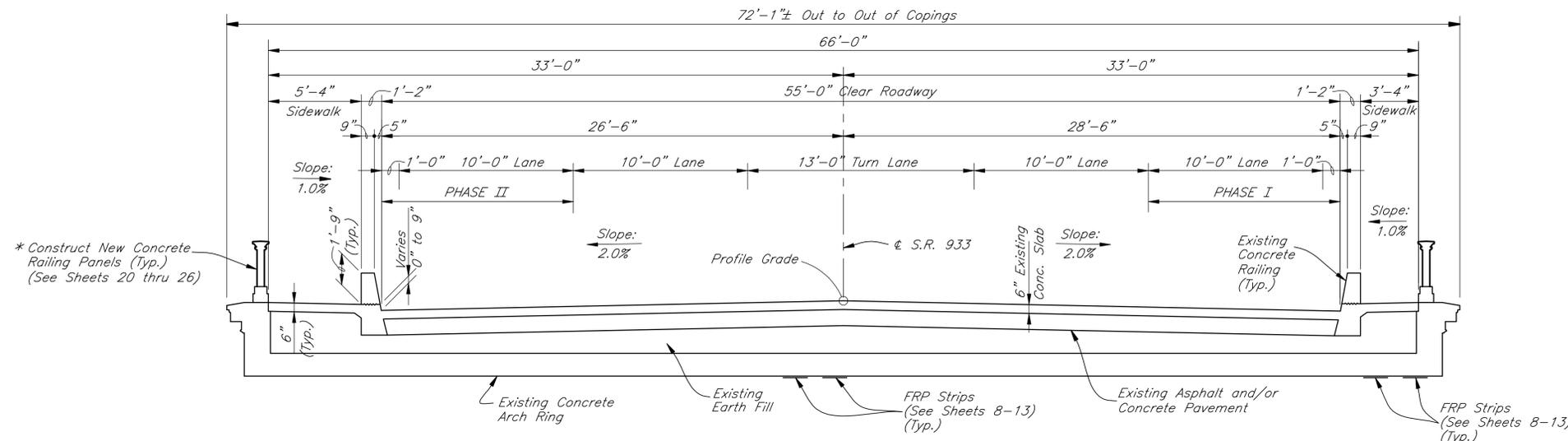
Chamfer exposed corners of concrete 1/2" unless noted.



SECTION "A-A"
EXISTING

Scale: 1/4" = 1'-0"

* All Major Posts are to remain in place
All Minor Posts are to have Stone Removed
and Reset/Replumbed on New Mortar Bed
unless noted otherwise



SECTION "A-A"
PROPOSED

Scale: 1/4" = 1'-0"

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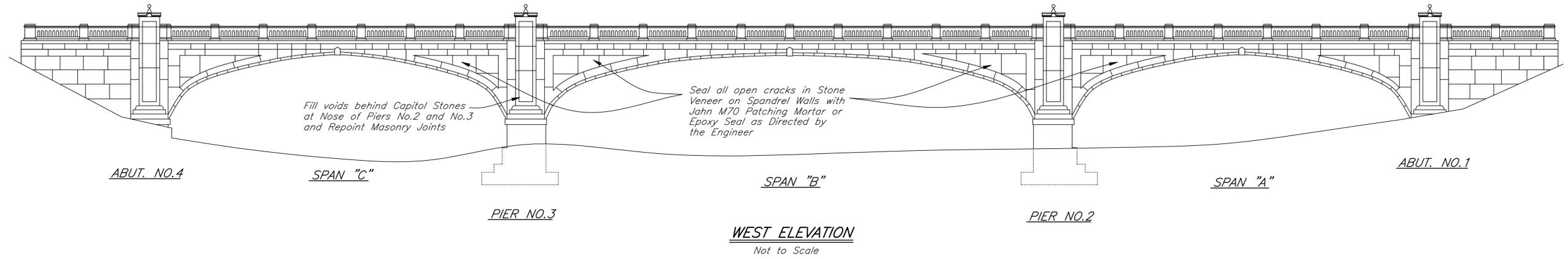
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GENERAL PLAN

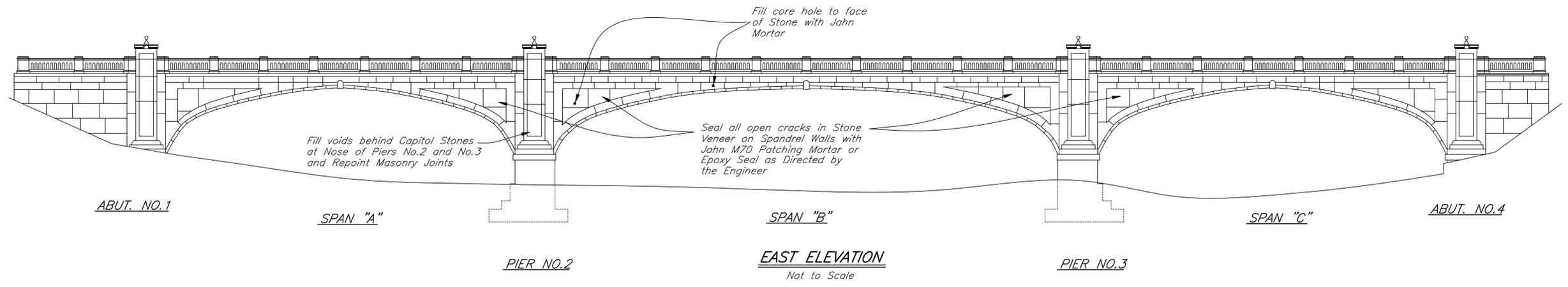
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VERTICAL SCALE	DESIGNATION
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Note: Repoint Masonry Joints on Spandrel Walls as required by the Engineer.



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B-11

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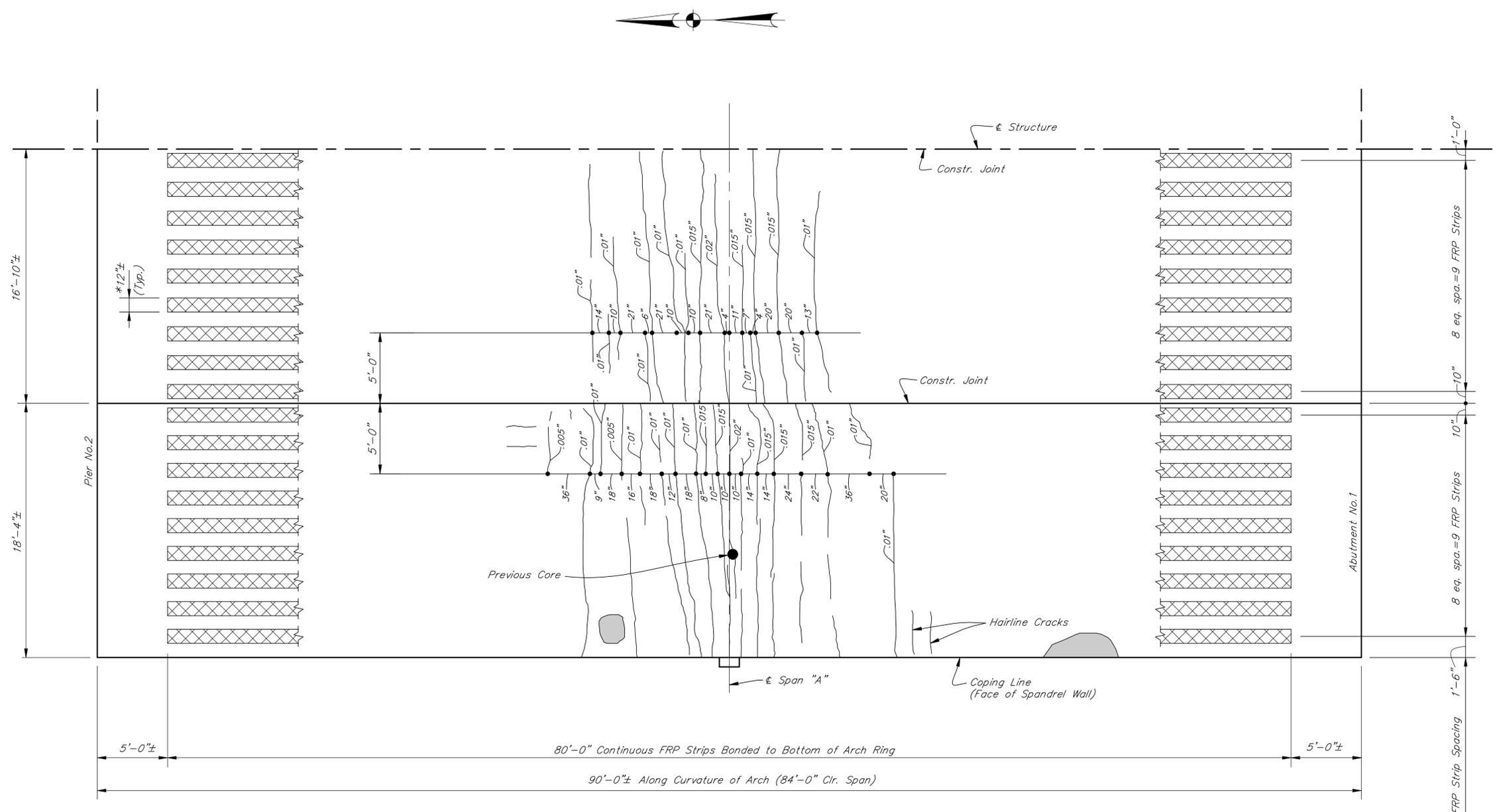
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MISCELLANEOUS DETAILS

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VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	7 OF 27
CONTRACT	PROJECT
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- *Single Ply FRP Strip
 - Existing Patching, Do not Remove Unless Delaminated
 - Concrete Core taken during Inspection. Hole to be filled to exposed surface of Arch Ring with Dry Pack Concrete.
- *See Special Provisions

CRACK MAPPING AND FRP STRIP PLAN
SPAN "A" - WEST HALF
 Not to Scale

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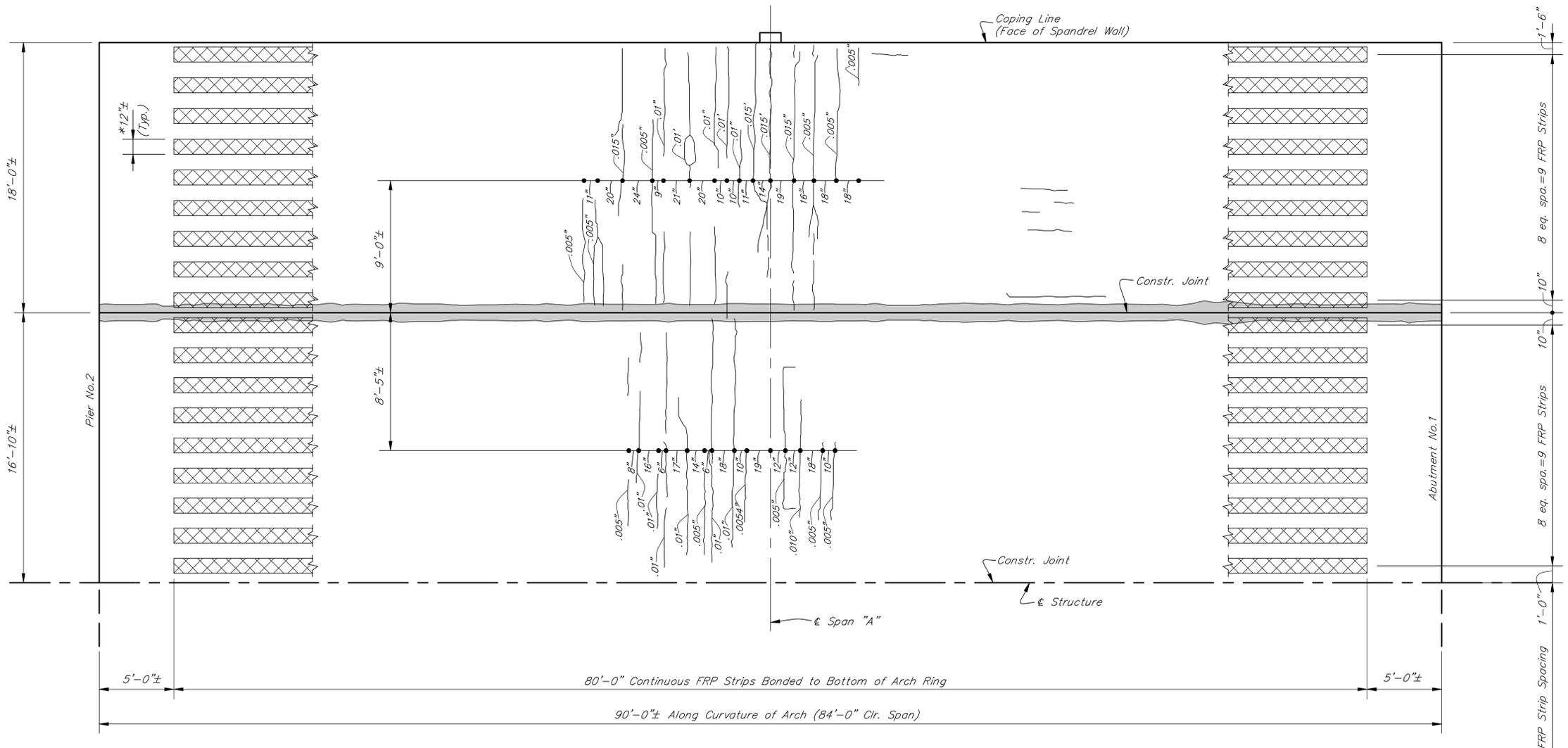
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CRACK MAPPING AND FRP STRIPS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
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	8 OF 27
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CRACK MAPPING AND FRP STRIP PLAN
SPAN "A" - EAST HALF
 Not to Scale

*Single Ply FRP Strip
 Existing Patching, Do not Remove Unless Delaminated

* See Special Provisions

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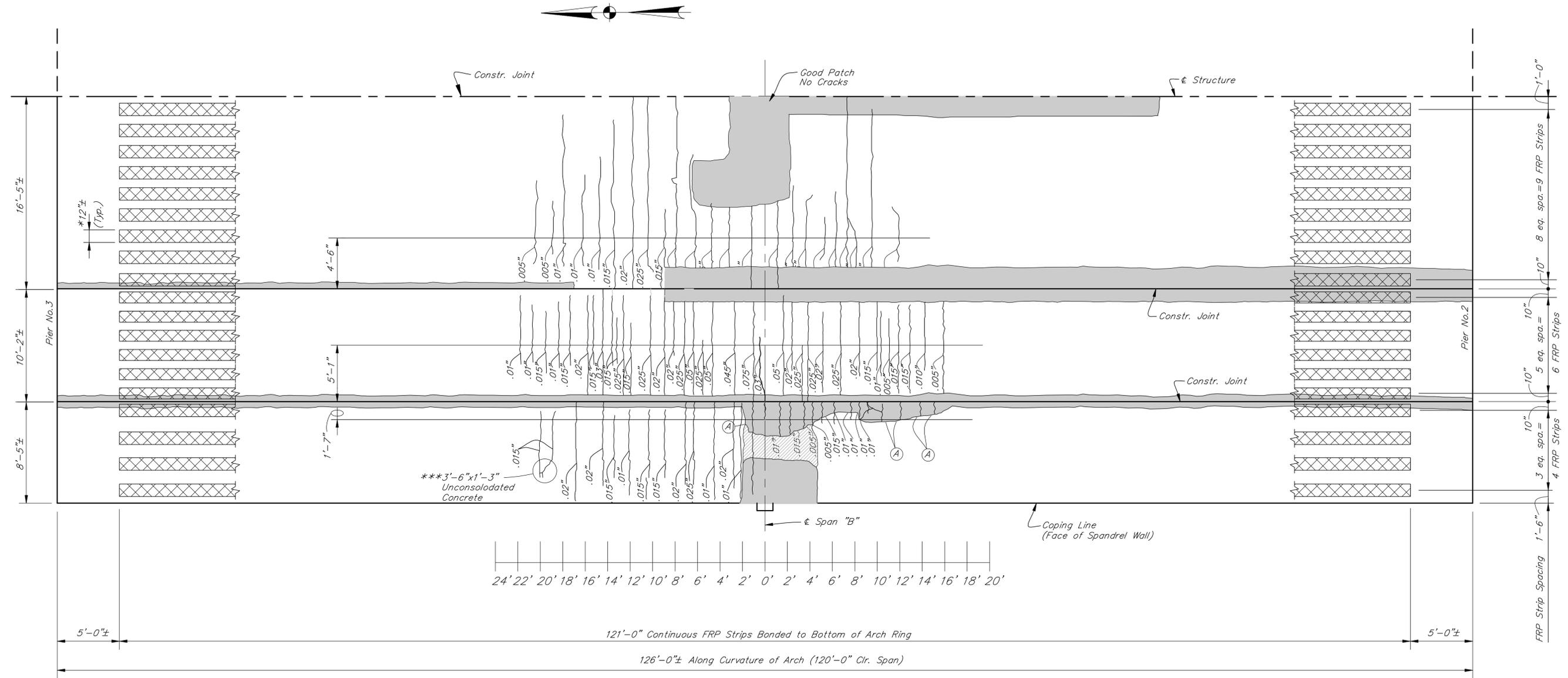
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CRACK MAPPING AND FRP STRIPS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
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* See Special Provisions
 ** Remove prior to application of FRP
 *** Fill and Smooth prior to Installation of FRP

CRACK MAPPING AND FRP STRIP PLAN SPAN "B" - WEST HALF Not to Scale

- *Single Ply FRP Strip
- Existing Patching, Do not Remove Unless Delaminated
- **Concrete Overspray from Patching
- (A) = Leaching Cracks, Unable to Measure

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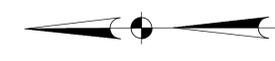
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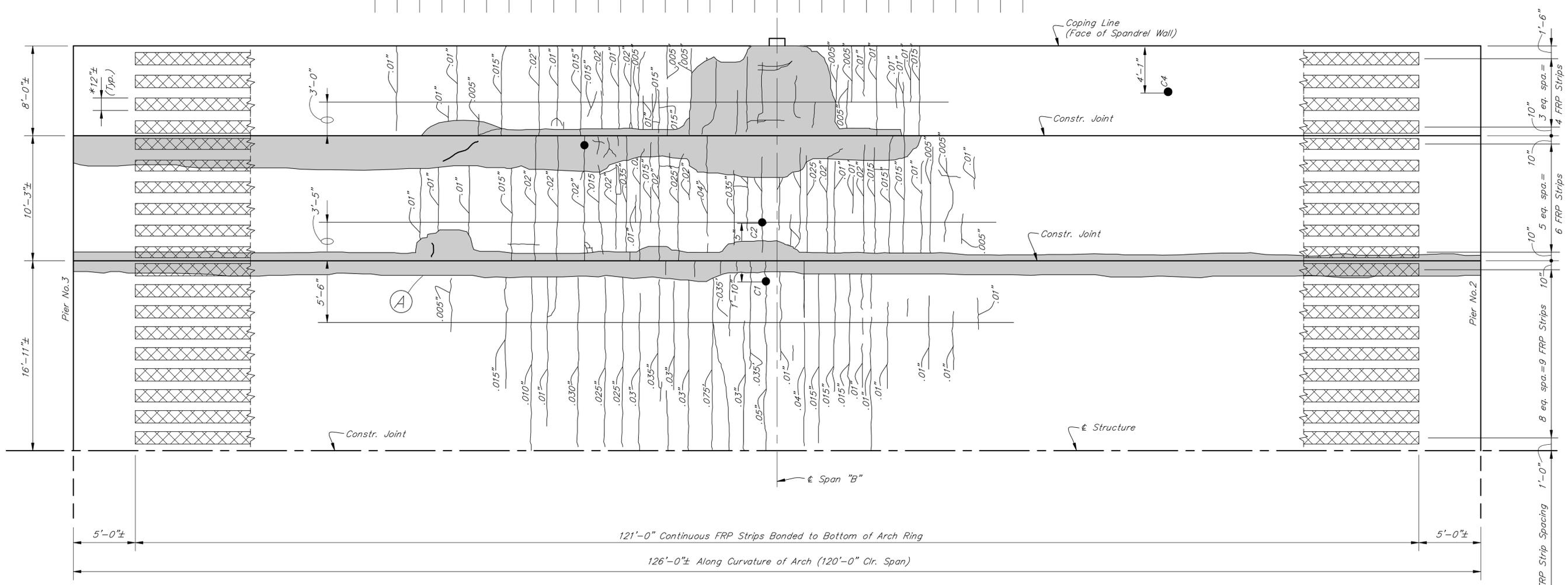
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CRACK MAPPING AND FRP STRIPS	

HORIZONTAL SCALE	FILE
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VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
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CONTRACT	PROJECT
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36' 34' 32' 30' 28' 26' 24' 22' 20' 18' 16' 14' 12' 10' 8' 6' 4' 2' 0' 2' 4' 6' 8' 10' 12' 14' 16' 18' 20' 22'



CRACK MAPPING AND FRP STRIP PLAN
SPAN "B" - EAST HALF
 Not to Scale

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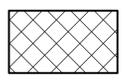
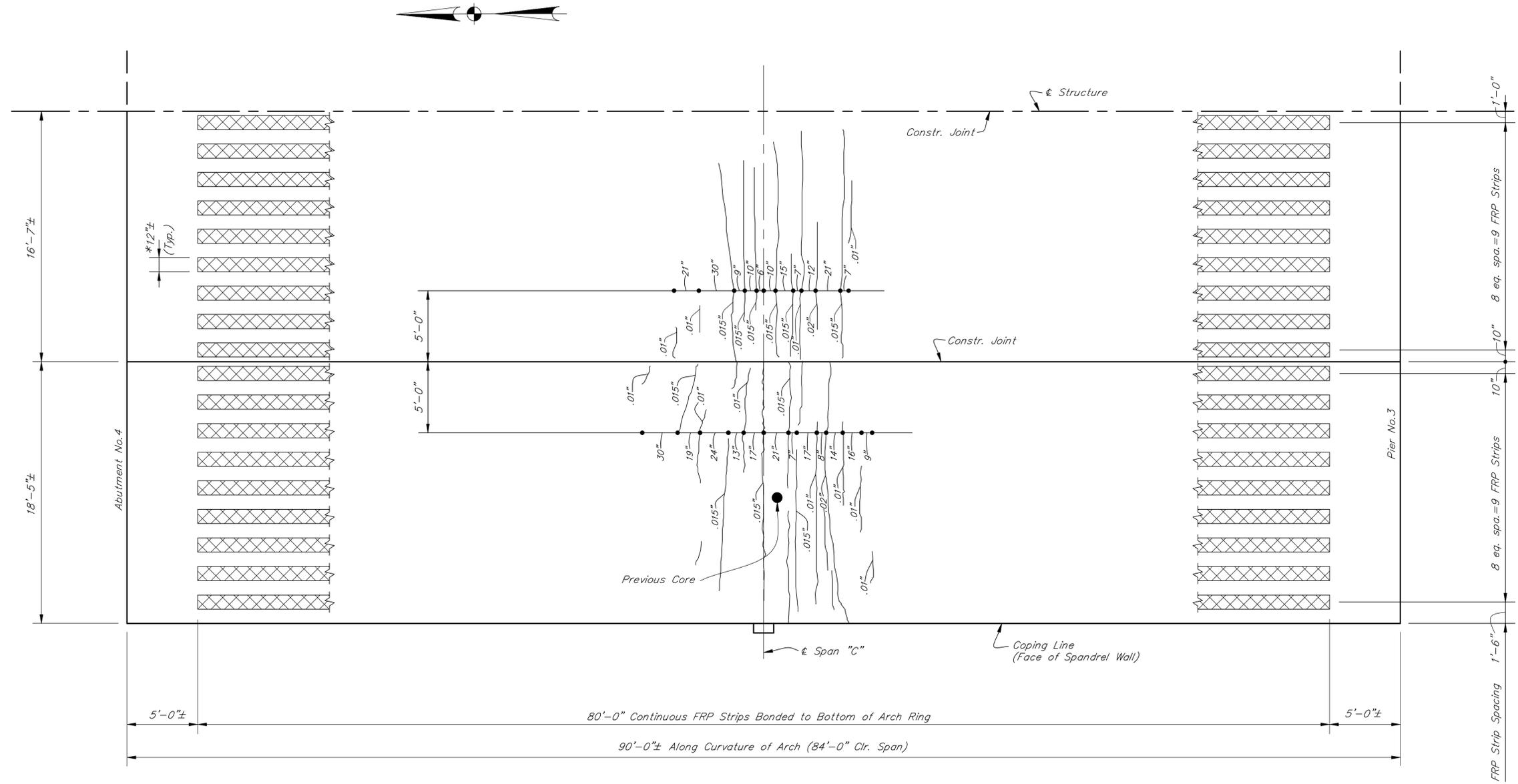
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VERTICAL SCALE AS NOTED	DESIGNATION 1173149
SURVEY BOOK	SHEET
	11 OF 27
CONTRACT	PROJECT
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HORIZONTAL SCALE AS NOTED	FILE (933)31-71-3690C
VERTICAL SCALE AS NOTED	DESIGNATION 1173149
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*Single Ply FRP Strip

* See Special Provisions

● Concrete Core taken during Inspection. Hole to be filled to exposed surface of Arch Ring with Dry Pack Concrete.

CRACK MAPPING AND FRP STRIP PLAN
SPAN "C" - WEST HALF
 Not to Scale

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B-16

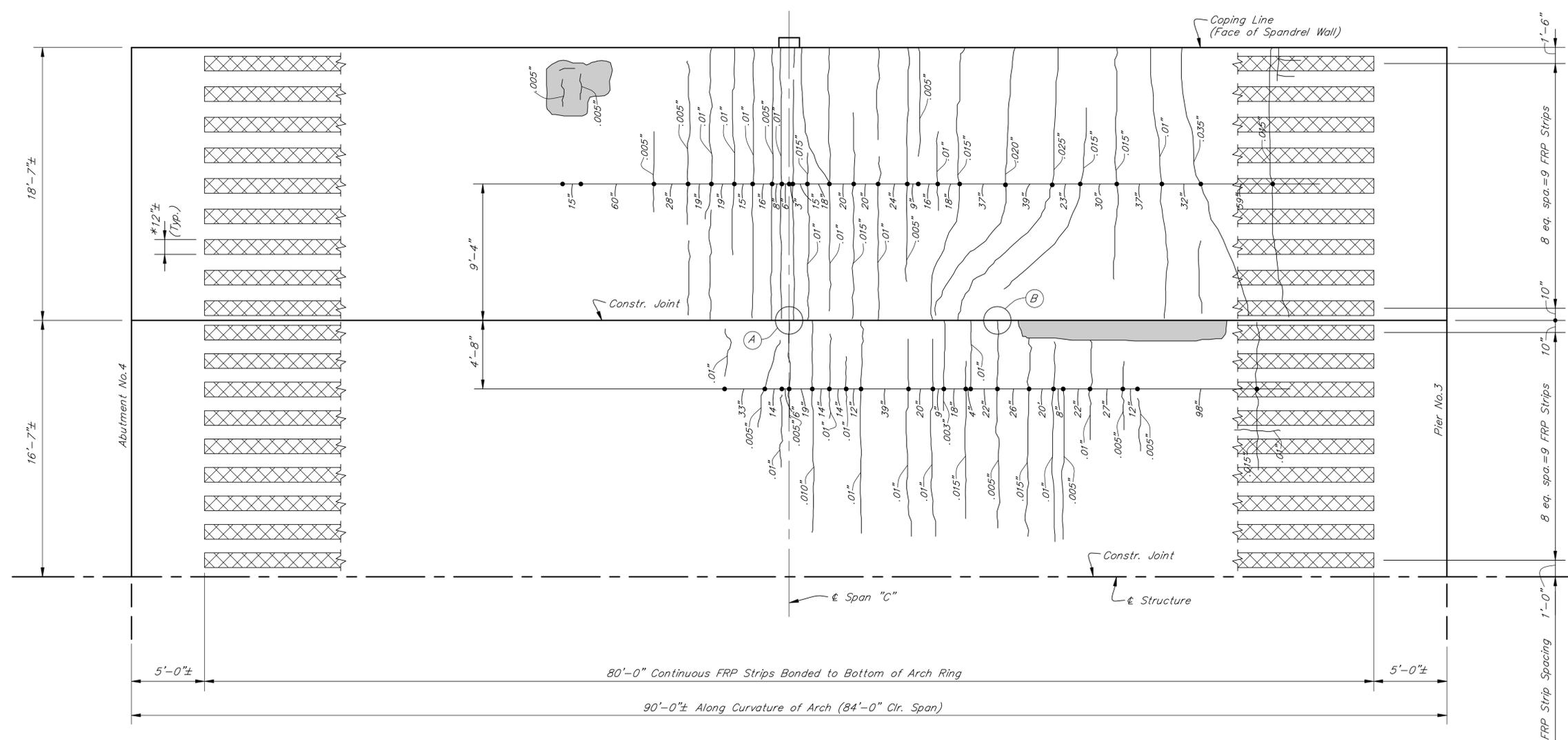
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 CRACK MAPPING AND FRP STRIPS

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- (A) East side of construction joint shifted down $\frac{3}{16}$ ".
- (B) East side of construction joint shifted down $1\frac{3}{8}$ ".

CRACK MAPPING AND FRP STRIP PLAN
SPAN "C" - EAST HALF
 Not to Scale

* See Special Provisions

*Single Ply FRP Strip

Existing Patching, Do not Remove Unless Delaminated

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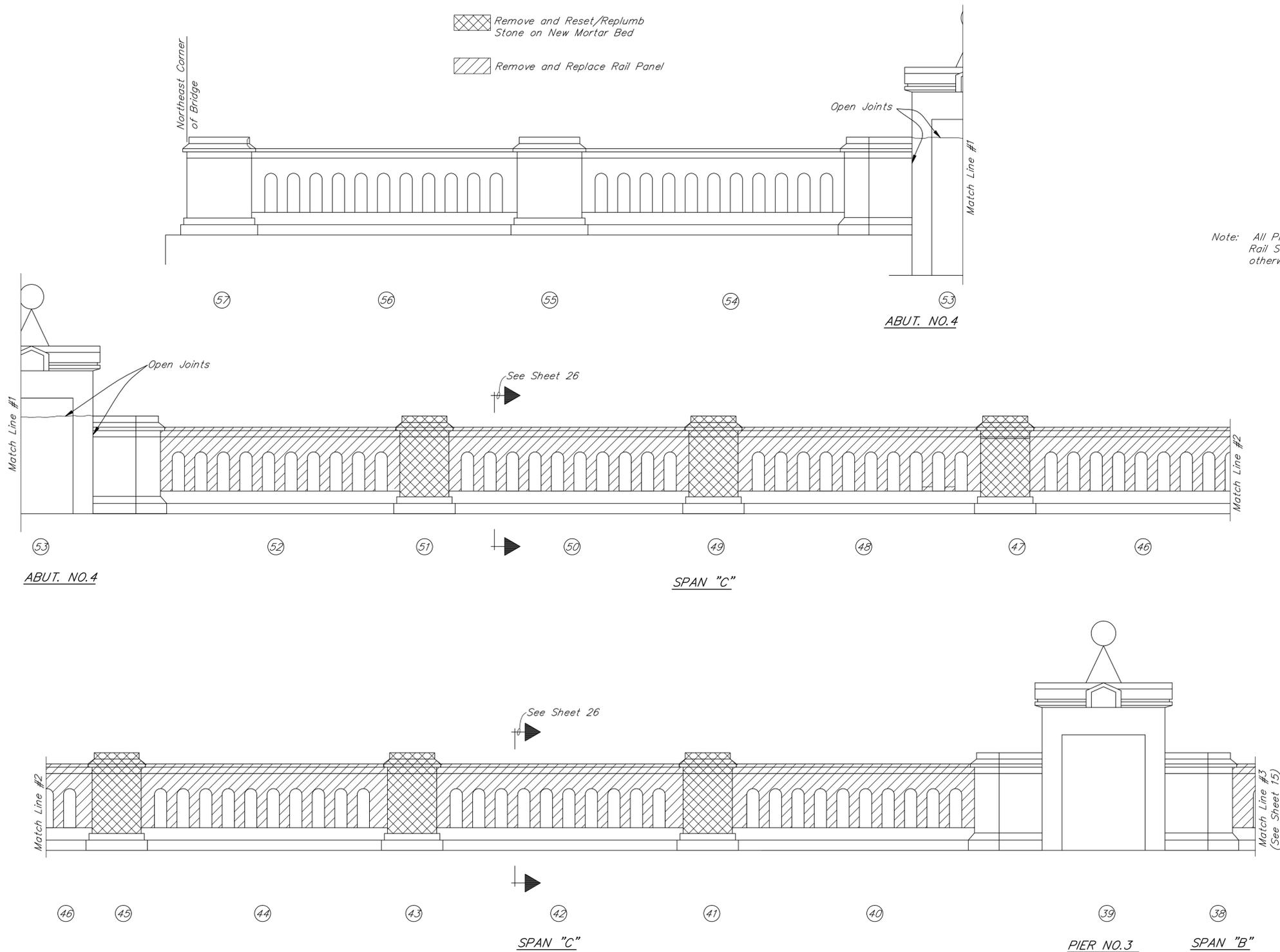
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CRACK MAPPING AND FRP STRIPS

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ELEVATION
EAST RAILING LOOKING EAST
Not to Scale

NOTE
See Sheets 20 thru 25 for Reconstruction Details.

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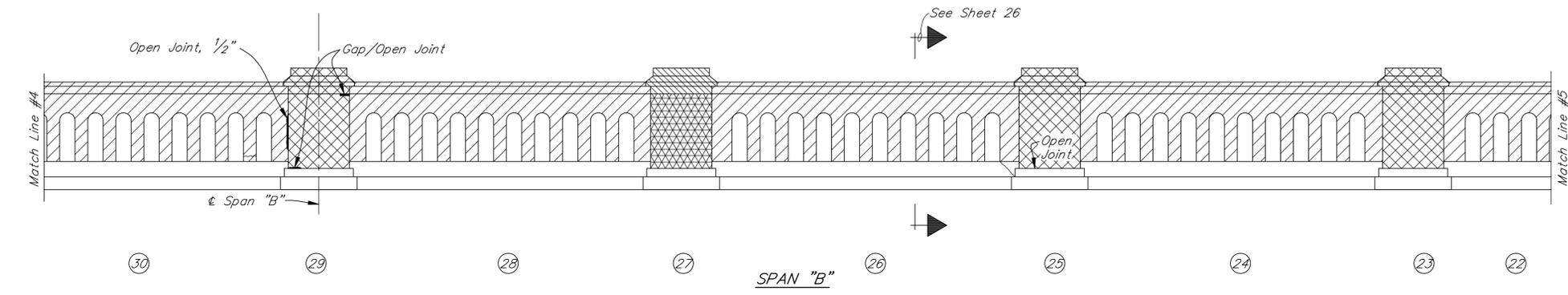
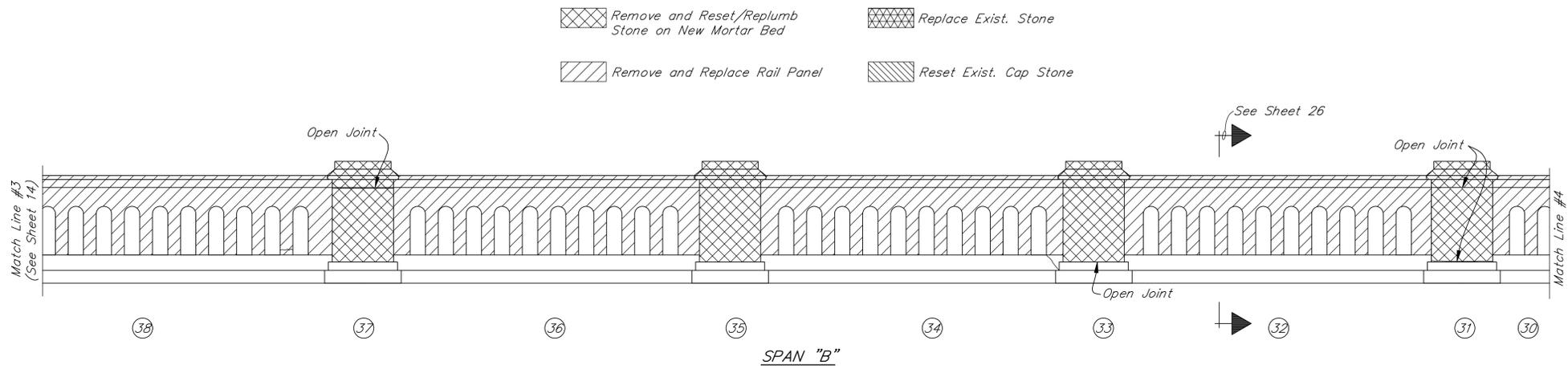
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**RAILING DETAILS-PHASE I
SHOWING REMOVALS AND
RAILING POST REHABILITATION**

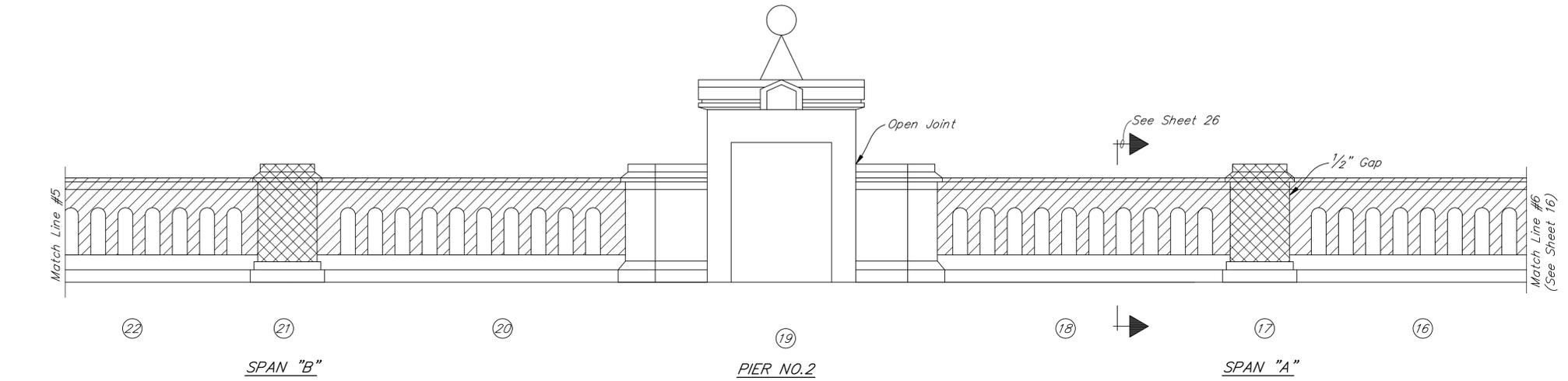
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AS NOTED	(933)31-71-3690C
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Note: All Pilasters are Stone and all Rail Sections are Concrete unless otherwise noted.



ELEVATION
EAST RAILING LOOKING EAST
Not to Scale

NOTE
See Sheets 20 thru 25 for Reconstruction Details.

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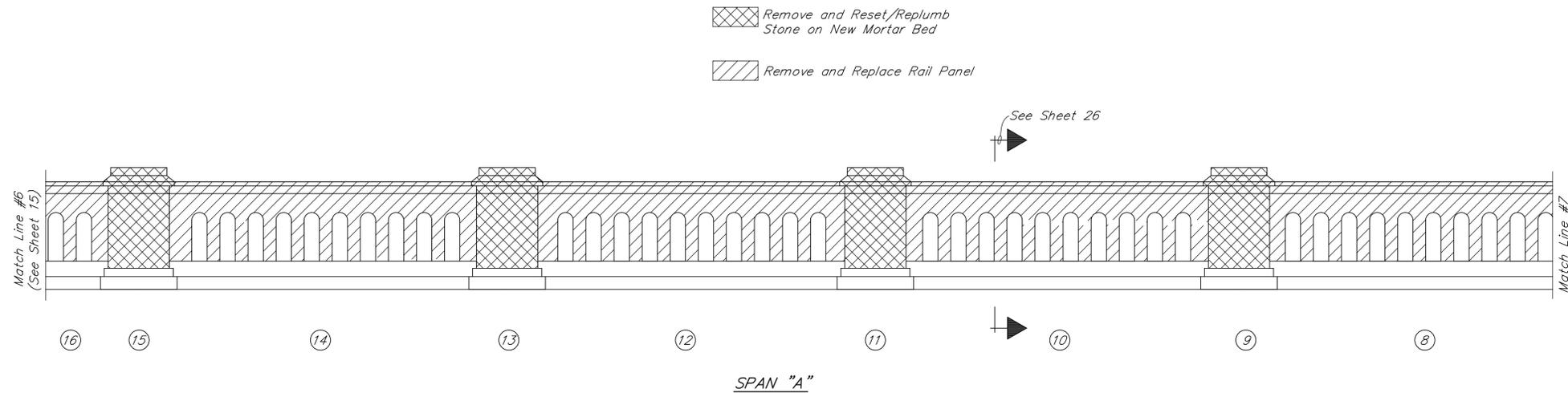
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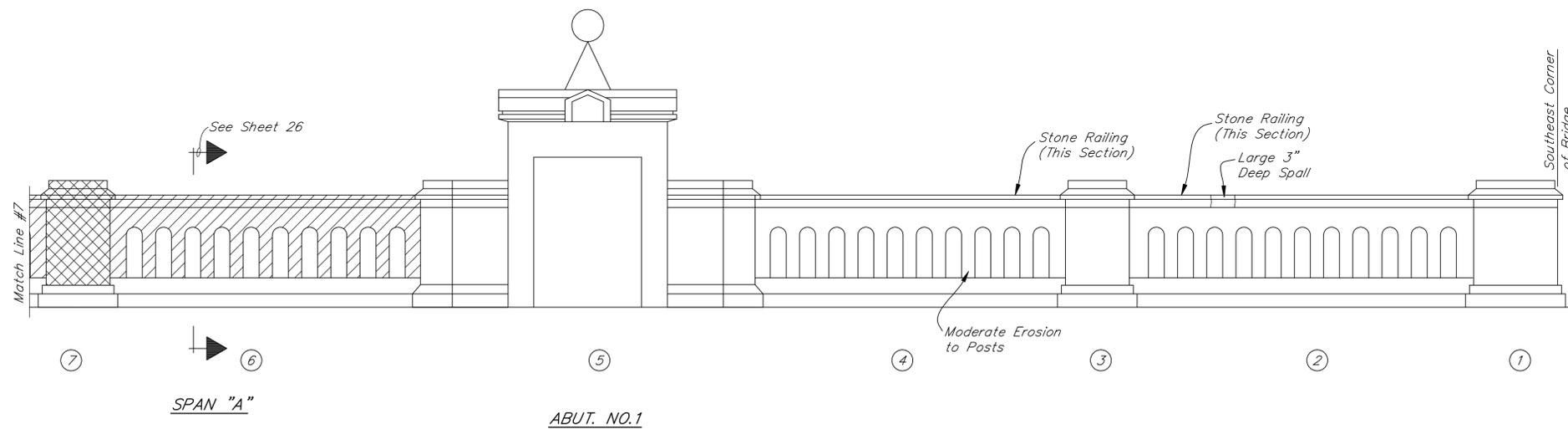
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RAILING DETAILS-PHASE I
SHOWING REMOVALS AND
RAILING POST REHABILITATION

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VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
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CONTRACT	PROJECT
	BR-230-6(002)



Note: All Pilasters are Stone and all Rail Sections are Concrete unless otherwise noted.



ELEVATION
EAST RAILING LOOKING EAST
Not to Scale

NOTE
 See Sheets 20 thru 25 for Reconstruction Details.

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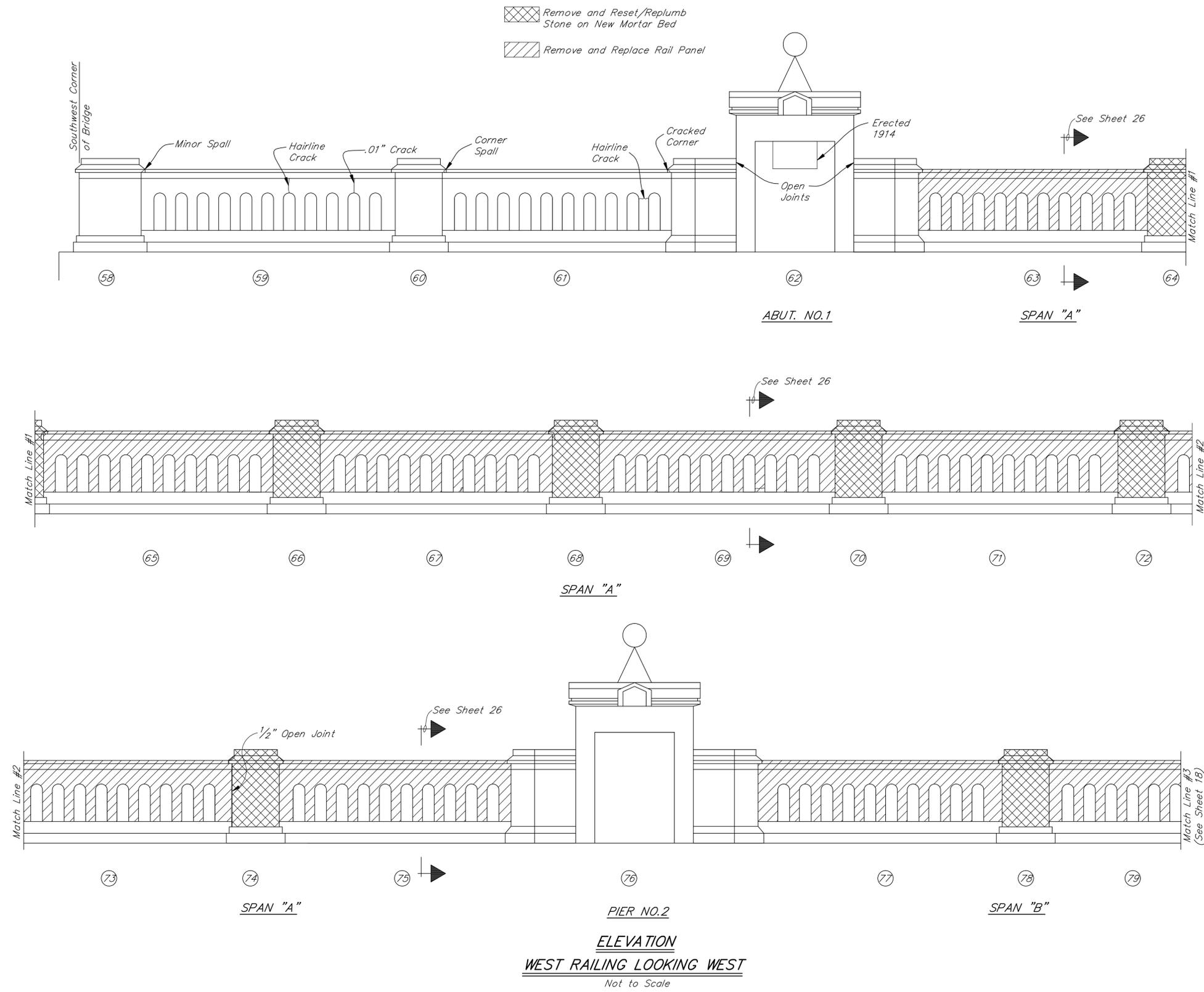
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RAILING DETAILS-PHASE I
 SHOWING REMOVALS AND
 RAILING POST REHABILITATION

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Note: All Pilasters are Stone and all Rail Sections are Concrete unless otherwise noted.

NOTE
See Sheets 20 thru 25 for Reconstruction Details.

ELEVATION
WEST RAILING LOOKING WEST
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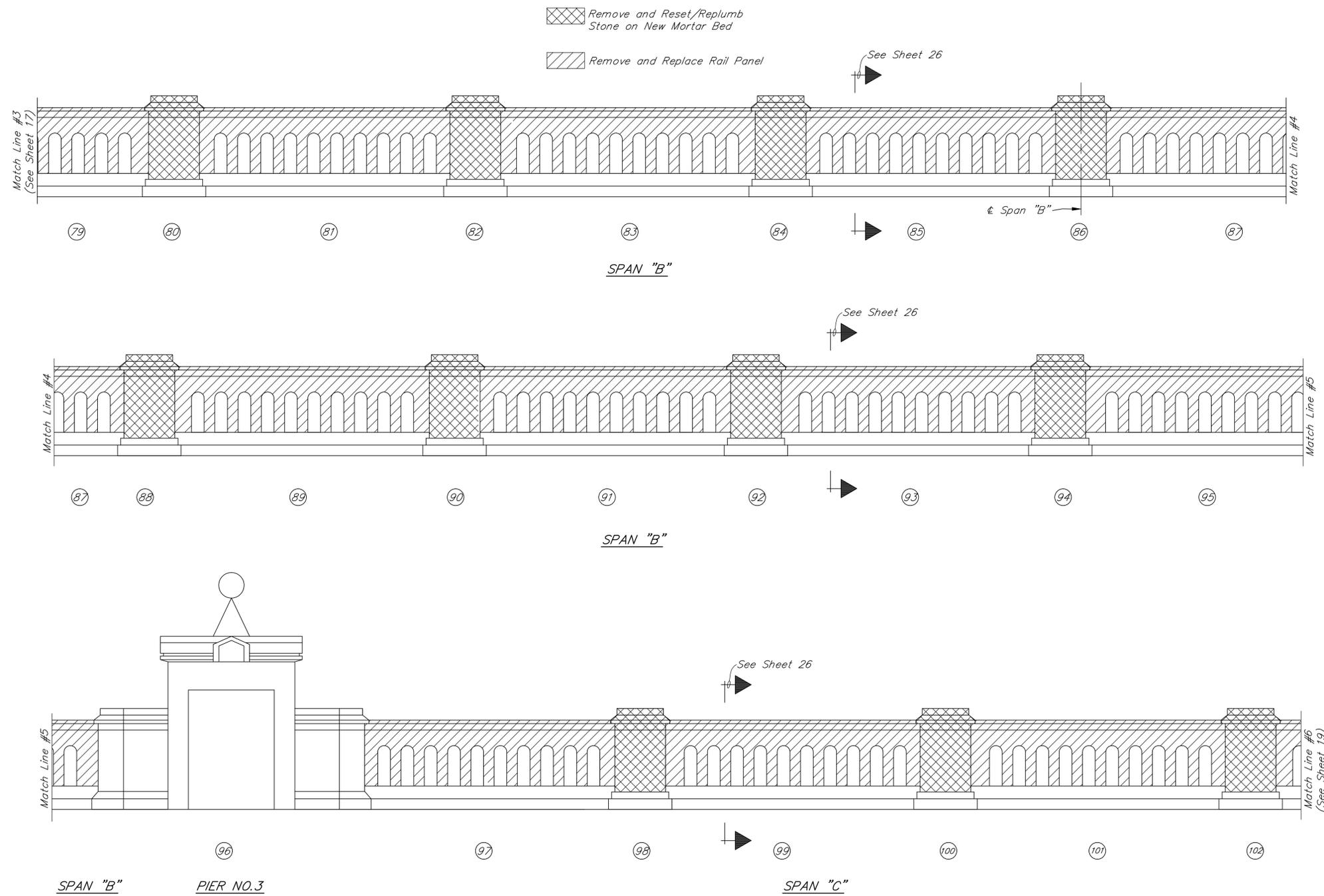
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SHOWING REMOVALS AND
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	BR-230-6(002)

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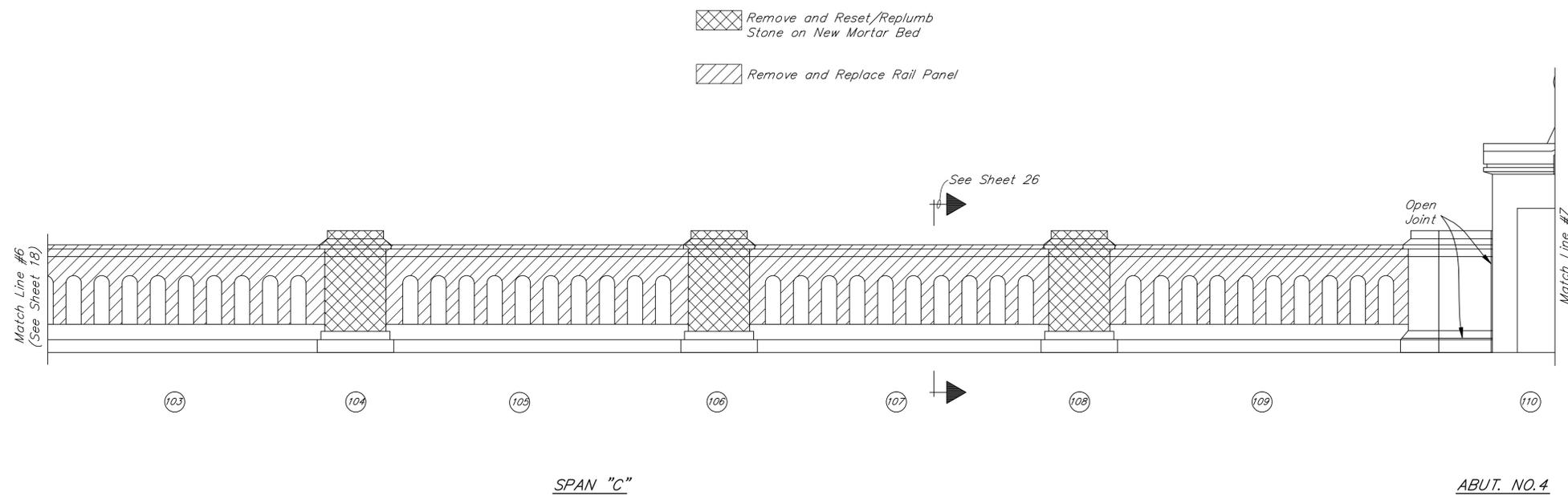
Note: All Pilasters are Stone and all Rail Sections are Concrete unless otherwise noted.

ELEVATION
WEST RAILING LOOKING WEST
 Not to Scale

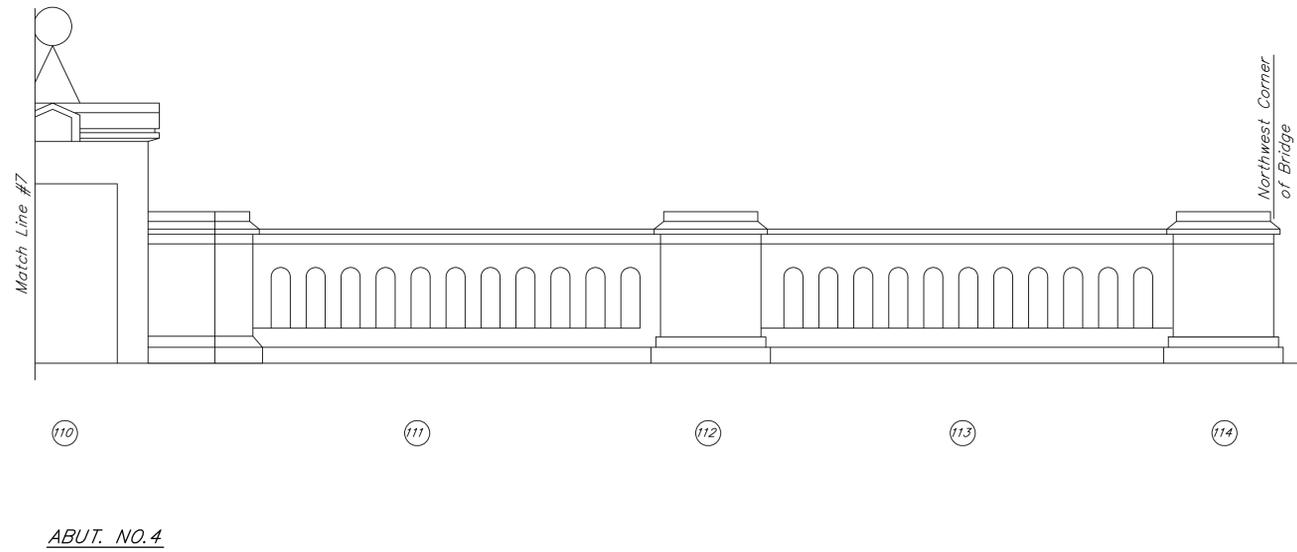
NOTE
 See Sheets 20 thru 25 for Reconstruction Details.

 Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSeng.com	Branch Locations FORT WAYNE 260-459-1532 JEFFERSONVILLE 812-286-0590 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242	RECOMMENDED FOR APPROVAL: _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE AS NOTED VERTICAL SCALE AS NOTED	FILE (933)31-71-3690C DESIGNATION 1173149	SHEET NO. 5298 OF 27
		DESIGNED: S. WEINTRAUT DRAWN: D. SHEETZ	RAILING DETAILS -PHASE II SHOWING REMOVALS AND RAILING POST REHABILITATION		SURVEY BOOK 18 OF 27		
		CHECKED: _____ CHECKED: _____	CONTRACT PROJECT BR-230-6(002)		CONTRACT PROJECT BR-230-6(002)		
		B-22	CONTRACT PROJECT BR-230-6(002)		CONTRACT PROJECT BR-230-6(002)		

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Note: All Pilasters are Stone and all Rail Sections are Concrete unless otherwise noted.



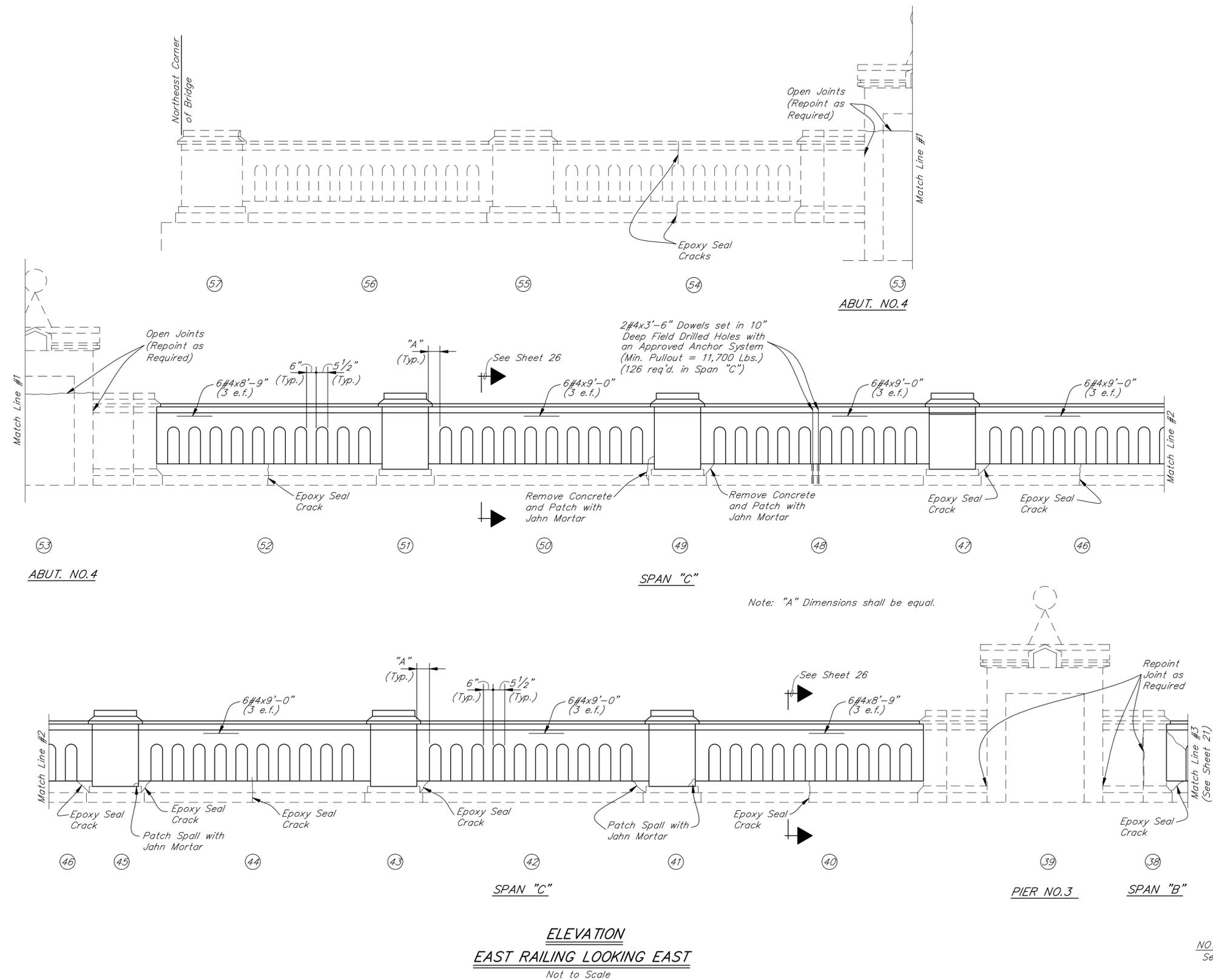
ELEVATION
WEST RAILING LOOKING WEST
 Not to Scale

NOTE
See Sheets 20 thru 25 for Reconstruction Details.

Butler Fairman Souffere <small>CIVIL ENGINEERS</small>	B-23	RECOMMENDED FOR APPROVAL: _____	DESIGN ENGINEER _____ DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION RAILING DETAILS-PHASE II SHOWING REMOVALS AND RAILING POST REHABILITATION	HORIZONTAL SCALE AS NOTED	FILE (933)31-71-3690C
		DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ		VERTICAL SCALE AS NOTED	DESIGNATION 1173149
Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSinc.com		Branch Locations FORT WAYNE 260-459-1532 JEFFERSONVILLE 812-285-0590 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242		SURVEY BOOK SHEET 19 OF 27		CONTRACT PROJECT BR-230-6(002)

BFS NO. 5298

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Note: "A" Dimensions shall be equal.

NOTE
See Sheet 26 for Bill of Materials.

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B-24	RECOMMENDED FOR APPROVAL:	
	DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ
CHECKED:	CHECKED:	

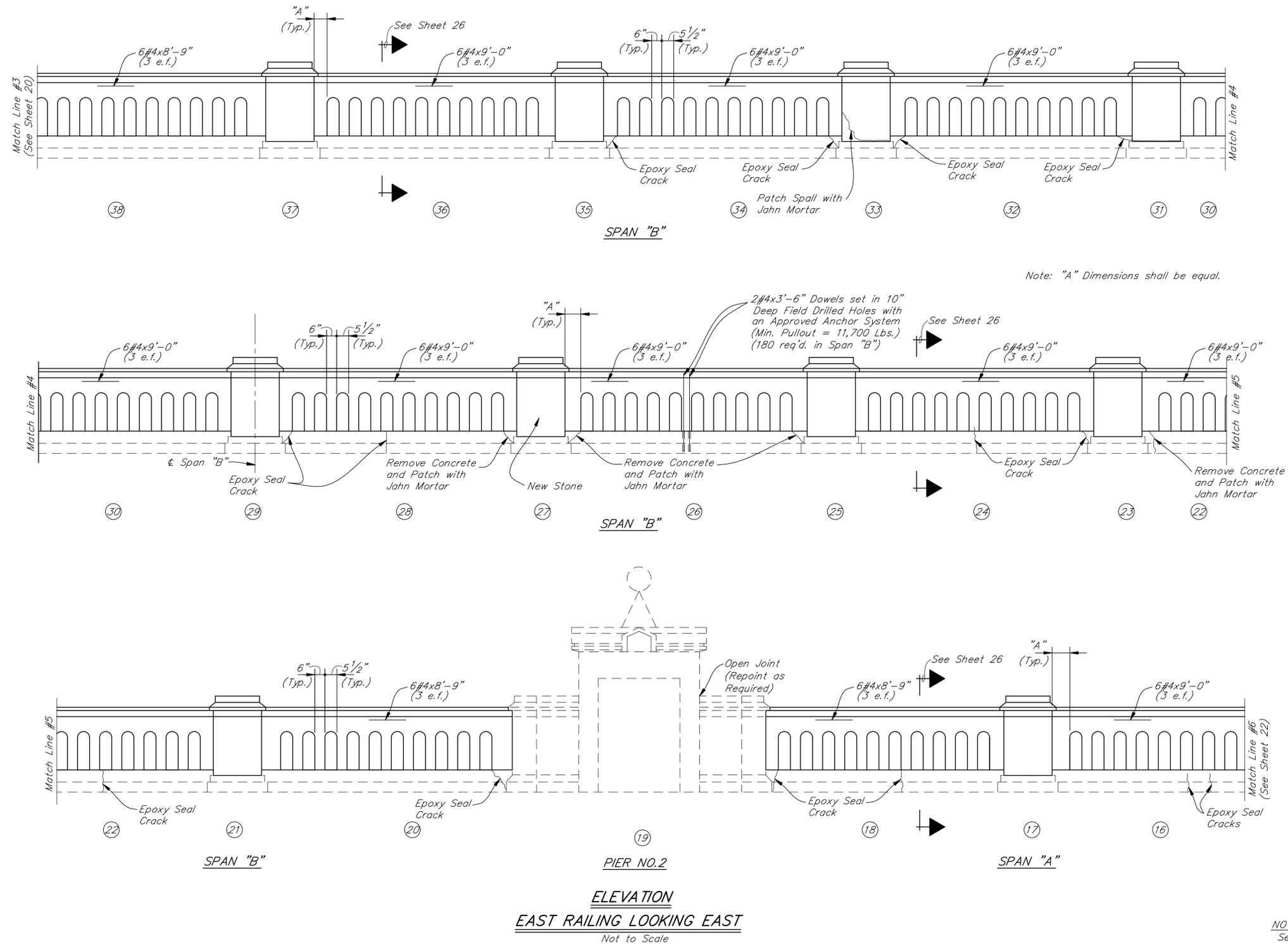
INDIANA
DEPARTMENT OF TRANSPORTATION

RAILING DETAILS-PHASE I
SHOWING RECONSTRUCTION

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	20 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

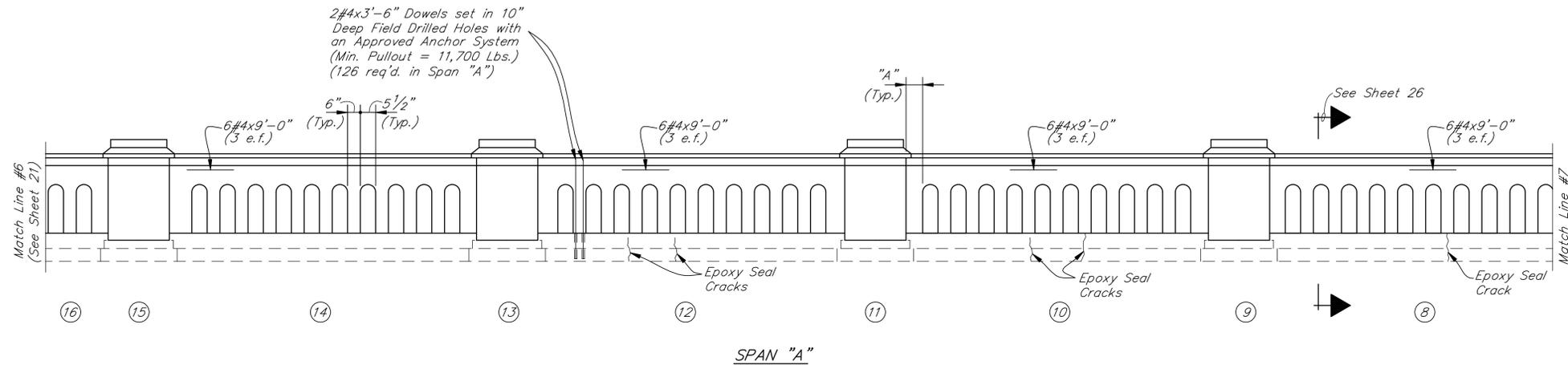
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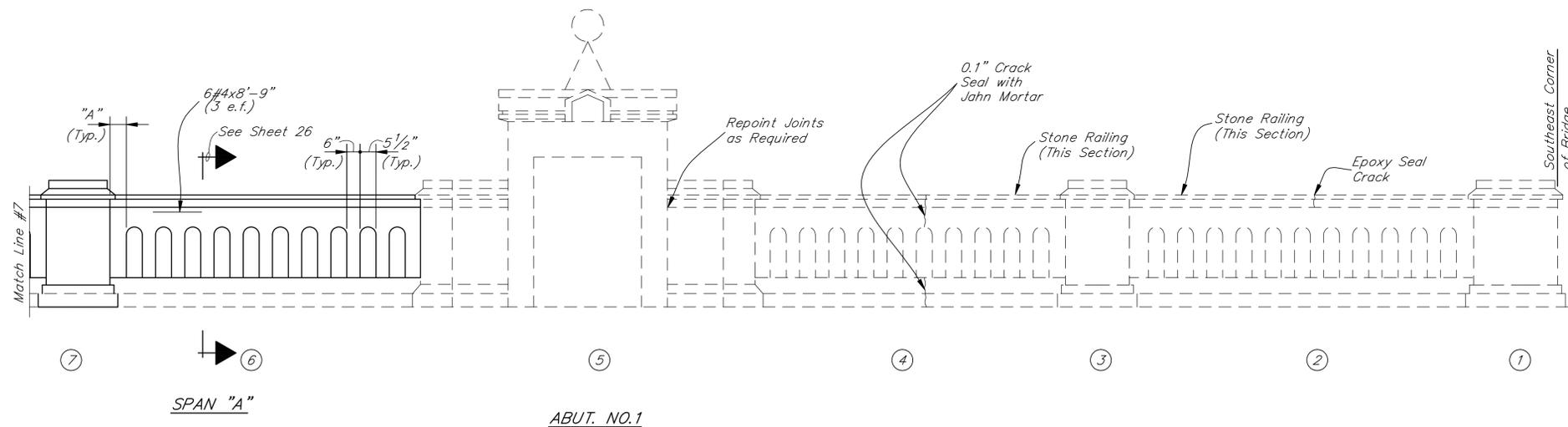


NOTE
See Sheet 26 for Bill of Materials.

Butler Fairman Souffere <small>CIVIL ENGINEERS</small> Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSinc.com	B-25	RECOMMENDED FOR APPROVAL:		INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	FILE	
		DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ	DATE	AS NOTED DESIGNATION 1173149	AS NOTED	(933)31-71-3690C
		CHECKED: _____	CHECKED: _____		RAILING DETAILS-PHASE I SHOWING RECONSTRUCTION	SURVEY BOOK	SHEET
					CONTRACT	PROJECT	21 OF 27
					BR-230-6(002)	BFS NO. 5298	



Note: "A" Dimensions shall be equal.



ELEVATION
EAST RAILING LOOKING EAST
 Not to Scale

NOTE
 See Sheet 26 for Bill of Materials.

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 MERRILLVILLE 219-769-2333
 PLAINFIELD 317-839-3242

B-26	
RECOMMENDED FOR APPROVAL:	DESIGN ENGINEER DATE
DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ
CHECKED:	CHECKED:

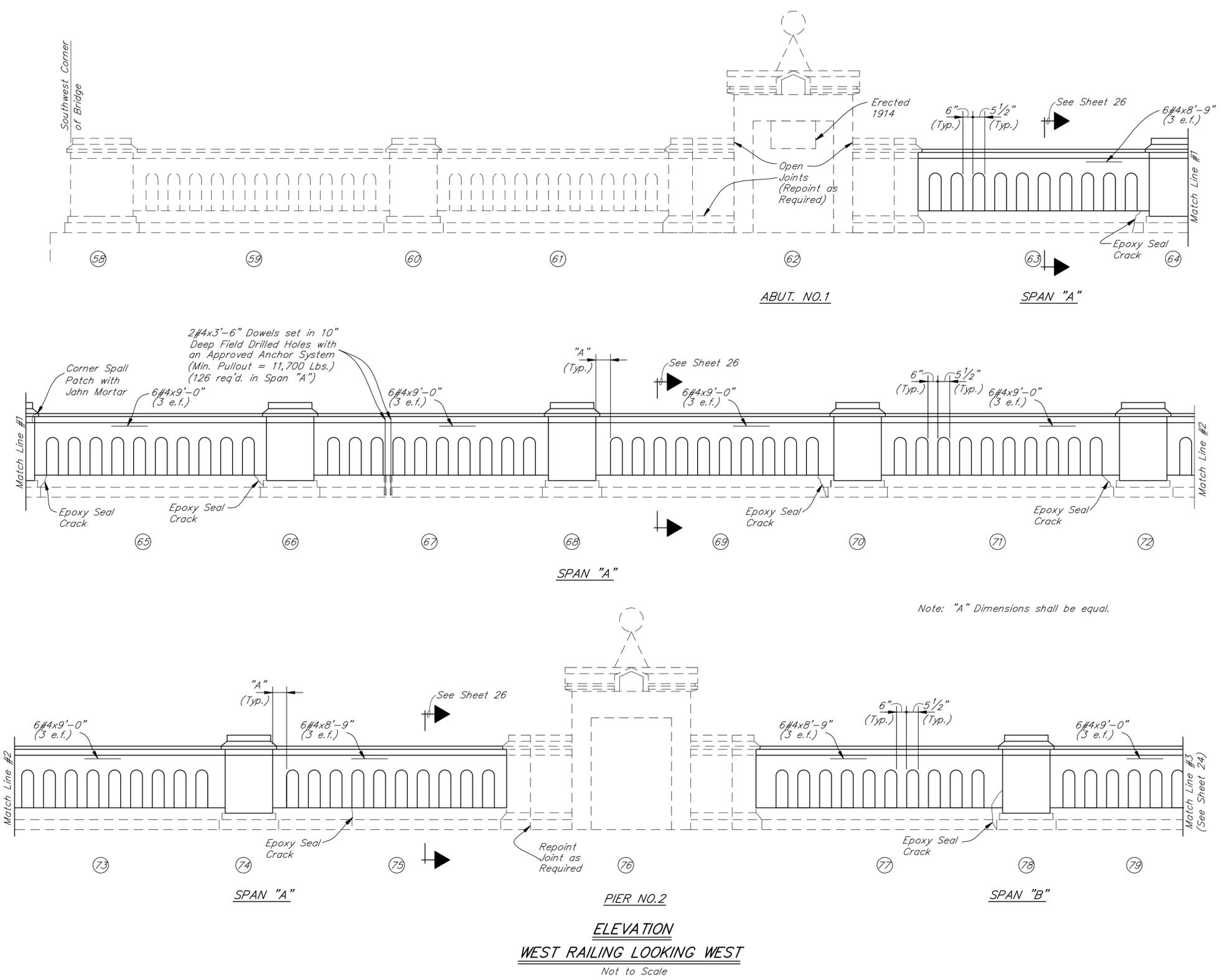
INDIANA
 DEPARTMENT OF TRANSPORTATION

**RAILING DETAILS-PHASE I
 SHOWING RECONSTRUCTION**

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	22 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

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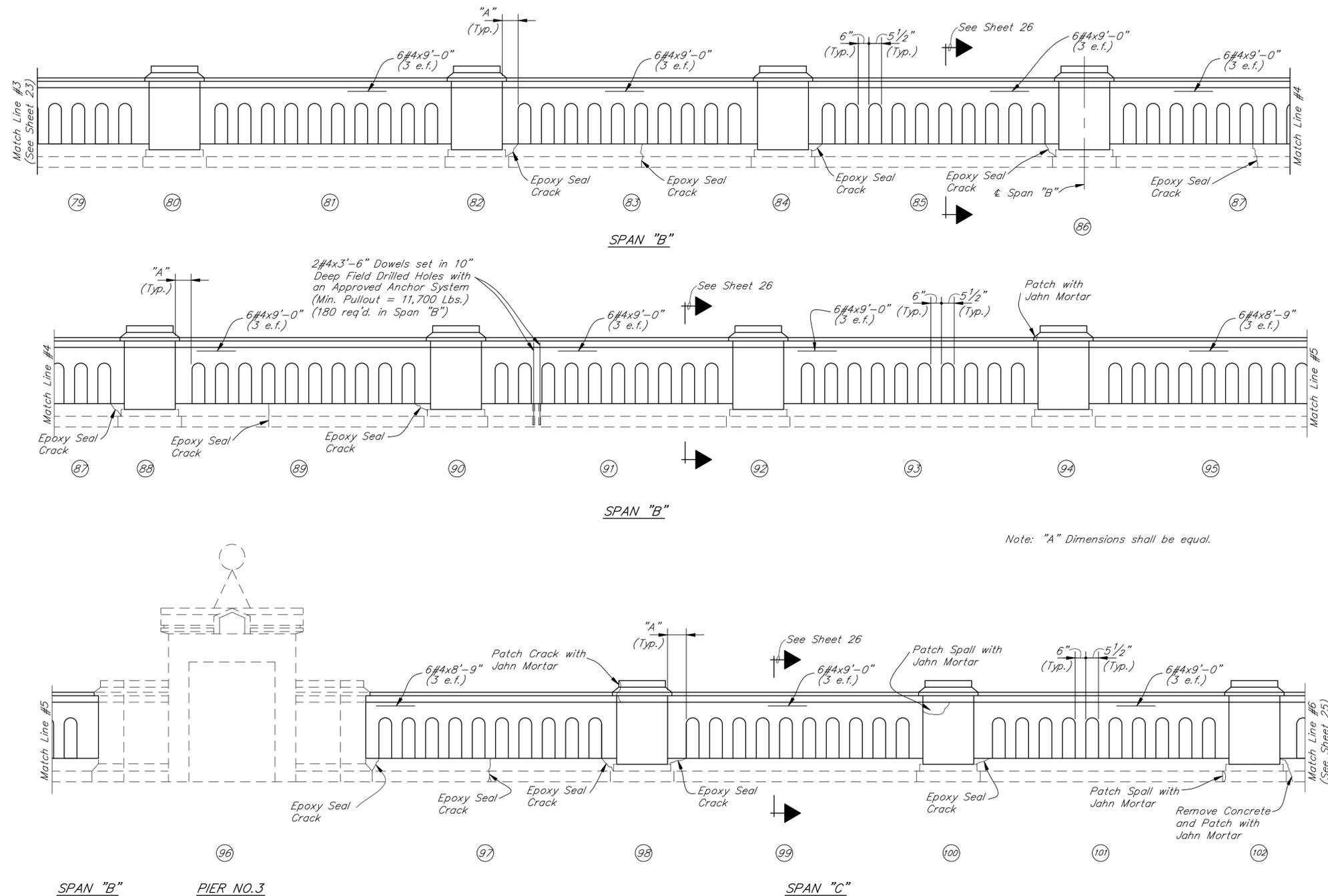


ELEVATION
WEST RAILING LOOKING WEST
 Not to Scale

NOTE
 See Sheet 26 for Bill of Materials.

BFS Butler Fairman Soufer CIVIL ENGINEERS	<div style="background-color: yellow; padding: 5px; display: inline-block;">B-27</div>	RECOMMENDED FOR APPROVAL:		INDIANA DEPARTMENT OF TRANSPORTATION RAILING DETAILS-PHASE II SHOWING RECONSTRUCTION	HORIZONTAL SCALE	FILE
		DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ		HORIZONTAL SCALE AS NOTED	(933)31-71-3690C
		CHECKED: _____	CHECKED: _____		VERTICAL SCALE AS NOTED	DESIGNATION 1173149
HEADQUARTERS: 8450 WESTFIELD BLVD., SUITE 300, INDIANAPOLIS, IN 46240-8302 TEL: 317-713-4615 FAX: 317-713-4616 www.BFSeng.com				BRANCH LOCATIONS: FORT WAYNE: 260-459-1532 JEFFERSONVILLE: 812-285-0590 LAFAYETTE: 765-423-5602 MERRILLVILLE: 219-769-2333 PLAINFIELD: 317-839-3242		SURVEY BOOK SHEET 23 OF 27 CONTRACT PROJECT BR-230-6(002)

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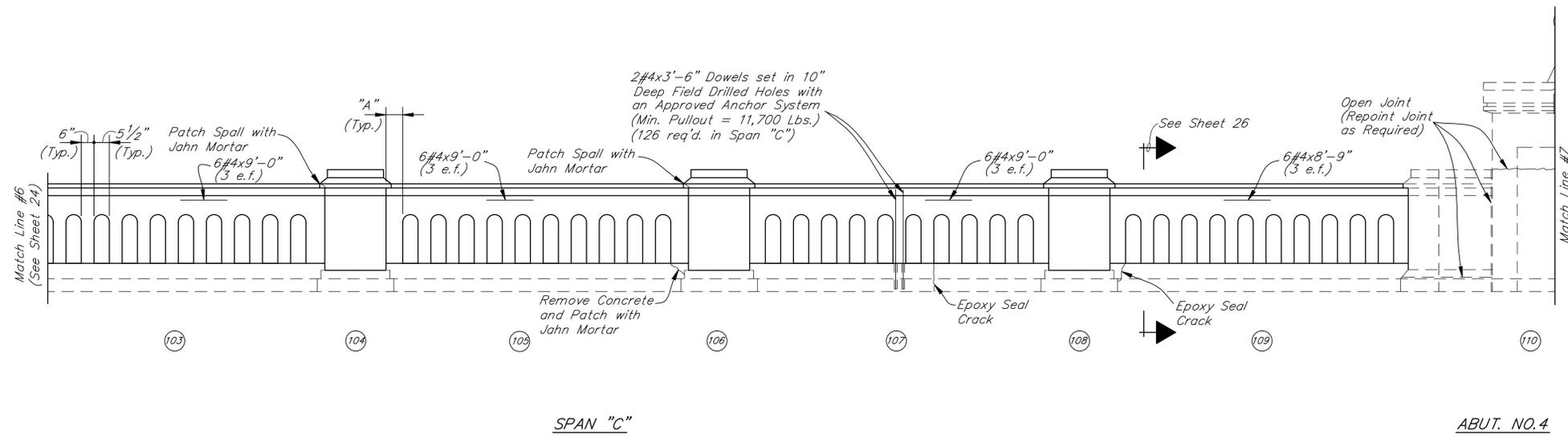
Note: "A" Dimensions shall be equal.

ELEVATION
WEST RAILING LOOKING WEST
Not to Scale

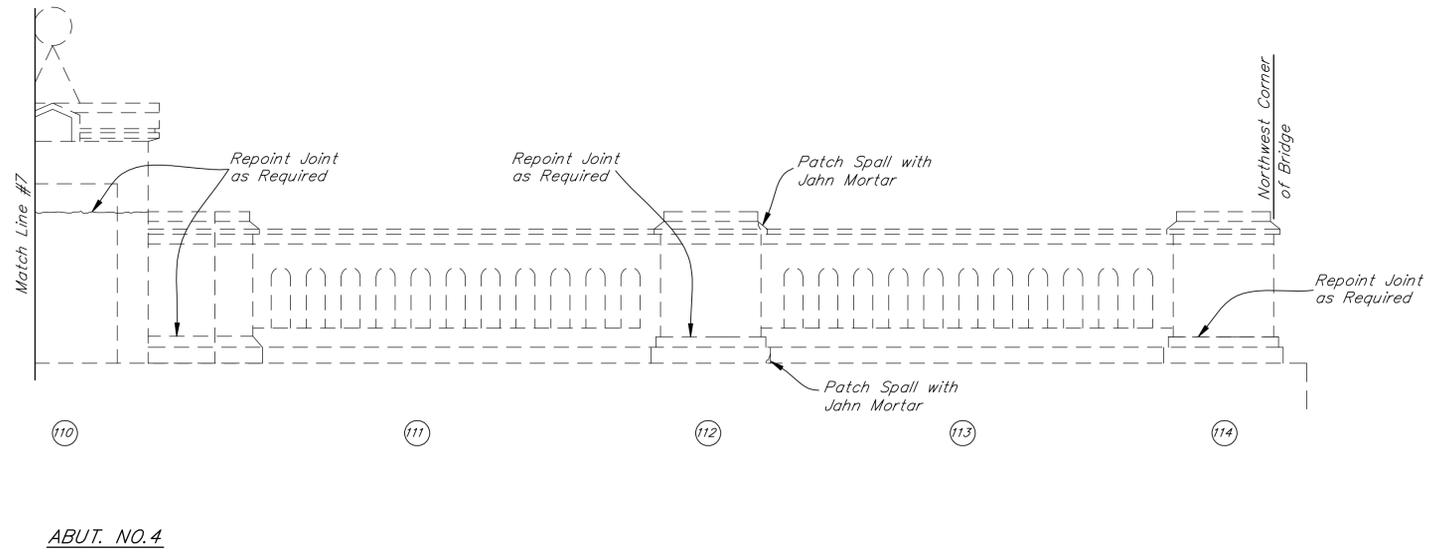
NOTE
See Sheet 26 for Bill of Materials.

<p>Headquarters 8450 WESTFIELD BLVD., SUITE 300 INDIANAPOLIS, IN 46240-8302 TEL 317-713-4615 FAX 317-713-4616 www.BFSeng.com</p> <p>Branch Locations FORT WAYNE 260-459-1532 JEFFERSONVILLE 812-285-0590 LAFAYETTE 765-423-5602 MERRILLVILLE 219-769-2333 PLAINFIELD 317-839-3242</p>	<div style="background-color: yellow; padding: 5px; border: 1px solid black;">B-28</div>	RECOMMENDED FOR APPROVAL:	INDIANA DEPARTMENT OF TRANSPORTATION		HORIZONTAL SCALE	FILE	
		DESIGNED: S. WEINTRAUT	DRAWN: D. SHEETZ	RAILING DETAILS-PHASE II SHOWING RECONSTRUCTION		AS NOTED	(933)31-71-3690C
		CHECKED:	CHECKED:	CONTRACT PROJECT		AS NOTED	DESIGNATION 1173149
				BR-230-6(002)		SURVEY BOOK	SHEET
					24	OF 27	

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Note: "A" Dimensions shall be equal.



ELEVATION
WEST RAILING LOOKING WEST
 Not to Scale

NOTE
See Sheet 26 for Bill of Materials.

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civil engineers

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 MERRILLVILLE 219-769-2333
 PLAINFIELD 317-839-3242

B-29

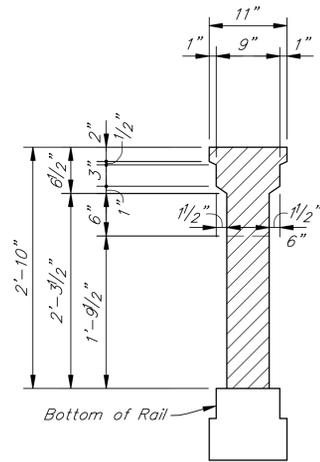
RECOMMENDED FOR APPROVAL: _____
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 CHECKED: _____ CHECKED: _____

INDIANA
DEPARTMENT OF TRANSPORTATION
RAILING DETAILS-PHASE II
SHOWING RECONSTRUCTION

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	25 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

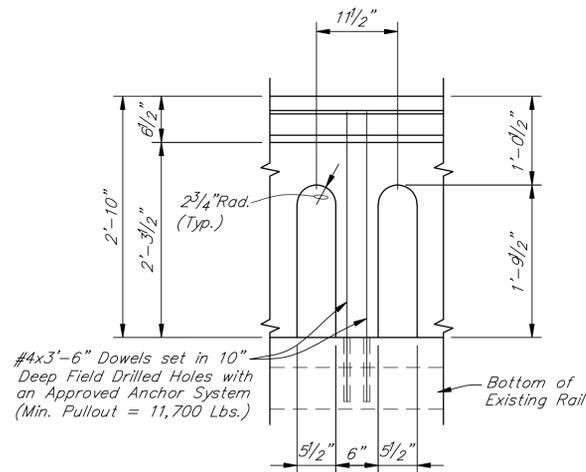
BFS NO. 5298

Note: Hatched Areas indicate Portions to be Removed.

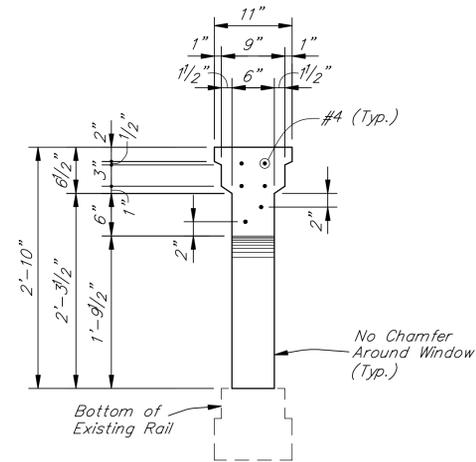


EXISTING RAIL SECTION
(SHOWING REMOVALS)

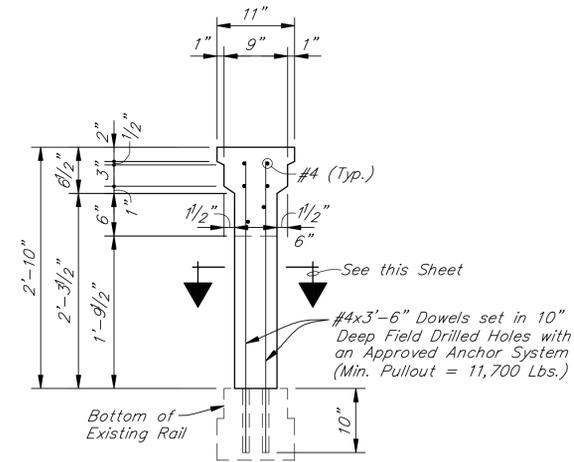
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ELEVATION



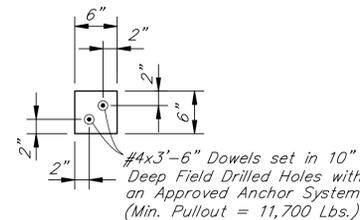
SECTION AT OPENINGS



SECTION BETWEEN OPENINGS

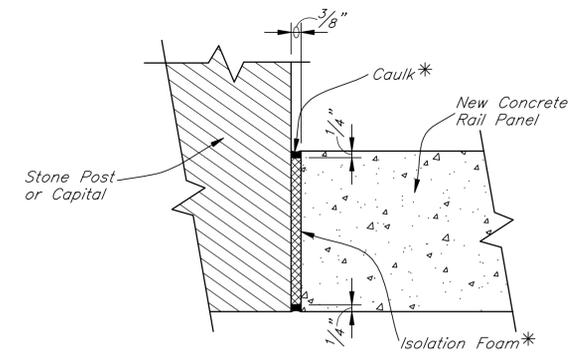
RAILING DETAILS

Scale: 1"=1'-0"



SECTION

Scale: 1"=1'-0"



TYPICAL ISOLATION JOINT*

Not to Scale

* See Special Provisions

BILL OF MATERIALS
EAST RAILING-PHASE I
WEST RAILING
PHASE II (SAME)

REINFORCING BARS			
Mark or Size	No. of Bars	Length (Ft.)	Weight (Lbs.)
#4	108	9'-0"	
#4	36	8'-9"	
#4	432	3'-6"	
Total Steel (Epoxy Coated)			1870
CONCRETE			
Class "C" in Railing			480 Lft.
MISCELLANEOUS			
Field Drilled Holes in Conc.			432 Each

A.S.T.M. A615, Grade 60

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B-30

RECOMMENDED FOR APPROVAL: _____
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INDIANA DEPARTMENT OF TRANSPORTATION
RAILING DETAILS

HORIZONTAL SCALE	FILE
AS NOTED	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
AS NOTED	1173149
SURVEY BOOK	SHEET
	26 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

STRUCTURE QUANTITIES

ITEM	CONCRETE			COMP. AGGR. NO. 5,3 BASE	CURB, CONCRETE	CONCRETE, C, RAILING, MODIFIED	REINF. STEEL (PLAIN)	REINF. STEEL (EPOXY COATED)	EST. WEIGHT STR. STEEL	STEEL ENCASED CONC. PILES	STEEL H--PILES	PILE TIPS	CAST IRON PIPE 6"	CAST IRON DRAIN TYPE "SQ-A"	PVC PIPE 6"	THRIE BEAM BRIDGE RAIL	THREADED TIE BAR ASSEMBLIES (EPOXY COATED)	EXP. JOINT TYPE BS	STRUCTURAL MEMBERS CONCRETE I-BEAM	EST. AREA SURFACE SEAL	EST. AREA MASONRY COATING	FIELD DRILLED HOLES IN CONCRETE	BARRIER DELINEATORS					
	PCCP, 6"	CLASS A IN SUBSTR.	CLASS B IN FOOTING																									
	SYS.	CYS.	CYS.																									TONS
SUBSTRUCTURE																												
Abut. No.1																												
Pier No.2																												
Pier No.3																												
Abut. No.4																												
SUPERSTRUCTURE																												
Spans "A", "B" & "C"																												
TOTALS								⊕	⊕																			

⊕ A.S.T.M. A615, Grade 60

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B-31

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DESIGN ENGINEER DATE

DESIGNED: D. SHEETZ DRAWN: D. SHEETZ

CHECKED: _____ CHECKED: _____

INDIANA
DEPARTMENT OF TRANSPORTATION

BRIDGE SUMMARY

HORIZONTAL SCALE	FILE
NONE	(933)31-71-3690C
VERTICAL SCALE	DESIGNATION
NONE	1173149
SURVEY BOOK	SHEET
	27 OF 27
CONTRACT	PROJECT
	BR-230-6(002)

BFS NO. 5298

Appendix C---Photos/Tables

Appendices: Photos



Photo 1: Looking north.

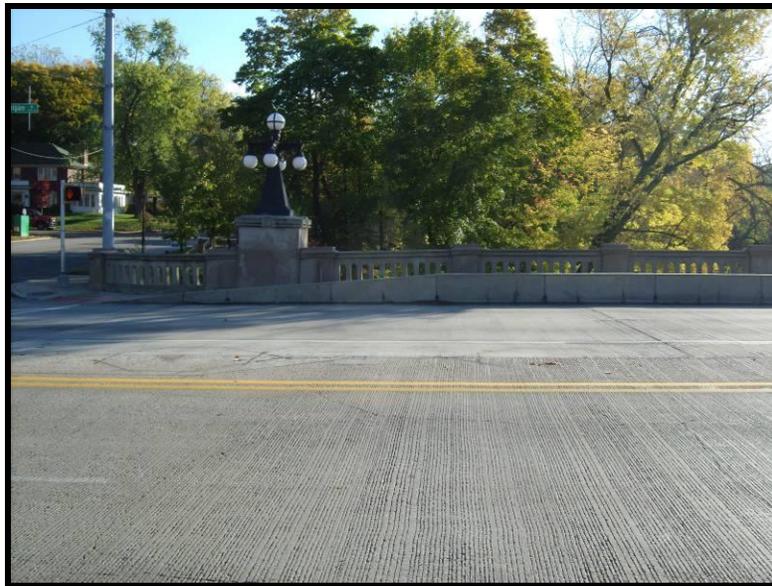


Photo 2: On structure, looking east.

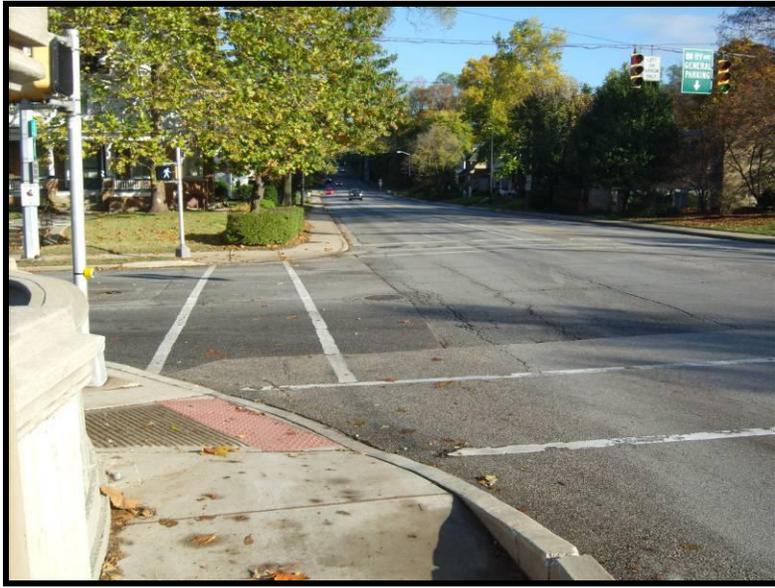


Photo 3: On structure, looking north.



Photo 4: On structure, looking north.



Photo 5: On structure, looking south.



Photo 6: On structure, looking south.



Photo 7: On structure, looking north.

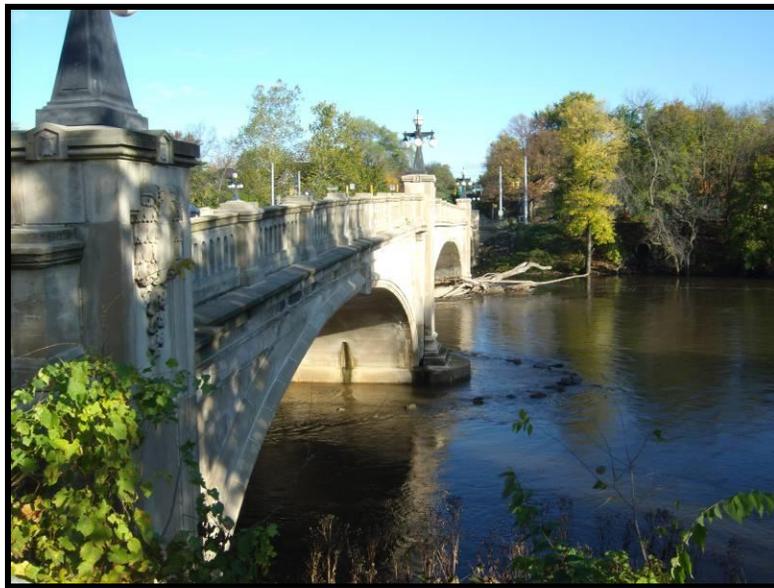


Photo 8: Looking northwest.



Photo 9: Looking west/northwest.

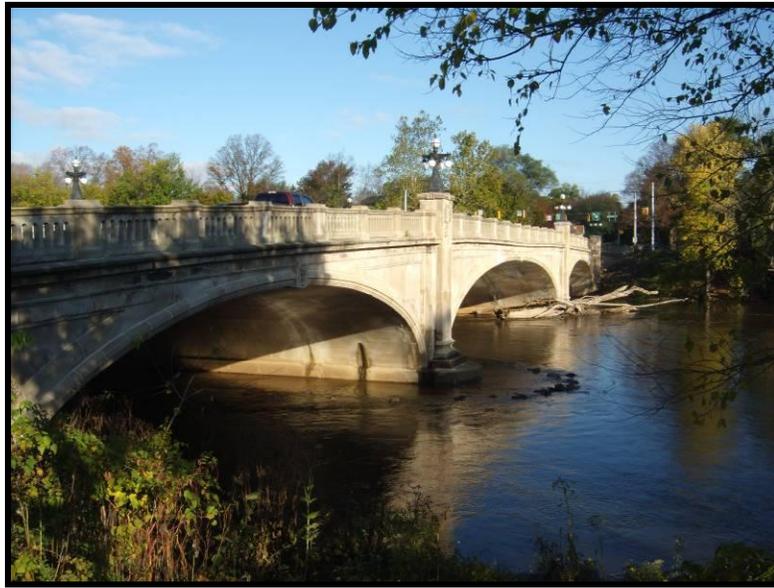


Photo 10: Looking west/northwest from south bank.



Photo 11: Looking northwest from structure southeast corner.

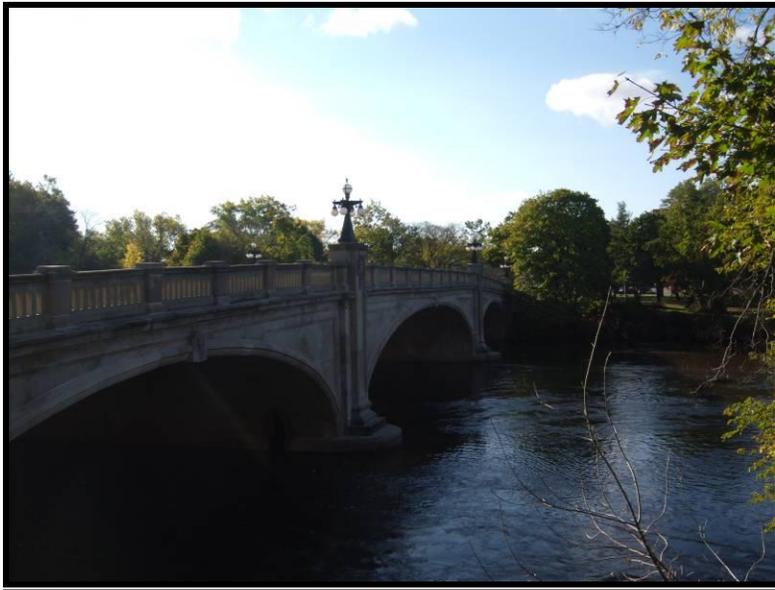


Photo 12: South side of river, looking west/northwest.



Photo 13: Looking south from south end of structure.



Photo 14: Looking south from east side of structure.

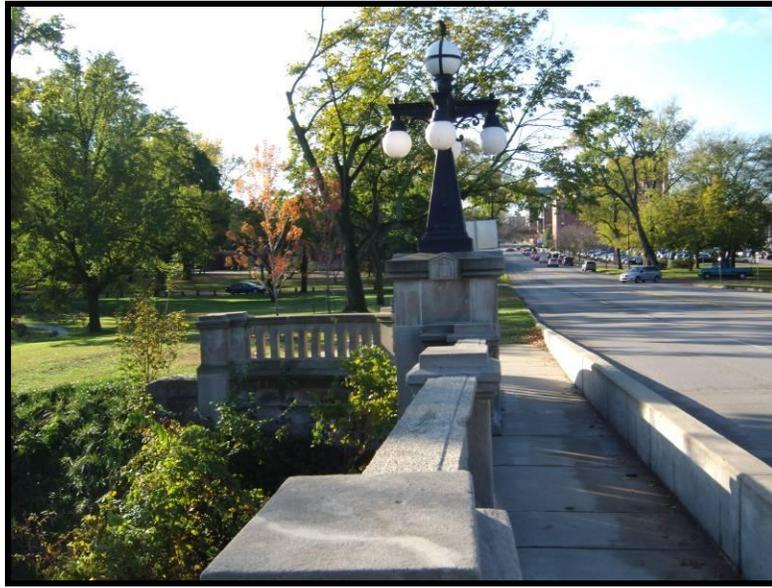


Photo 15: Same as previous photo.



Photo 16: West side of bridge, south end.

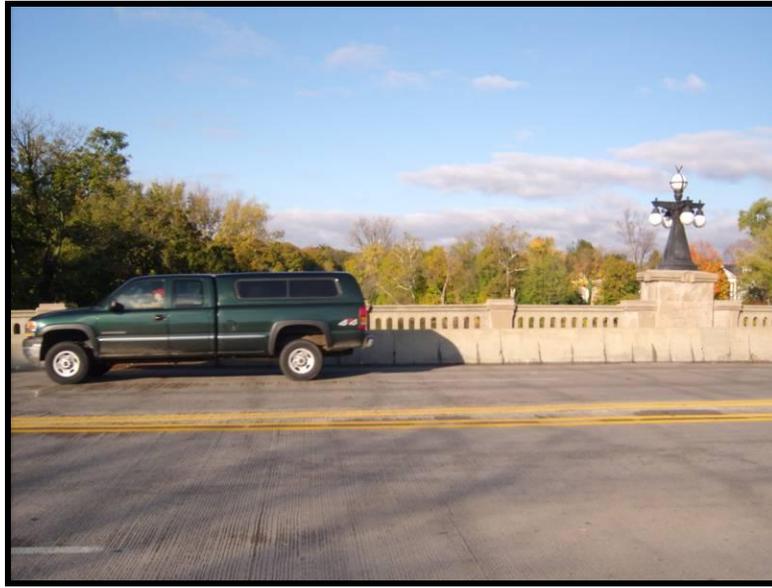


Photo 16: Looking west from east side of bridge.



Photo 17: Ca.-1925 American Four Square (NA E. Northshore Dr.)
Not included in 2005 interim report.

Appendices: Tables: Surveyed Resources Located in Project APE

Leeper Park Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.-1930	Leeper Park	Wellhouse; Renaissance Revival	#201-598-34006; Outstanding
	Ca.-1912	Leeper Park	North Pumping Station	#201-598-34014; Outstanding; Individually NR-listed 1996

Leeper Park Historic District resources in APE	Date	Addresses	Property Name/Style	Survey #/Interim Report Rating
	Ca.-1914	Leeper Park	SR 933/Michigan Street/Leeper Bridge	#201-598-34018; Outstanding
	Ca.-1820	Leeper Park	Pierre Navarre Log House/single- pen	#201-598-34008; Outstanding

Leeper Park Historic District resources in APE	Date	Addresses	Property Name/Style	Survey #/Interim Report Rating
	Ca.-1913; Ca.-1930	Leeper Park	Ca.-1913 20 th century functional cistern; photo far right; Ca. 1930 Colonial Revival wellhouse, photo far left	Wellhouse, photo left: #201- 598-34007, Outstanding; Cistern, photo right: #201-598- 34015, Contributing

Harter Heights Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.-1955	101 E. North Shore Drive	International	#201-597/598- 35141; Notable
	Ca.-1904	1020 Michigan St.	Kruger House; Dutch Colonial Revival	#210-597/598- 35155; Notable

Harter Heights Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.-1908	1022 Michigan St.	Gilbert House; Dormer-front bungalow	#201-597/598- 35156; Notable
	Ca.-1908	1026 Michigan St.	Dutch Colonial Revival	#201-597/598- 35157; Contributing

Harter Heights Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.-1921	121 E. Northshore Drive	Mattes House; Colonial Revival	#201-597/598- 35142; Outstanding
	Ca.-1921	127 E. Northshore Drive	Van Dien House; Colonial Revival	#201-597/598- 35143; Outstanding

Harter Heights Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.-1950	137 E. Northshore Drive	Contemporary house	#201-597/598- 35144; Outstanding

West Northshore Historic District resources in APE	Date	Address	Property Name/ Style	Survey #/Interim Report Rating
	Ca.- 1903	107 Northshore Drive	Lent House; Neoclassical	#201-598-37020; Outstanding
	Ca.- 1906	103 Northshore Drive	Judge Slick House; Free Classic	#201-598-37019; Notable

West Northshore Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.- 1888	113 Northshore Drive	Samuel Leeper, Jr. House; gable- front/ Italianate/QA	#201-598-37018; Outstanding; NR- listed
	Ca.- 1904	117 Northshore Drive	Rutherford House; Free Classic	#201-598-37017; Notable

West Northshore Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.- 1903	123 Northshore Drive	Mack House; American Four Square	#201-598-37016; Notable
	Ca.- 1905	129 Northshore Drive	Miller House; Neoclassical	#201-598-37015; Notable

West Northshore Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.- 1906	201 Northshore Drive	Rider House; Neoclassical	#201-598-37014; Notable
	Ca.- 1905	209 Northshore Drive	Neff House; Dutch Colonial Revival	#201-598-37013; Outstanding

West Northshore Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.- 1905	215 Northshore Drive	Carson House; American Four Square	#201-598-37012; Notable
	Ca.- 1925	219 Northshore Drive	Bryan House; Italian Renaissance Revival	#201-598-37011; Outstanding

West Northshore Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Report Rating
	Ca.- 1920	221 Northshore Drive	Hoover House; Mission Revival	#201-598-37010; Outstanding
	Ca.- 1910	229 Northshore Drive	Doloh House; Tudor Revival	#201-598-37009; Outstanding

Northshore Triangle Historic District resources in APE	Date	Address	Property Name/Style	Survey #/Interim Rating
	Ca.-1902	102 Marquette Ave.	Woodward House; Queen Anne	#201-598-36235; Outstanding
	Ca.-1911	106 Marquette Ave.	Judge Slick House; American Four Square/Prairie	#201-598-36234; Outstanding

Appendix D---Correspondence

December 26, 2011

Mr. Staffan Peterson
Manager, Cultural Resources Office,
INDOT Environmental Services
100 North Senate Ave.
Room N758
Indianapolis, IN 46204



Dear Mr. Peterson,

Thank you for including the Michigan Road Byway organization in your list of consulting parties for the bridge project identified by Des. #1173149, over the St. Joseph River in South Bend.

The bridge and the area in which the project is located is an important historical resource with several historical resources as identified in your report. We were contacted as a potential consulting party; however the Michigan Road Byway is not located in the APE of the project. The byway turns west on Lincolnway and continues west from the city on that route, therefore we will decline to be a consulting party on this project. We wish you well in this important restoration endeavor.

Thank you again.

Sincerely,

Kurt West Garner, Co-chair

Historic Michigan Road

12954 6th Road - Plymouth, IN 46563

574-936-0613 (phone) kwgarner@kwgarner.com (email)



South Bend and St. Joseph County
HISTORIC PRESERVATION COMMISSION

227 WEST JEFFERSON BLVD.
SOUTH BEND, IN 46601-1830

Phone: 574-235-9798 Fax: 574-235-9578
e-mail: SBSJCHPC@co.st-joseph.in.us



Timothy S. Kluszczinski, President

A Certified Local Government

Catherine D. Hostetler, Director

29 December 2011

Staffan D. Peterson,
Manager, Cultural Resources Office
INDOT Environmental Services
100 N. Senate Avenue, Room 642
Indianapolis, IN 46204-2217

RE: Des #1173149, Bridge Project, SR 933 over the St. Joseph River, 1.59 mile north of SR 23, South Bend, Portage Township, St. Joseph County

Dear Mr. Peterson,

Thank you for the information on the proposed restorations, in-kind repairs and structural upgrades to the Leeper Park Bridge. The Historic Preservation Commission of South Bend and St. Joseph County wishes to be a consulting party on this project.

Since the Leeper Park Bridge is a local landmark, all of the work proposed will require a Certificate of Appropriateness (COA) from the Historic Preservation Commission (HPC). I have included a copy of the COA application along with the HPC's 2012 meeting dates and COA deadlines. The fee for a COA in 2012 is \$20.00.

The project as presented does not pose a negative impact on any of the built environment located in the APE. However, Leeper Park itself is a Landscape Local Landmark and on the National Register. Leeper Park has its own set of Standards and Guidelines which I have included for your reference and use. In the past, work on this bridge used portions of Leeper Park as a staging area for supplies and equipment. It is necessary when these temporary disruptions in Park's landscape occur that the trees and tree roots are protected and that the Park's landscape is restored when the work on the Bridge is finished.

Sincerely,

Catherine D. Hostetler,
Director

COMMISSIONERS

Jerry A. Niezgodski (Vice President)
Kevin Buccellato, R.A..
Alice McLane

Lynn Patrick (Secretary)
David Steinhauer,
Architectural Historian

Sandra L. Rossow
Michael Voll

D-75

D-3

Branigin, Susan

From: Todd Zeiger [TZeiger@indianalandmarks.org]
Sent: Friday, January 20, 2012 2:15 PM
To: Branigin, Susan
Subject: Des 1173149 SR 933 over St. Joseph River

Hello, Susan. Pertaining to the proposed rehabilitation of the SR 933 bridge (Designation 1173149) I do not have any comments or concerns pertaining to the proposed rehabilitation or its impact on nearby/adjacent historic resources. Perhaps it was in the information – but I did not see any discussion about addressing what is causing the east pier to sink. Is there some consideration or that issue so that the extensive repairs are not required again in the future or worse the bridge requires complete replacement? Thank you.

Sincerely,

.....
Todd Zeiger
Director, Northern Regional Office

.....
Indiana Landmarks
402 W. Washington
South Bend, IN 46601
Ph. 574-232-4534
Fax: 574-232-5549
www.indianalandmarks.org

Indiana Landmarks revitalizes communities, reconnects us to our heritage, and saves meaningful places.

Division of Historic Preservation & Archaeology 402 W. Washington Street, W274 Indianapolis, IN 46204-2739
Phone 317-232-1646 Fax 317-232-0693 dhpa@dnr.IN.gov



January 20, 2012

Staffan D. Peterson, Ph.D.
Manager, Cultural Resources Office
Environmental Services Division
Indiana Department of Transportation
100 North Senate Avenue, Room N642
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration (“FHWA”)

Re: Historic property report (Branigin, 12/11) and archaeological short report (Miller, 10/11)
regarding the SR 933 Bridge carrying Michigan Street over the St. Joseph River -scope
undetermined- (Designation No. 1173149; DHPA No. 12724)

Dear Dr. Peterson:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the “Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana,” the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated December 21, 2012, and received on December 22, 2012, for the above indicated project in South Bend, Portage Township, St. Joseph County, Indiana.

In regard to archaeological resources, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (“NRHP”) within the proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the National Register of Historic Places in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior’s “Standards and Guidelines for Archaeology and Historic Preservation” (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

In regard to buildings and structures, we concur with INDOT’s assessment that the Mason House at 140 East North Shore Drive (Indiana Historic Sites and Structures Interim Report - St. Joseph County site # 201-597-10019), the O’Hara House at 134 East North Shore Drive (site # 201-597-10020), the Taylor House at 128 East North Shore Drive (site # 201-597-10021) and the Mattes House at 122 East North Shore Drive (site # 201-597-10021) are not eligible for inclusion in the National Register of Historic Places.

Additionally, we concur with INDOT’s assessment that the following properties are eligible for inclusion in the National Register of Historic Places.

- 1) West North Shore District (sites #201-598-37001-37020) for its collection of late 19th and early 20th century Revival residential architecture along with its association to the early planning and development of South Bend.
- 2) Harter Heights Historic District (sites # 201-597/598-35001-35277) for its association with early planning and development of South Bend. The district also has strong architectural integrity and many of the properties were designed by local architectural firms.
- 3) North Triangle Historic District (sites # 201-598-36001-36290) for its association with early planning and development of South Bend. Also, the properties within the district embody a high level of architectural integrity and it includes several properties that were designed by Ernest Young.

Furthermore, we agree that the following properties were listed in the National Register of Historic Places:

- 1) Leeper Park Historic District (sites #201-598-34001-34018) listed 06-15-2000 (HPR states 1998)
- 2) Samuel Leeper, Jr. House (site #201-598-37018) listed 03-21-1985
- 3) North Pumping Station (site # 201-598-37014) listed 01-02-1997 (HPR states 1996)

Also, we agree that St. Joseph County Bridge No. 933 (NBI No.11046, site #201-598-34018) is an "Outstanding" contributing resource within the Leeper Park Historic District and is the subject of the proposed project. We note that the cover letter and HPR state this bridge is individually eligible as a contributing resource to the Leeper Park Historic District. Since the criteria for contributing resources and individually eligible resources differs, we assumed that the intention was to state that the bridge is a contributing resource to the district and it also meets the criteria to be considered individual eligible. However, we do agree that the Indiana Historic Bridges Inventory states that St. Joseph County Bridge No. 933 is a "Contributing resource in a listed historic district."... "Therefore, this bridge was not reevaluated as part of this inventory project." We also concur that the Indiana Historic Bridges Inventory determined St. Joseph County Bridge No. 933 to be a Select bridge.

Based on the information provided to our office, we do not believe the characteristics that qualify the West North Shore District, Harter Heights Historic District, North Triangle Historic District, Samuel Leeper, Jr., House, or the North Pumping Station for inclusion in the National Register of Historic Places will be diminished as a result of this project.

Some indication of the condition of St. Joseph County Bridge No. 933 has been provided. However, please provide a more detailed account of the bridge's condition supported by clear photographs.

It appears that the proposed rehabilitation of St. Joseph County Bridge No. 933 is the "prudent and feasible" alternative which minimizes harm to the historic bridge. However, we note that in the cover letter's description of the preferred alternative, bullet point number four (4) states that the "spandrel walls will be replaced as follows" and that the subsequent language appears to be repairs instead. We also note that the HPR's description of the preferred alternative does not include the repair/replacement of the spandrel walls. Please clarify the scope of work. Additionally, preliminary design plans were provided, though it is unclear if these are the 30% plans. Per the Historic Bridges PA, it is our understanding that rehabilitation plans will be provided to the Indiana SHPO when the design is approximately 30% complete, 60% complete, and when final design plans are complete.

Under Attachment B of the Historic Bridges PA and Appendix 1 of the Historic Bridges PA Project Development Process, if a Select bridge is to be rehabilitated, the bridge owner is to consult with the Indiana SHPO to determine if photographic documentation is needed. We believe that due to the significance of St. Joseph County Bridge No. 933 and its integrity of design and materials, photographic documentation is needed. We recommend digital photographs be taken of the St. Joseph County Bridge No. 933, in accordance

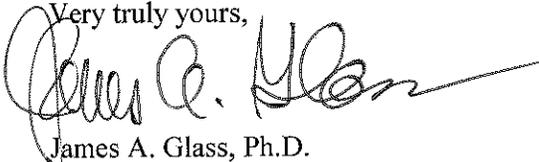
with the "State of Indiana, Indiana DNR - Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" (attached).

Once the indicated information is received, the Indiana SHPO will resume identification and evaluation procedures for this project. Please keep in mind that additional information may be requested in the future.

If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about archaeological issues please contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. If you have questions about buildings or structures please contact Toni Lynn Giffin at (317) 233-9636 or tgiffin@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA No. 12724.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:TLG:WTT:wt

Enclosure: (1)

emc: Staffan D. Peterson, Ph.D., Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation

South Bend Area Genealogical Society
c/o The Mishawaka-Penn-Harris Public Library
209 Lincoln Way E.
Mishawaka, IN 46544



January 23, 2012

Mr. Staffan D. Peterson
Manager, Cultural Resources Office,
INDOT Environmental Services
100 North Senate Avenue
Indianapolis, IN 46204

RE: Des. #1173149 Bridge Project, SR 933 over St. Joseph River

Dear Mr. Peterson:

Relative to your letter of December 21, 2011, and the proposed repairs to the Michigan Street/Leeper bridge over the St. Joseph River, our Society has reviewed your letter and attached documents and find no objections to the repair.

Thank you for consulting our Society regarding this matter.

Sincerely yours,


William H. Minish
President



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N758
Indianapolis, Indiana 46204

PHONE: (317) 232-5533
FAX: (317) 232-0238

Mitchell E. Daniels, Jr., Governor
Michael B. Cline, Commissioner

1 February, 2012

Mr. Robert E. Carter, Jr.,
Indiana State Historic Preservation Officer
Indiana Department of Natural Resources
Division of Historic Preservation & Archaeology
Indiana Government Center South, Rm. W 274
Indianapolis, IN 46204

Re: DHPA correspondence #12724
Route No.: SR 933/Michigan Street over the St. Joseph River
Des. No.: 1173149
Description: Bridge Project
City of South Bend, Portage Township, St. Joseph County

Dear Mr. Carter:

With regard to the above-referenced INDOT project, your office's letter dated January 20, 2012 (DHPA #12724), stated in part the following.

- ***"...Some indication of the condition of St. Joseph County Bridge No. 933 has been provided. However, please provide a more detailed account of the bridge's condition supported by clear photographs..."***

As was discussed in a January 26, 2012 phone conversation conducted between DHPA and INDOT staff members, detailed, descriptive information obtained from the bridge inspection report was included in the INDOT early coordination letter mailed to consulting parties on December 21, 2011. This information was and remains the most-specific and up-to-date available on the current conditions of the Leeper Bridge and the inherent need for the project. In an effort to augment the previously provided project information, please find enclosed--also per the January 26, 2012 phone conversation--25 representative bridge inspection photographs that highlight areas of deterioration necessitating the project work as specified under Des. #1173149.

- ***"...we note that in the cover letter's description of the preferred alternative, bullet point number four (4) states that the 'spandrel walls will be replaced as follows' and that the subsequent language appears to be repairs instead. We also note that the HPR's description of the preferred alternative does not include the repair/replacement of the spandrel walls. Please clarify the scope of work..."***

The INDOT LaPorte District and the project consultant have indicated that the provided scope of work contained in the December 21, 2011 early coordination packet was accurate. With reference to bullet point #4, the consultant has confirmed that the spandrel walls will be **repaired** and that cracks in stone veneer will be sealed with either Jahn mortar or epoxy. The remainder of the scope of work as provided in the December 21, 2011 mailing remains valid.

- ***"...Additionally, preliminary design plans were provided, though it is unclear if these are the 30% plans. Per the Historic Bridges PA, it is our understanding that rehabilitation plans will be provided to the Indiana SHPO when the design is approximately 30% complete, 60% complete and when the final plans are complete..."***

Given the aggressive schedule for this high priority project, the INDOT LaPorte District and the project consultant have provided INDOT-CRO with the final project design plans, which are enclosed in this mailing.

- ***"...Under Attachment B of the Historic Bridges PA and Appendix 1 of the Historic Bridges PA Project Development Process, if a Select Bridge is to be rehabilitated, the bridge owner is to consult with the Indiana SHPO to determine if photographic documentation is needed. We believe that due to the significance of St. Joseph County Bridge No. 933 and its integrity of design and materials, photographic documentation is needed. We recommend digital photographs be taken of the St. Joseph County Bridge No. 933, in accordance with the "State of Indiana, Indiana DNR-Division of Historic Preservation and Archaeology Minimum Architectural Documentation Standards" (attached)..."***

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D-81

D-9

INDOT-CRO respectfully proposes that required photographic documentation for this project not include the production of photographic prints. Rather, and as your office suggested in your January 18, 2011 letter for Des. #0800834 (SR 55 Bridge over Big Pine Creek; DHPA #9939), INDOT proposes that digital photographs/images taken of the SR 933 Bridge over the St. Joseph River (taken in accordance with the "State of Indiana Minimum Architectural Documentation Standards") be placed into the Indiana Memory Project collection, with the cooperation of the Indiana State Library. The Indiana SHPO would be provided a compact disc of these images, which would be taken prior to commencement of any project activities as specified under Des. #1173149. Your January 18, 2011 letter (DHPA #9939) referenced a precedent for this documentation method in the case of the US 27 bridge over the Whitewater River in Richmond. INDOT prefers this method of documentation and dissemination of the photographs because it is likely that more people will have easier access to them through the Indiana Memory Project than if the prints are filed in one or both of our offices.

Due to the previously referenced schedule for Des. #1173149, INDOT requests that you reply to this communication as soon as possible, preferably no later than the close of business on Wednesday, February 15, 2012. Doing so will allow INDOT to proceed with the Section 106 component of this high priority project, as Des. #1173149 is scheduled for letting in summer, 2012. Should you desire to meet to discuss the project and review the project plans with INDOT design staff, please let us know as soon as possible so that a meeting can be arranged. Please do not hesitate to contact Susan Branigin of this section at sbranigin@indot.in.gov or 317-234-0412 with any questions regarding these materials. Thank you in advance for your cooperation in this matter.

Sincerely,



Staffan D. Peterson, Ph.D.
Manager, Cultural Resources Office
INDOT Environmental Services

SDP/SRB/srb
Enclosures

CC: Mr. K-Todd Behling, INDOT-LaPorte District Project Manager
Mr. Travis Mast, INDOT LaPorte District Environmental Manager
Mr. Steve Weintraut, Butler, Fairman & Seufert Project Manager

ES Project File

Division of Historic Preservation & Archaeology-402 W. Washington Street, W274-Indianapolis, IN 46204-2739
Phone 317-232-1646-Fax 317-232-0693-dhpa@dnr.IN.gov



February 15, 2012

Staffan D. Peterson, Ph.D.
Manager, Cultural Resources Office
Environmental Services Division
Indiana Department of Transportation
100 North Senate Avenue, IGCN, Room N642
Indianapolis, Indiana 46204

Federal Agency: Federal Highway Administration ("FHWA")

Re: Additional project information regarding the SR 933 Bridge carrying Michigan Street over the St. Joseph River -scope undetermined- (Designation No. 1173149; DHPA No. 12724)

Dear Dr. Peterson:

Pursuant to Section 106 of the National Historic Preservation Act (16 U.S.C. § 470f), 36 C.F.R. Part 800, and the "Programmatic Agreement among the Federal Highway Administration, the Indiana Department of Transportation, the Advisory Council on Historic Preservation, the Indiana State Historic Preservation Officer regarding the implementation of the Federal Aid Highway Program in the State of Indiana," the staff of the Indiana State Historic Preservation Officer has conducted an analysis of the materials dated February 1, 2012, and received on February 2, 2012, for the above indicated project located in the City of South Bend, St. Joseph County, Indiana.

Thank you for providing the information we requested in our letter dated January 20, 2012.

We agree with INDOT that rather than documenting St. Joseph County Bridge No. 933 with photographic prints, we recommend that the Indiana Department of Transportation take digital photographs of the St. Joseph County Bridge No. 933, in accordance with the "State of Indiana Minimum Architectural Documentation Standards" and, with the cooperation of the Indiana State Library, place the images into the Indiana Memory Project collection and also provide the Indiana SHPO with a compact disc of the images.

We are satisfied with the final design plans for the rehabilitation of the SR 933 Bridge carrying Michigan Street over the St. Joseph River.

Additionally, based on the information provided to our office, we do not believe that there will be any alterations to the characteristics of the Leeper Park Historic District qualifying it for inclusion in or eligibility for the National Register (*see* 36 C.F.R. § 800.16[i]).

In regard to archaeological resources, as previously indicated, based upon the submitted information and the documentation available to the staff of the Indiana SHPO, we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places ("NRHP") within the proposed project area. However, this identification is subject to the project activities remaining within areas disturbed by previous construction of a recent and non-historical nature. If archaeological deposits are encountered from the post-contact period, they will be evaluated regarding their eligibility for the National Register of Historic Places in consultation with the staff of the Indiana SHPO. Please contact our office if such deposits are encountered. The archaeological recording must be done in accordance with the Secretary of the Interior's "Standards and Guidelines for Archaeology and Historic Preservation" (48 F.R. 44716) and a report of the archaeological documentation must be submitted to our office for review and comment.

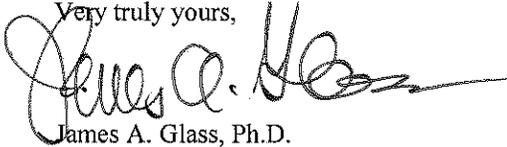
If any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and -29) requires that the discovery be reported to the Department of

Staffan D. Peterson, Ph.D.
February 15, 2012
Page 2

Natural Resources within two (2) business days. In that event, please call (317) 232-1646. Be advised that adherence to Indiana Code 14-21-1-27 and -29 does not obviate the need to adhere to applicable federal statutes and regulations.

If you have questions about archaeological issues please contact Wade T. Tharp at (317) 232-1650 or wtharp1@dnr.IN.gov. If you have questions about buildings or structures please contact Toni Lynn Giffin at (317) 233-9636 or tgiffin@dnr.IN.gov. Additionally, in all future correspondence regarding the above indicated project, please refer to DHPA No. 12724.

Very truly yours,



James A. Glass, Ph.D.
Deputy State Historic Preservation Officer

JAG:WTT:TLG:tlg

emc: Staffan D. Peterson, Ph.D., Indiana Department of Transportation
Mary Kennedy, Indiana Department of Transportation
Shaun Miller, Indiana Department of Transportation
Melany Prather, Indiana Department of Transportation

Des. #1173149
SR 933/Michigan Street Bridge
over St. Joseph River
South Bend, Portage Township, St. Joseph County



Magnified view of east spandrel wall in the center span. Note the vertical deflection of coping.



Magnified view of west spandrel wall in the center span. Note the vertical deflection of coping.

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



View of Abutment 4, Looking East.



View of the West (Downstream) Fascia, Looking Northeast.

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



View of Pier 3, Looking North.



Typical Crack on the Top Surface of the Encasement.

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



Timber Debris at Upstream Nose of Pier 3 from the South Face of the Pier, Looking East.



View of the North Face of Pier 3, Looking East.

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



View of Abutment 4, Looking North.



View of the Arch Joint Separation in the Superstructure at the Easternmost Joint of the Center Span, Looking South from Pier 3 (Toward Pier 2).

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



View of the Arch Joint Separation in the Superstructure at the Easternmost Joint of the Center Span, Looking East from North Side of Pier 3.



View of Pier 2, Looking South.

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



View of Typical Scaling at the Waterline at Abutment 4 near the Midpoint, Looking West.



View of Pier 2, Looking Northwest.



East Side End Post No. 1



East Side Rail Panel No. 2



East Side Intermediate Post No. 3



East Side Rail Panel No. 4



East Side Major Post No. 5



East Side Rail Panel No. 6



East Side Minor Post No. 7



East Side Rail Panel No. 8



East Side Minor Post No. 9



East Side Rail Panel No. 10



East Side Minor Post No. 11



East Side Rail Panel No. 12



East Side Minor Post No. 15



East Side Rail Panel No. 16



East Side Minor Post No. 13



East Side Rail Panel No. 14



East Side Minor Post No. 17



East Side Rail Panel No. 18



East Side Major Post No. 19



East Side Rail Panel No. 20



Bottom of arch ring in span B, looking NE at pier #3. The vertical difference in elevation between arch segments is probably due to settlement of the nose of the pier prior to the 2006 concrete encasement



Bottom of arch ring in span C, looking SE at pier #3. The vertical difference in elevation between arch segments is probably due to settlement of the nose of the pier prior to the 2006 concrete encasement



Bottom of arch ring span B. Note that most of the transverse cracks do not extend through the area that was patched in 2006. This indicates the cracks were present prior to the 2006 repairs.



Bottom of arch ring span B. Note that most of the transverse cracks do not extend through the area that was patched in 2006. This indicates the cracks were present prior to the 2006 repairs.

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



View of the Timber Debris at the Upstream Nose of Pier 3, Looking Northwest.



View of the Upstream Fascia, Looking North.

Indiana Department of Transportation Bridge Inspection Photos

NBI Number: 011046
Facility Carried: SR-933

Bridge Number: (933)31-71-03690 A
Feature(s) Intersected: ST. JOSEPH RIVER



View of the Timber Debris at the Upstream Nose of Pier 3, Looking North.



View of the Upstream Nose of Pier 2 from the Deck. Note the Riprap around the Steel Sheeting.

Comments:

The proposed rehabilitation of Leeper Bridge is restricted to the existing structure. No new right-of-way or undisturbed soils will be impacted by this project. In addition, two previous Phase Ia investigations examined areas adjacent to and under the southern end of the bridge (Waters and Zoll 1999, Cochran and Zoll 2002). Each reconnaissance noted previous disturbances around the bridge and neither found evidence of archaeological resources.

Recommendation

- The archaeological records check has determined that the project area has the potential to contain archaeological resources and a Phase Ia archaeological reconnaissance is recommended.
 - The archaeological records check has determined that the project area does not have the potential to contain archaeological resources and no further work is recommended before the project is allowed to proceed.
 - The Phase Ia archaeological reconnaissance has located no archaeological sites within the project area and it is recommended that the project be allowed to proceed as planned.
- The Phase Ia archaeological reconnaissance has determined that the project area includes landforms which
- have the potential to contain buried archaeological deposits. It is recommended that Phase Ic archaeological subsurface reconnaissance be conducted before the project is allowed to proceed.
- The Phase Ia archaeological reconnaissance has determined that the project area is within 100 feet of a cemetery and a Cemetery Development Plan is required per IC-14-21-1-26.5.

Cemetery Name:

Other Recommendations/Commitments:

Pursuant to IC-14-21-1, if any archaeological artifacts or human remains are uncovered during construction, demolition, or earthmoving activities, state law (Indiana Code 14-21-1-27 and 29) requires that the discovery must be reported to the Department of Natural Resources within two (2) business days. In that event, please call (317) 232-1646.

Attachments

- Figure showing project location within Indiana.
- USGS topographic map showing the project area (*1:24,000 scale*).
- Aerial photograph showing the project area, land use and survey methods.
- Photographs of the project area.
- Project plans (*if available*)

Other Attachments:

Des. #1173149
Archaeological Short Report
(Miller/October, 2011)
Conclusions Page

Appendix F-- Historic Properties Report (HPR) Conclusions Page

Conclusions

A long-form HPR was prepared for this project due to the fact that other above-ground resources within the APE--besides the subject structure--met the requisite age and/or conditions for NR-eligibility assessment. (As stated previously, Section 106 requirements for the SR 933/Michigan Street Bridge over the St. Joseph River (also known as Leeper Bridge; Bridge No. (933)31-71-3690-B/NBI No. 11046) are being met through the Historic Bridge Programmatic Agreement PDP.)

Of those above-ground resources present within the proposed project's APE, the following were analyzed for NR-eligibility assessment: **1) West North Shore Historic District** (WNSHD; #201-598-37001-020); **2) Northshore Triangle Historic District** (NSTHD); #201-598-36001-290); **3) Leeper Park Historic District** (NR-listed; LPHD; #201-598-34001-018); **4) Harter Heights Historic District (HHHD; #201-597/598-35001-277); 5) SBSS #201-597-10019** (Mason House; 140 E. North Shore Drive; ca.-1917 Prairie; rated "Notable"); **6) SBSS #201-597-10020** (O'Hara House; 134 E. North Shore Drive; ca.-1928 English Cottage; rated "Notable"); **7) SBSS #201-597-10021** (Taylor House; 128 E. North Shore Drive; ca.-1928 Dutch Colonial Revival; rated "Contributing"); **8) SBSS #201-597-10022** (Mattes House; 122 E. North Shore Drive; ca.-1907 Dutch Colonial Revival; rated "Notable").

As a result of the identification and evaluation efforts conducted for this project, this report recommends that the following properties remain eligible for NR-listing: **1) SR 933/Michigan Street/Leeper Bridge/ Bridge No. (933)31-71-3690-B/NBI No. 11046/#201-598-34018; 2) Leeper Park Historic District/#201-598-34001-018; 3) North Pumping Station/#201-598-34014; 4) Samuel Leeper, Jr. House/#201-598-37018.**

This report further recommends that the following surveyed South Bend historic districts are NR-eligible under Criterion A: Exploration/Settlement and Community Planning and Development and under Criterion C: Architecture: **1) Harter Heights Historic District/#201-597-598-35001-277; 2) West North Shore Historic District/#201-598-37001-020; 3) Northshore Triangle Historic District/#201-498-36001-290.**

Due to a collective lack of material integrity and/or historic significance, none of the surveyed South Bend Scattered Sites analyzed for this report **(1) SBSS #201-597-10019** (Mason House; 140 E. North Shore Drive; ca.-1917 Prairie; rated "Notable"); **2) SBSS #201-597-10020** (O'Hara House; 134 E. North Shore Drive; ca.-1928 English Cottage; rated "Notable"); **3) SBSS #201-597-10021** (Taylor House; 128 E. North Shore Drive; ca.-1928 Dutch Colonial Revival; rated "Contributing"); **4) SBSS #201-597-10022** (Mattes House; 122 E. North Shore Drive; ca.-1907 Dutch Colonial Revival; rated "Notable") were recommended as NR-eligible.

Appendix E

Red Flag Investigation



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204-2216 (317) 232-5348 FAX: (317) 233-4929

Mitchell E. Daniels, Jr., Governor
Michael W. Reed, Commissioner

Date: September 29, 2011

To: Ben Lawrence, PE, Administrator
Environmental Policy Section
Office of Environmental Services
Indiana Department of Transportation
100 N Senate Avenue, Room N642
Indianapolis, IN 46204

From: Travis Mast
Environmental Scientist, Production
La Porte District

Re: Des. #1173149
Bridge Rehabilitation
SR 933
St. Joseph County, Indiana

Narrative:

This project includes bridge rehabilitation over the St. Joseph River, 1.59 miles north of SR 23 on SR 933, in South Bend Indiana. Maps of the project area are attached.

SUMMARY

Infrastructure			
Other road projects	0	Airports	0
Cemeteries	0	Hospitals	1
Railroads	1	Recreational Facilities	6
Religious Facility	0	Schools	4
Trails	2	Pipelines	0

Explanation: Several Infrastructure items are within the one half mile search radius, yet do not occur within the immediate project area. These infrastructure items should not be adversely impacted as a result of this project.

Supervisory concurrence: _____

Water Resources			
Canal Routes – Historic	0	Canal Structures – Historic	0
Wetland Line	0	Floodplain-DFIRM	1
Rivers and Lakes	1 and 0	Wetlands	1
Wetland Points	0	Lakes – Impaired*	0
Streams – Impaired*	1	Cave Entrance Density	0
Sinkhole Areas	0	Karst Springs	0
NWI – Line	0	Sinking-Stream Basins	0

*Reason for impairment(if any):

Explanation: A few Water Resources exist within the one half mile radius investigation area. The St. Joseph River traverses through the project area. The La Porte District Environmental Section will prepare the necessary waters and wetland determination reports. Coordination will occur accordingly, with the ecology and permitting section of INDOT's ES for possible permitting.

Supervisory concurrence: _____

Mining/Mineral Exploration			
Oil Wells	0	Gas Wells	0
Mines – Surface	0	Mines – Underground	0
Petroleum Fields	0		

Explanation: No Mining and Mineral exploration items were found in the project area. Therefore no further investigations concerning Mining and Mineral exploration are recommended at this time.

Supervisory concurrence: _____

Ecological Information

- 1 aquatic species, 8 terrestrial species (vertebrate/invertebrate), 13 avian species, and 47 vascular plants from the state list
- 0 aquatic species, 2 terrestrial species (vertebrate/invertebrate), no avian species, and 1 vascular plant from the federal list
- no state and or federal habitats listed

Species are listed as being in St. Joseph County. Research into the Indiana Heritage Database revealed no state or federal threatened or endangered species within a one half mile radius of the project area. Therefore, no further investigation concerning endangered species is necessary. If it is discovered that the project area does contain endangered or threatened species, then the appropriate state and federal agencies must be contacted.

Cultural Resources

Cultural Resources was contacted on 9/29/2011 for historical analysis of the proposed project area. Their findings will be included in any future documentation.

Supervisory concurrence: _____

Hazmat Concerns			
Confined Feeding Operation	0	Construction Demolition Waste	0
Industrial Waste Sites	0	Leaking UG Storage Tanks	2
Open Dump Waste Sites	0	NPDES Pipe Locations	7
NPDES Facilities	0	Corrective Active Sites	0
Restricted Waste Sites	0	Septage Waste Sites	0
Solid Waste Landfills	0	Superfund Sites	0
Tire Waste Sites	0	Underground Storage Tanks	2
Voluntary Remediation Program	0	Brownfields	0
Waste Transfer Stations	0	Waste Treatment Storage Disposal	0
Manufactured Gas Plant	0	State Cleanup Site	0
Etiological Waste Site	0	Lagoon	0
IDEM 303d Listed Streams*	1	303d Listed Rivers*	0
303d Listed Lakes*	0		

Explanation: Seven LUST sites and two underground storage tanks are located within the half mile search radius. Given the distance from the project area these items should not be impacted by project activities.

Supervisory concurrence: _____

Recommendations

Upon review of the subject material no further investigations are necessary. Several Infrastructure items are within the one half mile search radius, yet do not occur within the immediate project area. These infrastructure items should not be adversely impacted as a result of this project.

A few Water Resources exist within the one half mile radius investigation area. The St. Joseph River traverses through the project area. The La Porte District Environmental Section will prepare the necessary waters and wetland determination reports. Coordination will occur accordingly, with the ecology and permitting section of INDOT's ES for possible permitting.

No Mining and Mineral exploration items were found in the project area. Therefore no further investigations concerning Mining and Mineral exploration are recommended at this time.

Species are listed as being in St. Joseph County. Research into the Indiana Heritage Database revealed no state or federal threatened or endangered species within a one half mile radius of the project area. Therefore, no further investigation concerning endangered species is necessary. If it is discovered that the project area does contain endangered or threatened species, then the appropriate state and federal agencies must be contacted.

Cultural Resources was contacted on 9/29/2011 for historical analysis of the proposed project area. Their findings will be included in any future documentation.

Seven LUST sites and two underground storage tanks are located within the half mile search radius. Given the distance from the project area these items should not be impacted by project activities.

However, should there be any additional soil disturbance or indications of hazardous materials, the Hazardous Materials Unit should be contacted immediately.

Supervisory concurrence: _____(Signature)

Travis Mast
Environmental Scientist

Graphics:

A map for each report section with a .5 mile radius buffer around all project area(s) showing all items identified as possible items of concern is attached.

Red Flag Investigation - Infrastructure

SR 933

1173149, Bridge Replacement

St. Joseph County, Indiana



Infrastructure Legend



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data

www.indianamap.org

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

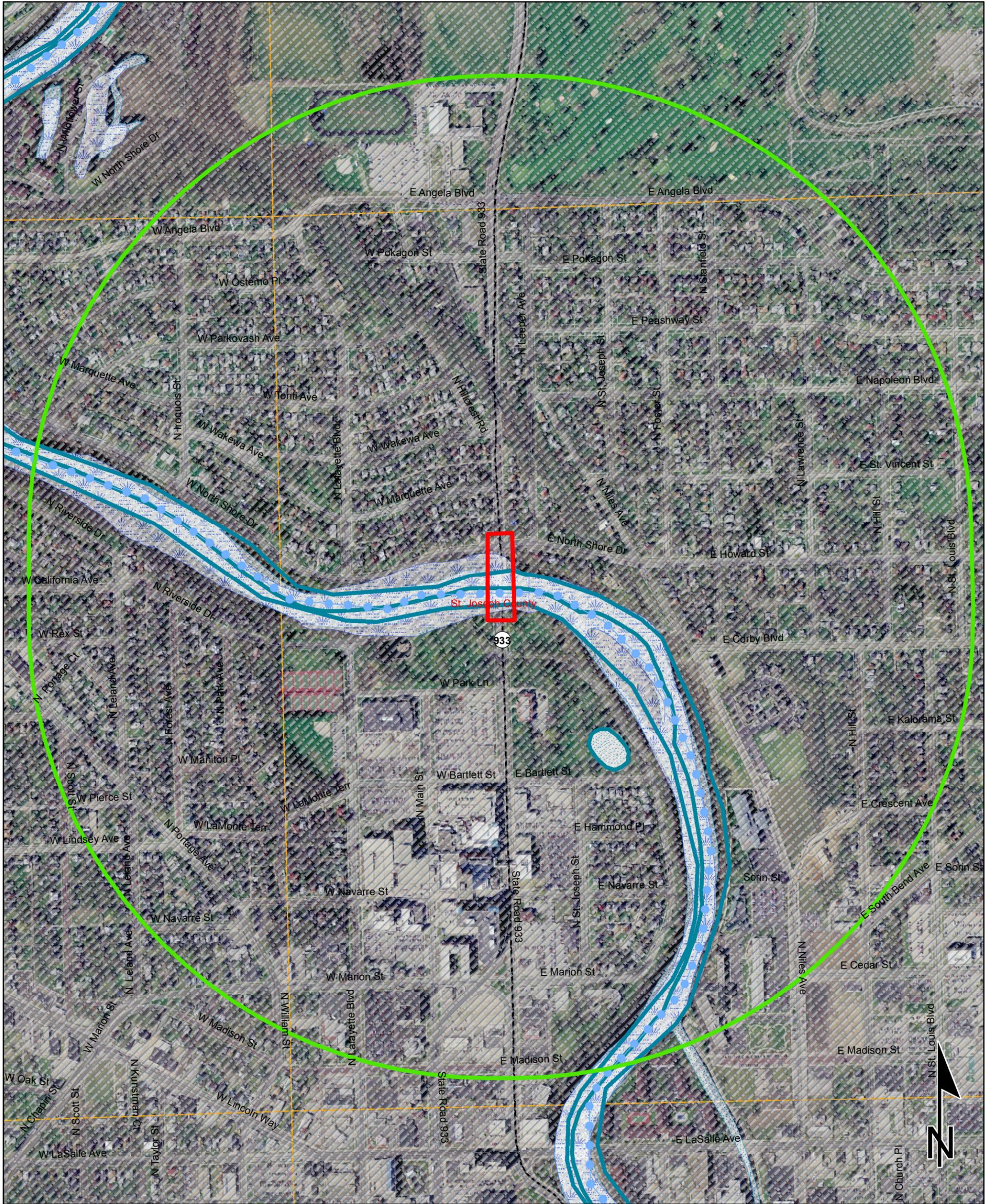
	Religious Facility		Recreation Facility		Project Area
	Airport		Pipeline		Half Mile Radius
	Cemeteries		Railroad		Interstate
	Hospital		Trails		State Route
	School		County Boundary		US Route
			Local Road		

Red Flag Investigation - Water Resources

SR 933

1173149, Bridge Replacement

St. Joseph County, Indiana



Water Resources Legend

0.1 0.05 0 0.1 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

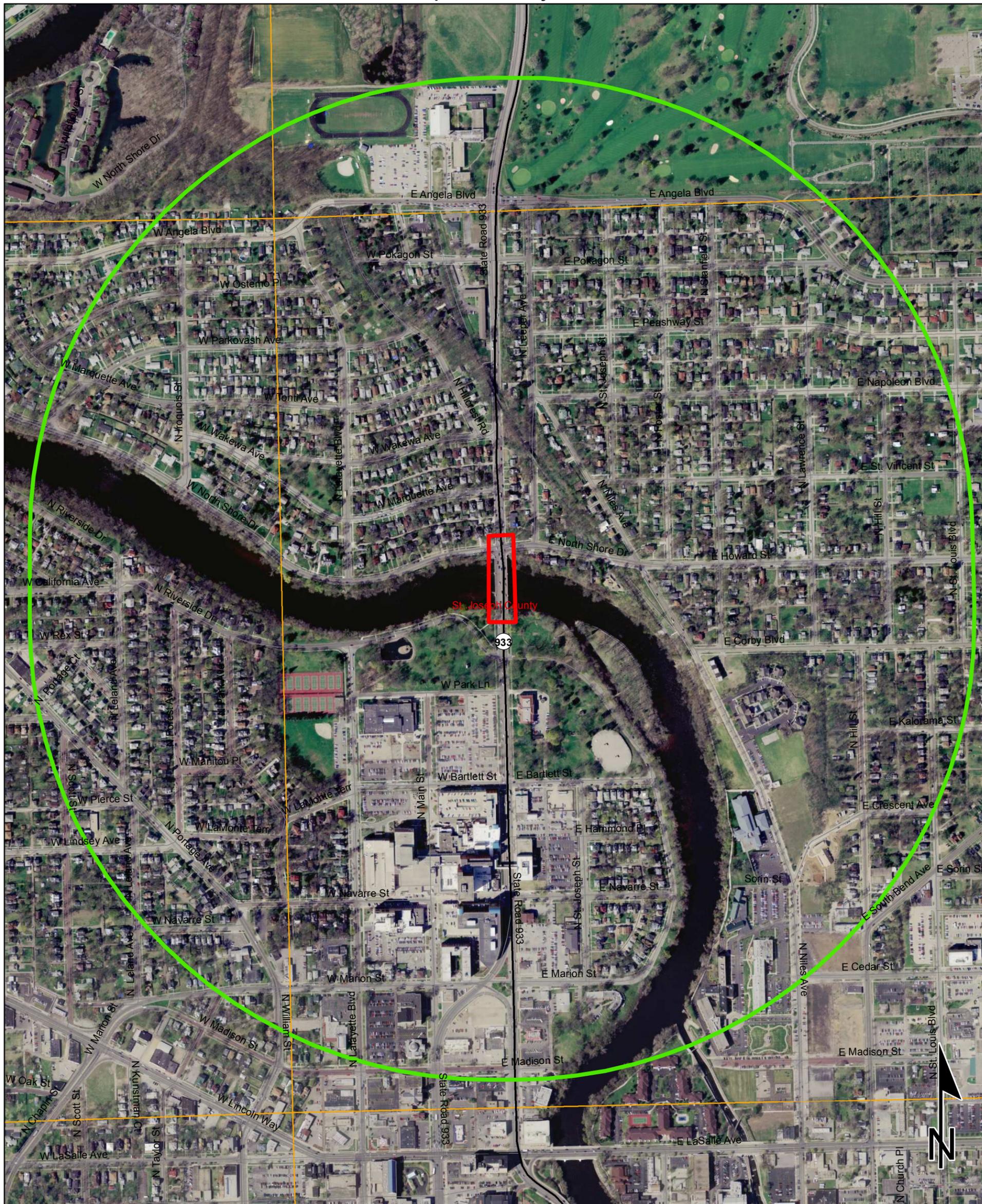
Orthophotography - Obtained from Indiana Map Framework Data

(www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

	Sinkhole Area		River		Interstate
	Sinking-Stream Basin		Canal Route - Historic		State Route
	NWI - Point		Wetlands		US Route
	Karst Spring		Lake - Impaired		Local Road
	Canal Structure - Historic		Lake		County Boundary
	NWI- Line		Floodplain - DFIRM		Half Mile Radius
	Stream - Impaired		Cave Entrance Density		Project Area

Red Flag Investigation - Mining/Minerals SR 933 1173149, Bridge Replacement St. Joseph County, Indiana



Mining/Mineral Exploration Legend



This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:

Non Orthophotography

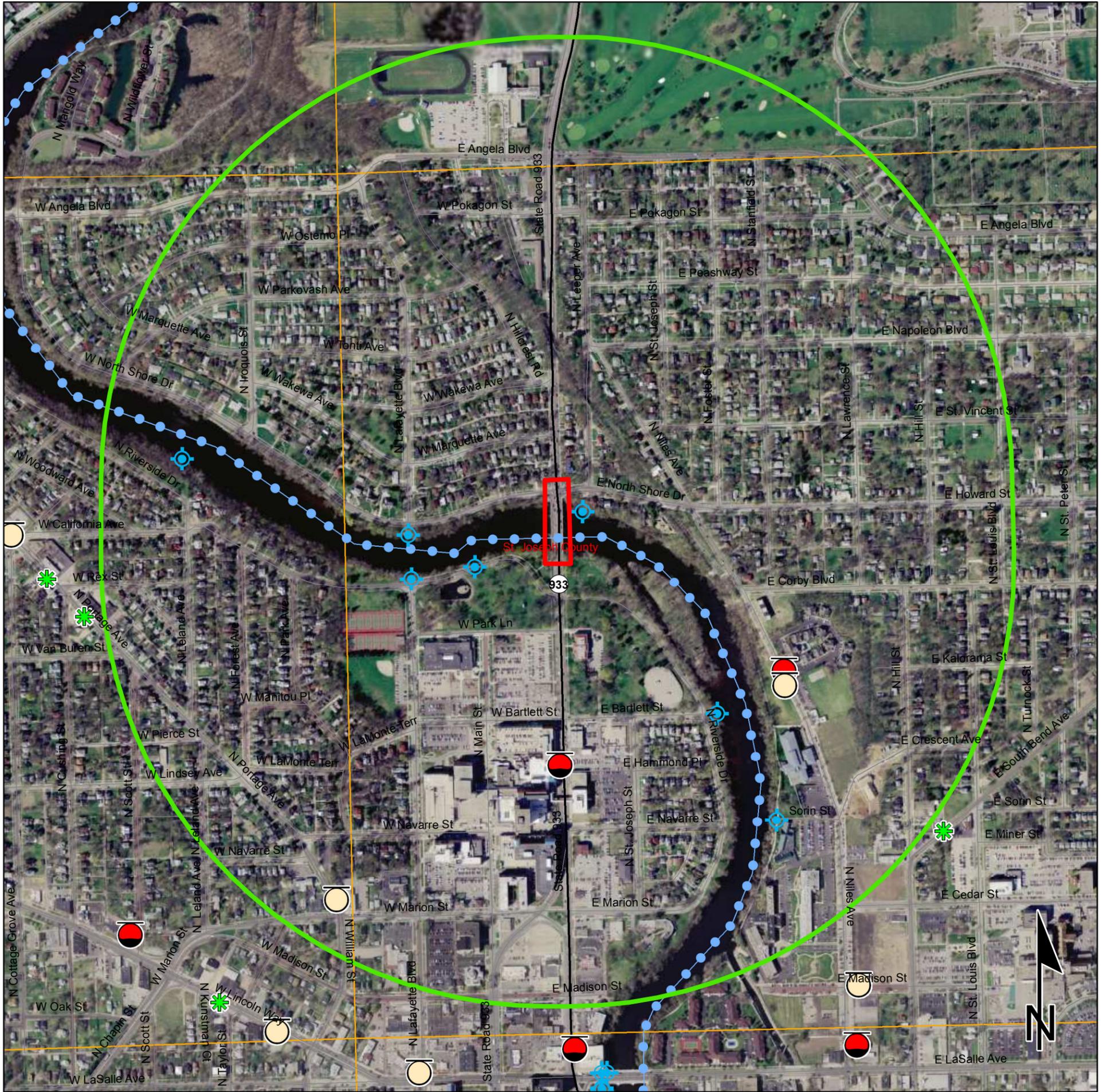
Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N **Map Datum:** NAD83

	Gas Well		Interstate
	Oil Well		State Route
	Mine - Surface		US Route
	Mine - Underground		Local Road
	Petroleum Field		Half Mile Radius
	County Boundary		Project Area

Red Flag Investigation - HazMat Concerns SR 933 1173149, Bridge Replacement St. Joseph County, Indiana



HazMat Concerns Legend

	Brownfield		Open Dump Waste Site		Superfund
	Corrective Action Sites		Restricted Waste Site		303d Listed Rivers
	Confined Feeding Operation		Septage Waste Site		303d Listed Lakes
	Construction/Demolition Site		Solid Waste Landfill		Interstate
	Leaking Underground Storage Tank		State Cleanup Site		State Route
	Etiological Waste Site		Tire Waste Site		US Route
	Lagoon		Waste Transfer Station		Local Road
	Manufactured Gas Plant		Waste Treatment Storage Disposal		County Boundary
	NPDES Facilities		Underground Storage Tank		Half Mile Radius
	NPDES Pipe Locations		Voluntary Remediation Program		Project Area

0.1 0.05 0 0.1 Miles

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.

Sources:

Non Orthophotography

Data - Obtained from the State of Indiana Geographical Information Office Library

Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)

Map Projection: UTM Zone 16 N Map Datum: NAD83

HAZARDOUS MATERIALS SITE VISIT FORM

Des # 1173149 Project # _____
 Road # SR 933 Type of Road Project Bridge Repair
 Description of area (either general location or exact location of parcel) _____
 Within the city of South Bend. The project is located adjacent to a residential area with a public park and waterworks building to the south.
 Person completing this Field Check Travis Mast - INDOT Environmental Manager

1. **Has a Red Flag Investigation been completed?** Yes No

Notes: As currently scoped no excavation is necessary. A red flag has been completed however it was not signed by central office due to limited disturbance in the scope of work.

2. **Right-of-Way Requirements:**

No New ROW Strip ROW Minor Take Whole Parcel Take Information Not Available

Notes:

3. **Land Use History and Development:** (Industrial, Light Industry, Commercial, Agricultural, Residential, Other – also, indicate source of data: visual inspection, aerial photos, U.S.G.S. topo maps, etc.)

Setting (rural or urban): Urban

Current Land Uses: Residential

Previous Land Uses: Residential

Adjacent Land Uses: Residential/Public Parks

Describe any structures on the property: The project involves a bridge, which is the only structure within the project limits.

4. Visual Inspection:	Property	Adjoining Property	Property	Adjoining Property
Storage Structures:			Evidence of Contamination:	
Underground Tanks	_____	_____	Junkyard	_____
Surface Tanks	_____	_____	Auto Graveyard	_____
Transformers	_____	_____	Surface Staining	_____
Sumps	_____	_____	Oil Sheen	_____
Ponds/Lagoons	_____	_____	Odors	_____
Drums	_____	_____	Vegetation Damage	_____
Basins	_____	_____	Dumps	_____
Landfills	_____	_____	Fill Dirt Evidence	_____
Other	_____	_____	Vent pipes or fill pipes	_____
			Other	_____

5. **Is a Phase I, Initial Site Assessment required?** Yes No

(Write additional notes on back)

Appendix F

Waters Report

WATERS OF THE U.S. DETERMINATION FORM
SR 933 in St. Joseph County, Indiana
Bridge Rehabilitation
Designation Number(s) 1173149
Prepared By: Travis Mast, INDOT, LaPorte District
October 25, 2011

Date of Waters Field Investigation: October 24, 2011

Location:

Section 1, Range 2E, Township 37N
South Bend, Indiana, Sound Bend West Quadrangle
St. Joseph County, Indiana

National Wetland Inventory (NWI) Information:

There are NWI wetlands identified near the project area.
NWI wetlands are on the attached maps.

Soils:

According to the Soil Survey Geographic (SSURGO) Database for St. Joseph County, Indiana, the project area does not contain nationally listed hydric soils.

Map Abbreviations Nationally Listed Hydric Soils

Attached Documents:

See the following pages for:

- A summary table of the waterways known to be present in the project area.
- Maps of the project area.
- Photographs of the project area.

Conclusions

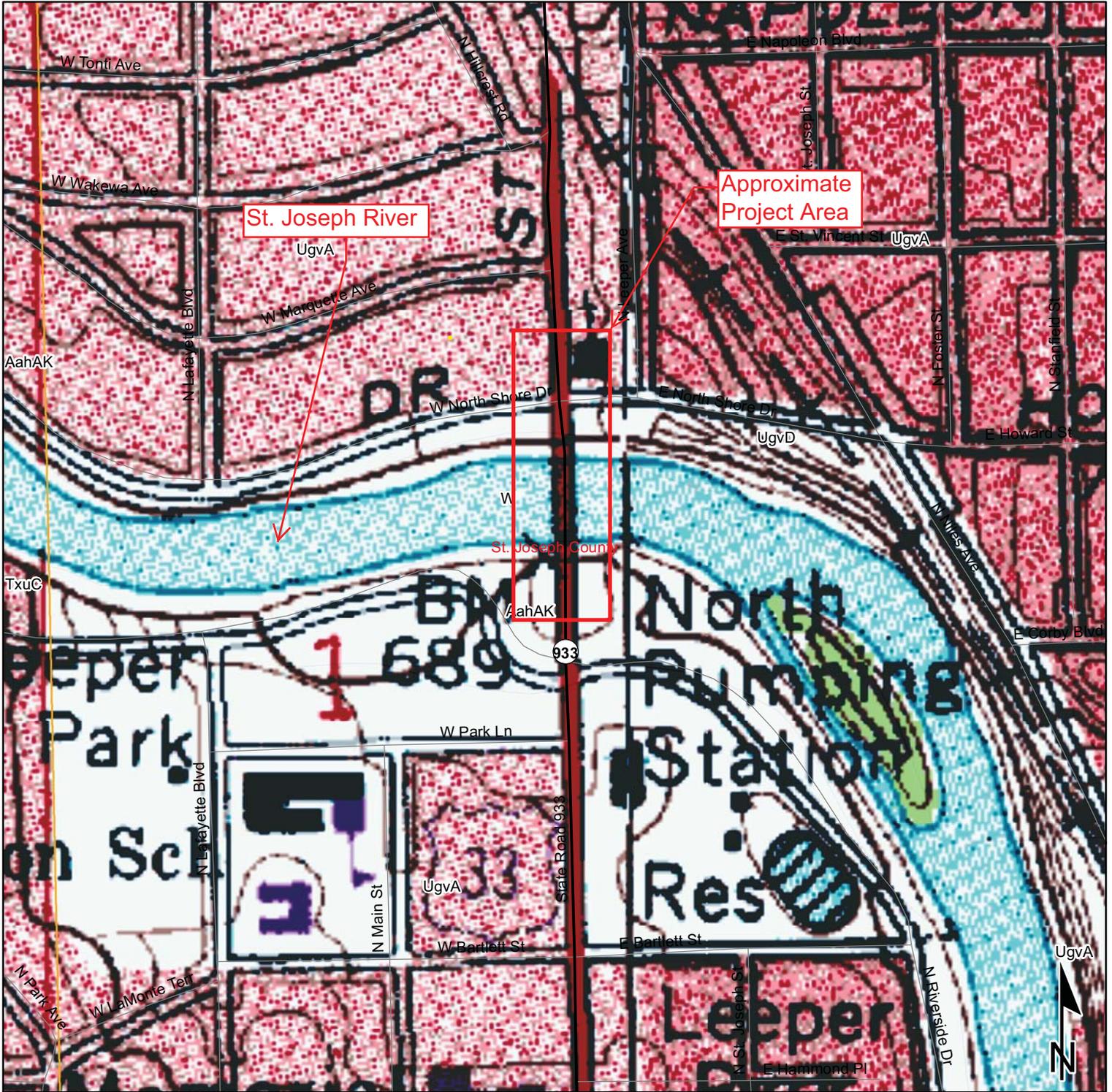
Based on the current scope of work, no impacts are anticipated below the OHWM. The scope of work includes repair and rehabilitation to bridge elements from the bridge deck. Field observations did reveal one waterway within the project area, the St. Joseph River. If impacts are necessary, then mitigation may be required. The INDOT Office of Environmental Services should be contacted immediately if impacts occur. The final determination of jurisdictional waters is ultimately made by the U.S. Army Corps of Engineers. This report is our best judgment based on the guidelines set forth by the Corps.

**Stream Summary Table
SR 933
Bridge Rehabilitation
St. Joseph County, Indiana
Designation Number(s): 1173149**

Stream Name	Photos	Lat/Long	OHW Width (ft)	Depth (inches)	USGS Blue-line?	Riffles? Pools?	Quality	Likely Water of U.S.? *
St. Joseph River	1-4	41.41 °N 86.15 °W	275 ft. estimate	36 in. estimate	Yes	Yes	High	Yes

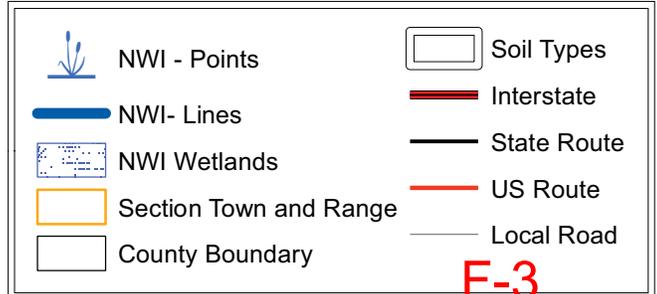
*Provide additional information under the Additional Comments and Justification heading within the Conclusions section if a stream has an Ordinary High Water Mark (OHWM) but is unlikely to be a Water of the U.S. or if a stream does not have an OHWM but is likely to be a Water of the U.S.

SR 933, St. Joseph County, Indiana Des 1173149, Bridge Rehabilitation



Sources: **Non Orthophotography**
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N **Map Datum:** NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



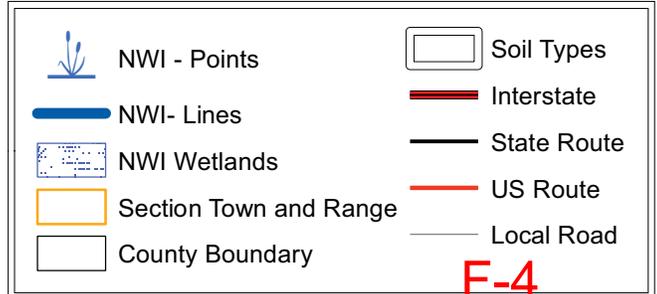
SR 933, St. Joseph County, Indiana Des 1173149, Bridge Rehabilitation



0.08 0.04 0 0.08 Miles
Scale 1:4,120

Sources: **Non Orthophotography**
Data - Obtained from the State of Indiana Geographical Information Office Library
Orthophotography - Obtained from Indiana Map Framework Data (www.indianamap.org)
Map Projection: UTM Zone 16 N Map Datum: NAD83

This map is intended to serve as an aid in graphic representation only. This information is not warranted for accuracy or other purposes.



Waters Report
October 24, 2011
SR 933, Bridge Repair
Des: 1173149
B-34153



Picture 1: St. Joseph River looking North.



Picture 2: SR 933 bridge over St. Joseph River looking South.



Picture 3: St. Joseph River looking East.



Picture 4: St. Joseph River looking West.

ATTACHMENT

PRELIMINARY JURISDICTIONAL DETERMINATION FORM

BACKGROUND INFORMATION

A. REPORT COMPLETION DATE FOR PRELIMINARY JURISDICTIONAL DETERMINATION (JD): October 24, 2011

B. NAME AND ADDRESS OF PERSON REQUESTING PRELIMINARY JD:
INDOT-Travis Mast, 315 E. Boyd Blvd., LaPorte, IN 46350

C. DISTRICT OFFICE, FILE NAME, AND NUMBER:LaPorte District,
1173149, SR 933, Bridge Repair

**D. PROJECT LOCATION(S) AND BACKGROUND INFORMATION:
(USE THE ATTACHED TABLE TO DOCUMENT MULTIPLE WATERBODIES
AT DIFFERENT SITES)**

State: IN County/parish/borough: St. Joseph City: South Bend
Center coordinates of site (lat/long in degree decimal format):
Lat. 41.41° N, Long. 86.15° W.

Universal Transverse Mercator:

Name of nearest waterbody: St. Joseph River

Identify (estimate) amount of waters in the review area:

Non-wetland waters: 275 linear feet: 275 width (ft) and/or acres.

Cowardin Class: Riverine

Stream Flow: Perennial

Wetlands:

Cowardin Class:

Name of any water bodies on the site that have been identified as Section 10 waters:

Tidal:

Non-Tidal:

E. REVIEW PERFORMED FOR SITE EVALUATION (CHECK ALL THAT APPLY):

Office (Desk) Determination. Date: October 25, 2011

Field Determination. Date(s): October 24, 2011

1. The Corps of Engineers believes that there may be jurisdictional waters of the United States on the subject site, and the permit applicant or other affected party who requested this preliminary JD is hereby advised of his or her option to request and obtain an approved jurisdictional determination (JD) for that site.

Nevertheless, the permit applicant or other person who requested this preliminary JD has declined to exercise the option to obtain an approved JD in this instance and at this time.

2. In any circumstance where a permit applicant obtains an individual permit, or a Nationwide General Permit (NWP) or other general permit verification requiring “pre-construction notification” (PCN), or requests verification for a non-reporting NWP or other general permit, and the permit applicant has not requested an approved JD for the activity, the permit applicant is hereby made aware of the following: (1) the permit applicant has elected to seek a permit authorization based on a preliminary JD, which does not make an official determination of jurisdictional waters; (2) that the applicant has the option to request an approved JD before accepting the terms and conditions of the permit authorization, and that basing a permit authorization on an approved JD could possibly result in less compensatory mitigation being required or different special conditions; (3) that the applicant has the right to request an individual permit rather than accepting the terms and conditions of the NWP or other general permit authorization; (4) that the applicant can accept a permit authorization and thereby agree to comply with all the terms and conditions of that permit, including whatever mitigation requirements the Corps has determined to be necessary; (5) that undertaking any activity in reliance upon the subject permit authorization without requesting an approved JD constitutes the applicant’s acceptance of the use of the preliminary JD, but that either form of JD will be processed as soon as is practicable; (6) accepting a permit authorization (e.g., signing a proffered individual permit) or undertaking any activity in reliance on any form of Corps permit authorization based on a preliminary JD constitutes agreement that all wetlands and other water bodies on the site affected in any way by that activity are jurisdictional waters of the United States, and precludes any challenge to such jurisdiction in any administrative or judicial compliance or enforcement action, or in any administrative appeal or in any Federal court; and (7) whether the applicant elects to use either an approved JD or a preliminary JD, that JD will be processed as soon as is practicable. Further, an approved JD, a proffered individual permit (and all terms and conditions contained therein), or individual permit denial can be administratively appealed pursuant to 33 C.F.R. Part 331, and that in any administrative appeal, jurisdictional issues can be raised (see 33 C.F.R. 331.5(a)(2)). If, during that administrative appeal, it becomes necessary to make an official determination whether CWA jurisdiction exists over a site, or to provide an official delineation of jurisdictional waters on the site, the Corps will provide an approved JD to accomplish that result, as soon as is practicable. This preliminary JD finds that there “*may be*” waters of the United States on the subject project site, and identifies all aquatic features on the site that could be affected by the proposed activity, based on the following information:

SUPPORTING DATA. Data reviewed for preliminary JD (check all that apply
- checked items should be included in case file and, where checked and requested, appropriately reference sources below):

- Maps, plans, plots or plat submitted by or on behalf of the applicant/consultant: .
- Data sheets prepared/submitted by or on behalf of the applicant/consultant.
 - Office concurs with data sheets/delineation report.
 - Office does not concur with data sheets/delineation report.
- Data sheets prepared by the Corps: .
- Corps navigable waters' study: .
- U.S. Geological Survey Hydrologic Atlas: .
 - USGS NHD data.
 - USGS 8 and 12 digit HUC maps.
- U.S. Geological Survey map(s). Cite scale & quad name: 7.5 Minute Series, South Bend West Quadrangle.
- USDA Natural Resources Conservation Service Soil Survey. Citation: Soil Survey Geographic Database, St. Joseph County.
- National wetlands inventory map(s). Cite name: USFWS NWI.
- State/Local wetland inventory map(s): .
- FEMA/FIRM maps: .
- 100-year Floodplain Elevation is: (National Geodetic Vertical Datum of 1929)
- Photographs: Aerial (Name & Date):
or Other (Name & Date):
- Previous determination(s). File no. and date of response letter: .
- Other information (please specify): .

IMPORTANT NOTE: The information recorded on this form has not necessarily been verified by the Corps and should not be relied upon for later jurisdictional determinations.

Signature and date of
Regulatory Project Manager
(REQUIRED)

Signature and date of
person requesting preliminary JD
(REQUIRED, unless obtaining
the signature is impracticable)

Site number	Latitude	Longitude	Cowardin Class	Estimated amount of aquatic resource in review area	Class of aquatic resource
St. Joseph River	41.41 N	86.15 W	Riverine	275 linear ft.	Non Section 10-Non Wetland

Appendix G

Section 6(f)

**United States Department of the Interior
National Park Service
Land & Water Conservation Fund**

Detailed Listing of Grants Grouped by County

Today's Date: 1/31/2012

INDIANA - 18

Page: 35

Grant ID & Element	Type	Grant Element Title	Grant Sponsor	Amount	Status	Date Approved	Exp. Date	Cong. District
ST JOSEPH								
34 - XXX	C	BENDIX PARK	ST. JOSEPH COUNTY PARK BOARD	\$306,039.79	C	12/30/1967	12/31/1970	3
46 - XXX	A	POTATO CREEK STATE PARK	DEPT. OF NATURAL RESOURCES	\$1,650,871.47	C	9/12/1968	12/31/1976	3
134 - XXX	D	MARTIN LUTHER KING PARK	SOUTH BEND PARK BOARD	\$39,754.00	C	4/17/1973	12/31/1975	3
138 - XXX	D	LEEPER PARK	SOUTH BEND PARK BOARD	\$14,630.97	C	2/16/1973	6/30/1975	3
142 - XXX	A	PAUL BOEHM PARK	SOUTH BEND PARK BOARD	\$62,500.00	C	2/23/1973	6/30/1975	3
195 - XXX	D	PIER PARK	SOUTH BEND PARK BOARD	\$111,071.20	C	4/28/1975	12/31/1977	3
235 - XXX	D	CENTRAL PARK	MISHAWAKA PARK BOARD	\$88,750.00	C	3/8/1976	12/31/1978	3
264 - XXX	C	D/NORTHSIDE PARK	MISHAWAKA PARK BOARD	\$155,128.50	C	2/3/1977	6/30/1980	3
271 - XXX	D	CENTRAL PARK/PHASE II	MISHAWAKA PARK BOARD	\$76,436.00	C	1/27/1977	6/30/1980	3
314 - XXX	D	ST. PATRICK'S COUNTY PARK-PHASE I	ST. JOSEPH COUNTY PARK BOARD	\$191,534.15	C	4/12/1978	6/30/1981	3
335 - XXX	D	MARY GIBBARD PARK	MISHAWAKA PARK BOARD	\$29,665.00	C	6/1/1979	6/30/1983	3
366 - XXX	D	EAST RACEWAY PARK	SOUTH BEND PARK BOARD	\$1,060,000.00	C	10/30/1980	12/31/1985	3
422 - XXX	C	D/BAUGO CREEK PARK-PHASE I	ST. JOSEPH COUNTY PARK BOARD	\$192,452.02	C	3/21/1984	6/30/1989	3
454 - XXX	R	CENTRAL PARK RENOVATION	MISHAWAKA PARK BOARD	\$69,881.15	C	5/22/1986	6/30/1990	3
481 - XXX	C	ABANDONED RAILROAD ACQ AND DEV	ROSELAND PARK BOARD	\$10,400.00	C	11/21/1991	12/31/1999	3
497 - XXX	C	F.D. SCHURZ, SR. ENVIR ED CTR PH II	ST. JOSEPH COUNTY PARK BOARD	\$57,483.00	C	9/23/1993	6/30/1999	2
543 - XXX	C	SCARBROUGH PARK AND WALKERTON TRAIL	WALKERTON PARK BOARD	\$147,771.00	C	1/28/2004	12/31/2008	2
550 - XXX	C	SPICER LAKE NATURE PRESERVE	ST. JOSEPH COUNTY PARK BOARD	\$200,000.00	C	2/15/2005	12/31/2009	2

ST JOSEPH County Total:

\$4,464,368.25

County Count:

18

**United States Department of the Interior
National Park Service
Land & Water Conservation Fund**

Detailed Listing of Grants Grouped by County

Today's Date: 1/31/2012

INDIANA - 18

Page: 36

Grant ID & Element	Type	Grant Element Title	Grant Sponsor	Amount	Status	Date Approved	Exp. Date	Cong. District
ST. JOSEPH								
151 - XXX	A	BELLEVILLE GARDENS ACQ.	SOUTH BEND PARK BOARD	\$17,500.00	C	12/31/1973	12/31/1975	3
223 - XXX	D	POTATO CREEK DEVELOPMENT	DEPT. OF NATURAL RESOURCES	\$2,001,889.53	C	10/23/1975	12/31/1979	3
274 - XXX	A	ST PATRICK'S FARM ACQUISITION	ST. JOSEPH COUNTY PARK BOARD	\$125,000.00	C	2/18/1977	6/30/1978	3
313 - XXX	C	D/SPICER LAKE NATURE PRESERVE	ST. JOSEPH COUNTY PARK BOARD	\$21,661.05	C	3/16/1978	12/31/1980	3
352 - XXX	D	ST. JOSEPH RIVER, MONROE/LASALLE	SOUTH BEND PARK BOARD	\$76,812.50	C	2/20/1979	12/31/1983	3
368 - XXX	C	D/ST. PATRICK'S COUNTY PK - PHASE II	ST. JOSEPH COUNTY PARK BOARD	\$396,789.09	C	2/12/1980	12/31/1984	3
376 - XXX	D	POTATO CREEK S.R.A. /PHASE II	DEPT. OF NATURAL RESOURCES	\$539,927.48	C	3/28/1980	12/31/1984	3
397 - XXX	C	D/TOLL ROAD FIELD	ROSELAND PARK BOARD	\$19,838.49	C	2/13/1981	12/31/1985	3
400 - XXX	C	D/SPICER LAKE NATURE PRESERVE-PH II	ST. JOSEPH COUNTY PARK BOARD	\$131,200.00	C	2/17/1981	12/31/1989	3
439 - XXX	C	EAST BANK TRAIL	SOUTH BEND PARK BOARD	\$100,000.00	C	2/16/1985	12/31/1989	3
475 - XXX	D	SPICER LAKE/F.D. SCHURZ ENVIR ED CTR	ST. JOSEPH COUNTY PARK BOARD	\$100,000.00	C	4/22/1991	6/30/1996	3
ST. JOSEPH County Total:				\$3,530,618.14		County Count:	11	
STARKE								
77 - XXX	D	BASS LAKE BEACH AND CAMPGROUND	DEPT. OF NATURAL RESOURCES	\$20,997.50	C	3/10/1971	12/31/1973	5
343 - XXX	C	D/HAMLET PARK	HAMLET PARK BOARD	\$11,715.00	C	2/9/1979	6/30/1983	3
STARKE County Total:				\$32,712.50		County Count:	2	

Appendix H

Public Involvement

Indiana Department of Transportation

County St. Joseph Route SR 933 Des. No. 1173149 Project No. _____

**FHWA-Indiana Environmental Document
CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM
GENERAL PROJECT INFORMATION**

Road No./County:	SR 933/St. Joseph County
Designation Number:	1173149
Project Description/Termini:	Bridge Rehabilitation and Repair on SR 933 over the St. Joseph river.

After completing this form, I conclude that this project qualifies for the following type of Categorical Exclusion (FHWA must review/approve if Level 4 CE):

X	Categorical Exclusion, Level 2 – The proposed action meets the criteria for Categorical Exclusion Manual Level 2 - table 1, CE Level Thresholds. Required Signatories: ESM (Environmental Scoping Manager).
	Categorical Exclusion, Level 3 – The proposed action meets the criteria for Categorical Exclusion Manual Level 3 - table 1, CE Level Thresholds. Required Signatories: ESM, ES (Environmental Services).
	Categorical Exclusion, Level 4 – The proposed action meets the criteria for Categorical Exclusion Manual Level 4 - table 1, CE Level Thresholds. Required Signatories: ESM, ES, FHWA.
	Environmental Assessment (EA) – EAs require a separate FONSI. Additional research and documentation is necessary to determine the effects on the environment. Required Signatories: ES, FHWA.

Note: For documents prepared by or for Environmental Services, it is not necessary for the ESM of the district in which the project is located to release for public involvement or sign for approval.

Approval _____
 ESM Signature _____ Date _____ ES Signature _____ Date _____

 FHWA Signature _____ Date _____

Release for Public Involvement JZM _____ 2-22-12
 ESM Initials _____ Date _____

RC 4-6-12 _____ _____
 ES Initials _____ Date _____
Certification of Public Involvement Ruthie Clark _____ 4-6-12
 Manager, Public Hearings Signature _____ Date _____

Note: Do not approve until after Section 106 public involvement and all other environmental requirements have been satisfied.

Reviewer Signature _____ Date _____

This is page 1 of 22 Project name: SR 933 over the St. Joseph river Date: 12/29/2011



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 232-6601
FAX: (317) 232-5349

Mitchell E. Daniels, Jr., Governor
Michael B. Cline, Commissioner

Wednesday, March 21, 2012

Dear Local Resident, Concerned Citizen, and Elected Public Official:

Welcome to the Indiana Department of Transportation's (INDOT) Public Hearing regarding a proposed S.R. 933 Bridge Rehabilitation and Repair Project over the St. Joseph River, located in South Bend, St. Joseph County.

The purpose of this public hearing is two-fold. Firstly, this is an opportunity to present the environmental document for this project which includes a summary of project alternatives studied, evaluated, and screened in an effort to identify a preferred alternative in moving forward with this proposed project. Secondly, this public hearing allows INDOT the opportunity to solicit public comment on the environmental document and preferred alternative.

There are several ways your comments may be presented this evening. You may submit comments in the following manner:

1. Complete one of the comment forms and return it to any of the INDOT representatives attending the public hearing. The comment forms are attached to this packet and extra copies are available on the table with the other handout materials.
2. Mail your comments to the Indiana Department of Transportation's Office of Public Involvement, 100 North Senate Avenue, Room N642 Indianapolis, IN 46204-2216. All comments submitted during the public hearing or during the public comment period will be reviewed, evaluated, and given full consideration by INDOT project officials during the decision making process. **INDOT respectfully request comments be submitted by Friday, April 6, 2012.**
3. Participate during the Public Comment Session and have your comments recorded for inclusion into the official public hearings transcript / public record.
4. Comments may be e-mailed to the INDOT Office of Public Involvement at: rclark@indot.in.gov.
5. A copy of the S.R. 933 Bridge Rehabilitation/Repair Project presentation is available via the INDOT website at: <http://www.in.gov/indot.htm> "Public Involvement" page then select "Project Documents".

INDOT sincerely appreciates the opportunity to meet with you tonight regarding the proposed S.R. 933 Bridge Rehabilitation and Repair Project over the St. Joseph River.

Rickie Clark, INDOT Office of Public Involvement
rclark@indot.in.gov (317) 232-6601

www.in.gov/dot/
An Equal Opportunity Employer

H-2

**S.R. 933 Bridge Rehabilitation and Repair
Project Over St. Joseph River**

South Bend, Indiana

Wednesday, March 21, 2012

5:30pm Presentation

St. Joseph County Public Library



WELCOME

- Rickie Clark, INDOT Office of Public Involvement
- Purpose/Explanation of Public Hearing
- Public Hearing Format
- Visit our sign-in table
- Informational Handouts
- Submitting public comments for hearings transcript
- Informal Q & A following formal presentation and comment session



Public Hearing

- Sign-in at attendance table to be added to project mailing list
- A public hearing notice was mailed to known property owners within project area
- Announcement of this hearing was posted to INDOT website at: <http://www.in.gov/indot.htm>
- Legal notice of public hearing published in South Bend Tribune on Tuesday, March 6, 2012 & Tuesday, March 13, 2012



Project Team

- Introduction of INDOT Project Officials
- Indiana Division Federal Highway Administration
- Michiana Area Council of Governments
- Butler, Fairman & Seufert
- Recognition of Elected Public Officials



Project Stakeholders

- Indiana Department of Transportation
- Indiana Division Federal Highways Administration
- Michiana Area Council of Governments
- St. Joseph County
- City of South Bend and local communities
- Elected Public Officials
- Community residents and citizens
- Emergency Services
- Commuters
- Schools
- Churches
- Community Organizations




Environmental Document

- Requirement of the National Environmental Policy Act (NEPA)
 - Requires INDOT to analyze and evaluate the impacts of a proposed project to the natural and socio-economic environments
 - Impacts are described in environmental document
 - Environmental document was released for public involvement February 2012



Environmental Document

- Development of document begins once purpose of need for the project has been clearly identified
- Develop a number of possible alternatives including a "Do Nothing" alternative as a baseline for comparison
- Screen alternatives to identify a preferred alternative
- Solicit public comment on environmental document and preliminary design plan
- Solicit, address, and fully consider public comments as part of decision making process



Elements of the Environmental Documentation

<ul style="list-style-type: none"> Real Estate Air Quality Noise Farmland Hazardous Materials Historic/Archaeological Cultural Resources 	<ul style="list-style-type: none"> Community Impacts Wetlands and Waterways Floodplains Endangered Species
---	--



Project Purpose & Need / Alternatives

- Repair deficiencies throughout structure
 - Cracking of arch ring
 - Deterioration of bridge railings & post stones
- Ensure a safe transportation route at this location
- Alternative 1: Do Nothing – did not meet purpose and need
- Alternative 2: Bridge Replacement – determined to be unnecessary and would result in more project costs than the preferred alternative
- Alternative 3: repair and rehabilitation meets purpose and need for the project



SR 933 /Michigan Ave./Leeper Bridge is historic

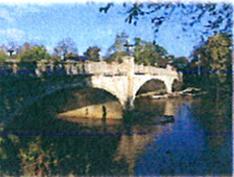



■ Part of the Leeper Park Historic District (LPHD), which is listed in the National Register of Historic Places (NR)

■ Individually eligible for NR-listing under Criterion C: Landscape Architecture & Engineering



Results of the Historic Bridge Inventory



The SR 933/Michigan Ave./Leeper Bridge is a "Select" bridge

"Select" bridges are:

- ❖ Excellent examples of a given type of historic bridge
- ❖ Most-suitable for preservation



Select Bridges

- Alternatives Analysis
 - Preferred Alternative
 - Rehabilitation for continued vehicular use



Historic Bridge Project Development Process

- Seek consulting party comment
 - No objection to project to date
- Identify preferred alternative
 - Rehabilitation/repair
- Hold Public Hearing
 - March 2012
- Environmental Document Approval
 - After hearing comment period




Other NR-listed Resources in Project Area

- Samuel Leeper House
- Leeper Park Historic District
- North Pumping Station
- Listed in National Register of Historic Places




NR-eligible Districts in project area



West North Shore Historic District:
 Pictured: Lent House; 107 Northshore Drive; ca.-1903 Neoclassical, rated "Outstanding").

Harter Heights Historic District:
 Pictured: Van Dlen House; 127 E. Northshore Drive; ca.-1921 Colonial Revival; rated "Outstanding").



NR-eligible Districts in project area



North Shore Triangle Historic District

Pictured: Woodward House, 102 Marquette Avenue; ca.-1902 Queen Anne



Project Effects

- The project will rehabilitate the Leeper Bridge according to the Secretary of Interior's Standards for Rehabilitation
- The project will not adversely impact the Leeper Bridge; Leeper Park Historic District; Samuel Leeper House; North Pumping Station; Harter Heights Historic District; West North Shore Historic District; North Shore Triangle Historic District
- Project will result in "No Adverse Effect" to historic resources



Design Detail

History

- ◆ 3 span Reinforced Concrete Arch
- ◆ Built in 1914 (\$94,000)
- ◆ Designed by Charles Cole
- ◆ Bedford Limestone Spandrel Wall Façade, Pedestals and Railings
- ◆ Rehabilitated Several Times
- ◆ Limestone Railing Replaced



Design Detail

Current Project Needs

- ◆ Floor Slab Cracking
- ◆ Arch Ring Cracking
- ◆ Rail Leaning



Design Detail

Current Project Needs

- ◆ Epoxy Inject Arch Ring
- ◆ Seal Cracks in Limestone Veneer



Design Detail

Current Project Needs

- ◆ Install Fiberglass Reinforcing on Arch Ring



Design Detail

Current Project Needs

- ◆ Railing will be Removed and Replaced to Match Existing



Maintenance of Traffic

Current Project Needs

- ◆ Traffic will be Phased
- ◆ Northshore Boulevard will be Closed



Project Schedule

- Public Hearing today
- Finalize project design and environmental document: Spring 2012
- Contract letting: May 2012
- Anticipated Project Start: May 2012
- Anticipated Project Completion: August 31, 2012



Submit Public Comments

- Submit public comments using the options described in 1st page of information packet
 - Public Comment Form
 - Via e-mail
 - Participating during public comment session via microphone
 - Verbal comments recorded and transcribed for inclusion into public hearings transcript
- **INDOT respectfully requests comments be submitted by Friday, April 6, 2012**
- All comments submitted will become part of public record, entered into transcript, reviewed, evaluated and given full consideration during decision making process



Project Resource Location

- INDOT LaPorte District Office, 315 East Boyd Blvd., LaPorte, Indiana
- Construction, maintenance and development activities in Northwest Indiana
- Customer Service Center (855) 464-6368
LaPorteDistrictCommunications@indot.in.gov
- Robert Alderman, Acting LaPorte District Deputy Commissioner
 - Jim Pinkerton - Communications Director
 - Matt Deitchley - Media Relations Director
 - Travis Mast - Environmental Manager
 - Mike Monahan - Area Engineer



THANK YOU

- Please visit with INDOT project officials
- Project Open House
 - Project maps, displays, public comments table
 - INDOT project officials available for informal Q & A
- Thank you for your attendance this evening



ATTENDANCE SHEET

PLEASE PRINT

D

NAME	STREET ADDRESS	CITY and ZIP	EMAIL ADDRESS
MARETHA KELINE	113 W. NORTH SHORE DR.	SOUTH BEND 46617	
GERALD KLINE ANS MONTGOMERY	113 W. NORTH SHORE 222 Marquette Ave	SOUTH BEND 46617 SBN 46617	montgomeryj.ano@gmail.com
KATHY RERR	103 W. NORTH SHORE	SOUTH BEND 46617	
Jessica Clark	227 W. Jefferson Blvd.	South Bend	jclark@west-joseph.in.us
Jean Crumlish	1091 Riverside Drive	South Bend	riversidedrive@stglob.net
Brendan Crumlish	"	South Bend	crumlishbeaol.com
CATHERINE D. FOSTER	708 N. ST. JOSEPH	SOUTH BEND	hwsocgd@hotmail.com
Lou Hasis		Indianapolis	louhasis@dot.gov
Tom Price	1049 Riverside Dr.	South Bend 46616	ThomasKPrice@comcast.net
Victor Karcewski	106 MARQUETTE Ave	46617	Robik317@SBCGlobal.net
 <p>Richard W. Mah 603 Parkview Ave South Bend, IN 46617</p>			rmaal_1@yahoo.com

SIGN IN SHEET

PLEASE PRINT

2

NAME	ADDRESS	E-MAIL
1 Patrick Henthoin	227 W. Jefferson #1316 South Bend, IN 46601	pkenthoin@southbend.in.gov
2 Barbara S. T. Taw	528 O Street P1 So Bend, IN 46617	BOUTTAUS28@gmail.com North Shore Triangle Neighborhood List Svc
3 Gary G. Lot	227 W. Jefferson #1316 South Bend, IN 46601	ggiloto@southbend.in.gov
4		
5		
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7		
8		
9		
10		

SIGN IN SHEET

3

PLEASE PRINT

NAME	ADDRESS	E-MAIL
1 Michael Favorite	123 W North Shore Ave 17	mfavorite@nd.edu
2		
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Name	Comment Method	Question/Comment Summarized	Response
Gerald Kline	Oral	What tests indicate the stress that a bridge can safely tolerate?	Qualified bridge design engineers have monitored the structure through onsite inspections and documented bridge inspection reports.
Gerald Kline	Oral	Can heavy weight trucks of eighty thousand pounds remain on present detour routes?	These comments were taken under advisement.
Gerald Kline	Oral	How closely will INDOT monitor bridge conditions after repairs?	Continual monitoring of the bridge will take place.
Kathy Kerr	Oral	If you're going to fix this bridge let's keep it historic and let it stay in good condition and please get the traffic from those heavy vehicles using the bypass that was put in.	This will be taken into consideration during design.
Barbara Sutton	Email	Comment received concerning the bridge rehabilitation project and timing of a local marathon.	This will be taken into consideration during design and construction.
Stephen Smith	Email	Comment regarding maintenance of traffic after construction.	This will be taken into consideration during design and construction.
Susan Searight	Email	Comment regarding current and maintenance of traffic during construction.	This will be taken into consideration during design and construction.

Heather Moriconi	Email	Comment regarding maintenance of traffic during construction.	This will be taken into consideration during design and construction.
John Wilford	Email	Comment and questions regarding maintenance of traffic during construction	This will be taken into consideration during design and construction.
Brendan Crumlish	Mail	Comment regarding railing repair.	This will be taken into consideration during design.
Aaron Frazee	Email	Comment regarding long-term use, previous repairs, logistics, and maintenance of traffic.	This will be taken into consideration during design.
Jessica Clark, PE	Written	General statement in support of the project.	
N/A	Written	Comment regarding maintenance of traffic and general maintenance concerns.	This will be taken into consideration during design.

Public Hearing for proposed SR 933 bridge rehabilitation & repair project of the St. Joseph River, located in South Bend, St. Joseph County.

Verbal public comment submitted on 3-21-12

St. Joseph County Public Library

6:00pm

Gerald Kline: Thank You. My name is Gerald Kline and several of my concerns have already been addressed but I'm going to stay with my text. My name is Gerald Kline and my home is at 113 West North Shore Drive.....slightly more than 100 feet from the north end of Leeper Bridge. We've lived at this address for more than 41 years, working when possible on the preservation of our historic house. Unfortunately, heavy truck traffic on Michigan Street has not been good for our house or for the bridge. Our file of correspondence with INDOT, along with related articles, spans more than 30 years. All of the material related to damage. If vibrations from heavy trucks cause cracks in our house, it is no surprise that the bridge is needy. Extensive bridge work was done in 1977 and 2006 and the cutter reproduction lights were installed in 2007. After repairs are completed this spring, several questions remain. First, what tests indicate the stress that a bridge can safely tolerate? Number two. Can heavy weight trucks of eighty thousand pounds remain on present detour routes? Three.....how closely will INDOT monitor bridge conditions after repairs? The architectural, aesthetic and historic values of the Leeper Bridge are without question, after nearly 100 years of service, the bridge deserves whatever care is necessary to preserve its integrity.....thank you.

Kathy Kerr: My name is Kathy Kerr and I live at 103 West North Shore Drive.....I am at the corner at 933 and North Shore. I fully agree with Mr. Kline regarding all of the problems that have occurred to his house.....he may be 100 feet away from the intersection..... we are practically in the middle of the intersection. I have been very bothered ever since the bypass went in....at the time the bypass went in we were told that trucks would not be using 933 for through traffic. If they were going downtown, that was a different situation, but there would be no through trucks and they would be made to use the bypass.....and that has not happened at all. The situation now still exists even with all of the signage that has been put up.....and I might add not really in the appropriate places by the state, so that trucks weighing over the twenty thousand pound limit would not use the bridge.....that's not been enforced at all. I agree with Mr. Kline as far as cracks entering into his house.....we lost one ceiling and of course we've gained our cracks in our house also. My concern is.....if you're going to fix this bridge let's keep it historic and let it stay in good condition and please get the traffic from those heavy vehicles using the bypass that was put in.....that's my comment.

INDOT project officials entertain questions during informal question and answer session.

Clark, Rickie

From: Barbara Sutton [bsutton528@gmail.com]
Sent: Wednesday, March 21, 2012 9:59 PM
To: Clark, Rickie
Cc: Peter Buttigieg
Subject: Leeper Bridge DES # 1173149Conflict with Sunburst

Mr. Clark,

The Sunburst Marathon is running in South Bend on June 2, 2012. Somewhere between 6,000 and 10,000 runners will be crossing Leeper Bridge and turning west on North Shore Drive to run through the North Shore Triangle Neighborhood as part of the Marathon. You may have to delay your project start until June 4. Please contact the Sunburst Marathon organizers for details. This has a large economic impact on South Bend and it important that it runs smoothly.

--
Barb Sutton

Clark, Rickie

From: Stephen Smith [futbollegends@gmail.com]
Sent: Thursday, March 22, 2012 6:11 PM
To: Clark, Rickie
Subject: rerouting trucks from Angela and elsewhere

I just read my north shore triangle neighborhood email from Barb Sutton which had your link. I usually try to put myself in the place of the other guy. Rerouting trucks after construction is complete will cost corporations and drivers money and time. While it may benefit those of us who use these roads frequently, I don't think it is fair to put every truck driver out of the way for me and the others in the neighborhood. These people are hardworking and I don't want to make life harder on them so that they have to spend less time with their families. I think they have a right to use roads and bridges to save them time and money (gasoline) just like I do.

--

Cheers!
-Steve

Clark, Rickie

From: Susan [sears0127@gmail.com]
Sent: Thursday, March 22, 2012 9:00 PM
To: Clark, Rickie
Cc: mayorpete@southbendin.gov; bsutton528@gmail.com
Subject: # 1173149

I know my neighbors have already shared some concerns about the repair planned for this summer on the nearby Leeper Bridge in South Bend. My biggest concerns about the bridge & project are these: one, I am very concerned about the re-routing of traffic through my neighborhood, the Northshore Triangle, which is full of children, playing in and around the streets. And second, what I see happening already, is the short cut some semi truck drivers are taking through a residential neighborhood that leads to ours, off of Lincolnway West. I have seen truckers turn onto Diamond from LWW, then through onto Angela, and up to 933/N. Michigan Street. There are children throughout the area on Diamond Avenue besides all the children in my neighborhood, and I fear a child will be hurt or killed if this is not curtailed.

Thank you for considering my comments as you prepare to repair this historic bridge. Susan Searight, 329 Tonti Street, South Bend, IN. 46617

Clark, Rickie

From: Heather Moriconi [heather.moriconi@att.net]
Sent: Friday, March 23, 2012 6:43 AM
To: Clark, Rickie
Subject: 1173149

To whom it may concern,
My family and I are residents of the North Shore Triangle. We have a five year old son. Please consider, when re-routing the traffic from the bridge repairs, directing traffic to Angela (not one of the side streets like Marquette or Wakewa. There are many, many families that live along these narrow streets. It would be very dangerous for children (and adults) if heavy traffic started using these streets to cross over to Portage. In fact, is there any way to prevent traffic (with signs, news coverage, etc.) from using the side streets?

Thank you for your help.

A concerned parent,
Heather Moriconi

Clark, Rickie

From: John Wilford [jwilfs@msn.com]
Sent: Friday, March 23, 2012 7:55 AM
To: Clark, Rickie
Subject: 933 Bridge Rehabilitation

Dear Rickie Clark,

Comments on the S.R. 933 Bridge Rehabilitation Project

I am in favor of the project, but I have a few concerns.

1. What will the impact be on the detour routes? I live in the North Shore Triangle area and how will the alternate routes affect streets like Angela and Riverside?
2. On the East side of the bridge, how will streets like Niles and Eddy be affected?
3. I would like to see trucks permanently detoured around the bridge as bringing the trucks back to use the bridge would weaken the structure again.
4. Proper detour signs are a must and should be well in advance of the construction area.

Sincerely,

John Wilford
520 Parkovash Ave.
South Bend IN 46617-1032

Clark, Rickie

From: Clark, Rickie
Sent: Thursday, March 22, 2012 9:37 AM
To: 'Barbara Sutton'
Cc: Peter Buttigieg
Subject: RE: Leeper Bridge DES # 1173149Conflict with Sunburst

Good Morning Ms. Sutton,

Thank you for the e-mail and information regarding the marathon. INDOT and the City of South Bend will coordinate activities in order to minimize construction impacts as much as possible. While INDOT did present a proposed/tentative schedule, the actual construction implementation will take place when INDOT awards the construction contract. During that process, INDOT with coordination and discussion with the city and also St. Joseph County will develop a plan for the contractor so that these very important events are not negatively impacted. These types of issues are typically discussed at the pre-construction meeting(s) with INDOT, the city, county, utility providers (if applicable), contractor and others so that provisions can be written in and agreed upon prior to construction beginning.

I have made a copy of the e-mail below and it will become part of the public hearings transcript which is what INDOT will review, evaluation and fully consider as decisions are part. Also, as a reminder, INDOT will prepare responses to public comments received during the comment period and make those responses available for public review once the environmental document is approved. INDOT will use the sign-in sheets from last evening and addresses we collect from letters/e-mails submitted to us over the next couple of weeks, to contact everyone and provide instruction as to where the final environmental document may be viewed. We will make the document (final environmental) available on our website and also send several copies other to the library where the hearing was held.

We anticipate having an approved document ready for public consumption by mid- April.

Thank you again for contacting INDOT and submitting comments regarding the SR 933 Bridge project.

Sincerely,

Rickie Clark, Indiana Department of Transportation
(317) 232-6601

From: Barbara Sutton [<mailto:bsutton528@gmail.com>]
Sent: Wednesday, March 21, 2012 9:59 PM
To: Clark, Rickie
Cc: Peter Buttigieg
Subject: Leeper Bridge DES # 1173149Conflict with Sunburst

Mr. Clark,

The Sunburst Marathon is running in South Bend on June 2, 2012. Somewhere between 6,000 and 10,000 runners will be crossing Leeper Bridge and turning west on North Shore Drive to run through the North Shore Triangle Neighborhood as part of the Marathon. You may have to delay your project start until June 4. Please contact the Sunburst Marathon organizers for details. This has a large economic impact on South Bend and it important that it runs smoothly.

--
Barb Sutton

Clark, Rickie

From: Clark, Rickie
Sent: Friday, March 23, 2012 8:24 AM
To: 'Stephen Smith'
Subject: RE: rerouting trucks from Angela and elsewhere

Good Morning Mr. Smith,

Thank you for submitting comments in regards to the SR 933 Bridge rehabilitation and repair project. I have made a copy of the e-mail below and it will become part of the public hearings transcript which is what INDOT will review, evaluation and fully consider as decisions are part. Also, as a reminder, INDOT will prepare responses to public comments received during the comment period and make those responses available for public review once the environmental document is approved. INDOT will use the sign-in sheets from Wednesday evening and addresses we collect from letters/e-mails submitted to us over the next couple of weeks, to contact everyone and provide instruction as to where the final environmental document may be viewed. We will make the document (final environmental) available on our website and also send several copies to the library where the hearing was held.

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Sincerely,

Rickie Clark, Indiana Department of Transportation
(317) 232-6601

From: Stephen Smith [<mailto:futbollegends@gmail.com>]
Sent: Thursday, March 22, 2012 6:11 PM
To: Clark, Rickie
Subject: rerouting trucks from Angela and elsewhere

I just read my north shore triangle neighborhood email from Barb Sutton which had your link. I usually try to put myself in the place of the other guy. Rerouting trucks after construction is complete will cost corporations and drivers money and time. While it may benefit those of us who use these roads frequently, I don't think it is fair to put every truck driver out of the way for me and the others in the neighborhood. These people are hardworking and I don't want to make life harder on them so that they have to spend less time with their families. I think they have a right to use roads and bridges to save them time and money (gasoline) just like I do.

--
Cheers!
-Steve

Clark, Rickie

From: Clark, Rickie
Sent: Friday, March 23, 2012 8:36 AM
To: 'Susan'
Cc: mayorpete@southbendin.gov; bsutton528@gmail.com
Subject: RE: # 1173149

Good Morning Ms. Searight,

Thank you for the submitting comments regarding the SR 933 bridge repair and rehabilitation project. INDOT and the City of South Bend will coordinate activities in order to minimize construction impacts as much as possible. While INDOT did present a proposed/tentative schedule, the actual construction implementation will take place when INDOT awards the construction contract. During that process, INDOT with coordination and discussion with the city and also St. Joseph County will develop a plan for the contractor so that construction related impacts and minimized as much as possible for local residents and the community.....and safety is the single most important element when developing and implementing a transportation improvement project. INDOT, the city, county, and others will perform the necessary due diligence where community safety is concerned. The comments we received on Wednesday from community residents will certainly be discussed internally at INDOT and also shared with city and county officials.

I have made a copy of the e-mail below and it will become part of the public hearings transcript which is what INDOT will review, evaluation and fully consider as decisions are part. Also, as a reminder, INDOT will prepare responses to public comments received during the comment period and make those responses available for public review once the environmental document is approved. INDOT will use the sign-in sheets from last evening and addresses we collect from letters/e-mails submitted to us over the next couple of weeks, to contact everyone and provide instruction as to where the final environmental document may be viewed. We will make the document (final environmental) available on our website and also send several copies other to the library where the hearing was held.

We anticipate having an approved document ready for public consumption by mid- April.

Thank you again for contacting INDOT and submitting comments regarding the SR 933 Bridge project.

Sincerely,

Rickie Clark, Indiana Department of Transportation
(317) 232-6601

From: Susan [<mailto:sears0127@gmail.com>]
Sent: Thursday, March 22, 2012 9:00 PM
To: Clark, Rickie
Cc: mayorpete@southbendin.gov; bsutton528@gmail.com
Subject: # 1173149

I know my neighbors have already shared some concerns about the repair planned for this summer on the nearby Leeper Bridge in South Bend. My biggest concerns about the bridge & project are these: one, I am very concerned about the re-routing of traffic through my neighborhood, the Northshore Triangle, which is full of children, playing in and around the streets. And second, what I see happening already, is the short cut some semi truck drivers are taking through a residential neighborhood that leads to ours, off of Lincolnway West. I have seen truckers turn onto Diamond from LWW, then through onto Angela, and up to 933/N. Michigan

Street. There are children throughout the area on Diamond Avenue besides all the children in my neighborhood, and I fear a child will be hurt or killed if this is not curtailed.

Thank you for considering my comments as you prepare to repair this historic bridge. Susan Searight, 329 Tonti Street, South Bend, IN. 46617

Clark, Rickie

From: Clark, Rickie
Sent: Friday, March 23, 2012 8:42 AM
To: 'Heather Moriconi'
Subject: RE: 1173149

Good Morning Ms. Moriconi,

Thank you for the submitting comments regarding the SR 933 bridge repair and rehabilitation project. INDOT and the City of South Bend will coordinate activities in order to minimize construction impacts as much as possible. While INDOT did present a proposed/tentative schedule, the actual construction implementation will take place when INDOT awards the construction contract. During that process, INDOT with coordination and discussion with the city and also St. Joseph County will develop a plan for the contractor so that construction related impacts and minimized as much as possible for local residents and the community.....and safety is the single most important element when developing and implementing a transportation improvement project. INDOT, the city, county, and others will perform the necessary due diligence where community safety is concerned. The comments we received on Wednesday from community residents will certainly be discussed internally at INDOT and also shared with city and county officials.

We have received similar comments in regards to the traffic re-route issue (specifically truck traffic) and will examine the issue and work with the city and county and also local law enforcement and the media to develop an enhanced plan to address this issue.

I have made a copy of the e-mail below and it will become part of the public hearings transcript which is what INDOT will review, evaluation and fully consider as decisions are part. Also, as a reminder, INDOT will prepare responses to public comments received during the comment period and make those responses available for public review once the environmental document is approved. INDOT will use the sign-in sheets from last evening and addresses we collect from letters/e-mails submitted to us over the next couple of weeks, to contact everyone and provide instruction as to where the final environmental document may be viewed. We will make the document (final environmental) available on our website and also send several copies other to the library where the hearing was held.

We anticipate having an approved document ready for public consumption by mid- April.

Thank you again for contacting INDOT and submitting comments regarding the SR 933 Bridge project.

Sincerely,

Rickie Clark, Indiana Department of Transportation
(317) 232-6601

-----Original Message-----

From: Heather Moriconi [<mailto:heather.moriconi@att.net>]
Sent: Friday, March 23, 2012 6:43 AM
To: Clark, Rickie
Subject: 1173149

To whom it may concern,

My family and I are residents of the North Shore Triangle. We have a five year old son. Please consider, when re-routing the traffic from the bridge repairs, directing traffic to Angela (not one of the side streets like Marquette or Wakewa. There are many, many families that live along these narrow streets. It would be very dangerous for children (and adults)

if heavy traffic started using these streets to cross over to Portage. In fact, is there any way to prevent traffic (with signs, news coverage, etc.) from using the side streets?

Thank you for your help.

A concerned parent,
Heather Moriconi

Clark, Rickie

From: Clark, Rickie
Sent: Friday, March 23, 2012 8:45 AM
To: 'John Wilford'
Subject: RE: 933 Bridge Rehabilitation

Good Morning Mr. Wilford,

Thank you for the submitting comments regarding the SR 933 bridge repair and rehabilitation project. INDOT and the City of South Bend will coordinate activities in order to minimize construction impacts as much as possible. While INDOT did present a proposed/tentative schedule, the actual construction implementation will take place when INDOT awards the construction contract. During that process, INDOT with coordination and discussion with the city and also St. Joseph County will develop a plan for the contractor so that construction related impacts and minimized as much as possible for local residents and the community.....and safety is the single most important element when developing and implementing a transportation improvement project. INDOT, the city, county, and others will perform the necessary due diligence where community safety is concerned. The comments we received on Wednesday from community residents will certainly be discussed internally at INDOT and also shared with city and county officials.

We have received similar comments in regards to the traffic re-route issue (specifically truck traffic) and will examine the issue and work with the city and county and also local law enforcement and the media to develop an enhanced plan to address this issue.....in which the signage or advanced signage is certainly part of the plan.

I have made a copy of the e-mail below and it will become part of the public hearings transcript which is what INDOT will review, evaluation and fully consider as decisions are part. Also, as a reminder, INDOT will prepare responses to public comments received during the comment period and make those responses available for public review once the environmental document is approved. INDOT will use the sign-in sheets from last evening and addresses we collect from letters/e-mails submitted to us over the next couple of weeks, to contact everyone and provide instruction as to where the final environmental document may be viewed. We will make the document (final environmental) available on our website and also send several copies other to the library where the hearing was held.

We anticipate having an approved document ready for public consumption by mid- April.

Thank you again for contacting INDOT and submitting comments regarding the SR 933 Bridge project.

Sincerely,

Rickie Clark, Indiana Department of Transportation
(317) 232-6601

From: John Wilford [<mailto:jwilfs@msn.com>]
Sent: Friday, March 23, 2012 7:55 AM
To: Clark, Rickie
Subject: 933 Bridge Rehabilitation

Dear Rickie Clark,

Comments on the S.R. 933 Bridge Rehabilitation Project

I am in favor of the project, but I have a few concerns.

1. What will the impact be on the detour routes? I live in the North Shore Triangle area and how will the alternate routes affect streets like Angela and Riverside?
2. On the East side of the bridge, how will streets like Niles and Eddy be affected?
3. I would like to see trucks permanently detoured around the bridge as bringing the trucks back to use the bridge would weaken the structure again.
4. Proper detour signs are a must and should be well in advance of the construction area.

Sincerely,

John Wilford
520 Parkovash Ave.
South Bend IN 46617-1032

Mast, Travis

From: Clark, Rickie
Sent: Tuesday, April 03, 2012 2:38 PM
To: Eichenauer, Mike; Mast, Travis; Branigin, Susan
Cc: Pinkerton, Jim; Deitchley, Matt
Subject: FW: Michigan Street Bridge

Follow Up Flag: Follow up
Flag Status: Flagged

Categories: Follow Up

An additional public comment (below) regarding the SR 933 bridge project. Please include in the transcript I e-mailed yesterday.

-----Original Message-----

From: Clark, Rickie
Sent: Tuesday, April 03, 2012 3:29 PM
To: Clark, Rickie
Subject: Fw: Michigan Street Bridge

----- Original Message -----

From: Martha and Gerald Kline [mailto:mandgkline@gmail.com]
Sent: Monday, April 02, 2012 09:08 PM
To: Clark, Rickie
Cc: montgomery.j.anne@gmail.com <montgomery.j.anne@gmail.com>
Subject: Michigan Street Bridge

INDOT: We appreciated the opportunity to meet those of you directly involved in the Michigan Street Bridge Rehabilitation and Repair Project. We also hope that our questions and concerns registered positively and constructively.

Our own experiences over the period of forty-one years that we have lived near the bridge have been frustrating, particularly regarding heavy truck traffic. We were

assured that the U.S. 31 bypass would accommodate thru trucks and nearly eliminate that traffic on Michigan Street. A number of us have followed such trucks on our own errands thru the city. They have not made local deliveries but access either the U.S. 20 bypass or continue south or north on 31/933.

Your proposal contends that the project will not adversely affect the specified historic sites, omitting the fact that adverse effects have been occurring for years and will recur without permanent weight restrictions. We can attest to the damage caused by heavy trucks since we feel the vibrations in our house and have cracked walls as tangible proof. Our near neighbors have had similar experiences. We live slightly more than 100 ft. from the north end of the bridge, so the structural damage that you have found is no surprise. Although the bridge was repaired in 2006, serious deterioration has occurred in the interim. If heavy truck traffic returns to the bridge, more damage is inevitable. You used the analogy of an aging patient needing restoration work. Please extend the analogy:

a patient nearing one hundred, having undergone restorative measures, is not going to perform the tasks of a thirty-year-old. Your proposal to repair the bridge with epoxy, fiberglass, and concrete may achieve a temporary fix but does not address the crucial issue of elder abuse.

If our understanding of IC9-20-1-3 subsection d is correct regarding size and weight regulation, INDOT is authorized to set weight limits on state highways with appropriate signage. The Michigan Street Bridge deserves lowered enforced weight limits. Such action is essential if the integrity of this structure is respected. As a historic landmark and as part of the National Register Leeper Park Complex, the bridge must be protected. We urge you to include reduced weight limits as an integral, essential part of your plan. The present detour of trucks should acclimate post-restoration traffic to permanent weight restrictions. To allow dangerously overweight traffic on this bridge is to court disaster. Only a clearly stated and enforced weight limit will indicate INDOT's commitment to preservation of the Michigan Street Bridge. Anything less is willfully abusive.

In addition, the approaches to the bridge must be maintained to eliminate pavement surface variations and manhole covers that are not flush with the surface. Any irregularity magnifies vibrations and stress.

We trust that your plans will include monitoring the condition of the restored bridge on a regular basis.

Sincerely,

Martha and Gerald Kline

SR933
COMMENT

crumlish and crumlish
architects, inc.

3215 Sugar Maple Court, Suite B
South Bend, Indiana 46628

Indiana Dept. of Transportation
Office of Public Involvement
100 N. Senate Ave. Room N642
Indianapolis, IN 46204-2216

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ZIP



(420) 46204

crumlish and crumlish
architects, incorporated



March 31, 2012

Indiana Department of Transportation
Office of Public Involvement
100 N. Senate Ave., Room N642
Indianapolis, IN 46204-2216

RE: SR 933 Bridge Rehabilitation
South Bend, IN

To Whom it May Concern:

This letter is to convey my professional concern regarding certain aspects of the pending bridge repairs for the Michigan Street Bridge in South Bend, IN over the St. Joseph River. I am completely satisfied with the project as presented recently by INDOT in South Bend. However I wish to address concerns regarding the ultimate design portion of the work.

I have been a lifelong resident of South Bend and live about 5 blocks away from the bridge. I travel across the bridge daily on foot and by vehicle. I have seen this bridge, up close, for a long time.

It appears the current guardrail/handrails (which are to be replaced in this upcoming project) are not only out-of-plumb vertically, but now have settled lower than their original height. The top of the center-span handrails on both sides of the bridge literally sag near mid-span. It appears these railings probably settled in conjunction with the below-grade pilings. They may or may not have been in this position before the 1977 reconstruction project. I am also aware these railings were not properly addressed in the 2006 deck replacement project that included other cosmetic repairs to the exterior of the bridge.

Therefore, I would like to know now that this pending handrail project will restore the correct and original height of these concrete railings as they are replaced. In my professional opinion, it is simply not enough to reconstruct them along their present 'fallen' alignment. They need to be re-cast, perhaps on a taller curb or base, sequentially, along with the remaining limestone piers to restore the proper appearance of the bridge from the river sides.

Attached are two photographs I took of the bridge last week. Both show the east façade of the bridge. Photo A was taken from the island park approximately 750 feet east of the bridge. Photo B was taken from the same park at the southeast side of the approach. For simplicity, these photos illustrate the condition of the handrails on the east side only, however the west side handrails are in virtually the same condition.

I have superimposed a series of bright colored lines over the photographs to suggest what I believe to be the original alignment of the railings and limestone piers. I made these illustrations from purely

3215 Sugar Maple Court, Suite B
South Bend, Indiana 46628
Phone: 574-282-2998 Fax: 574-282-2994

H-31

crumlish and crumlish
architects, incorporated



visual alignment and without any field measurements. However, I think I could approximate the distance between the current top position of the railing and its correct position at about 12 inches (at middle of the center span). And, from additional visual observations, I do not believe the railings above the north and south arch spans have settled nearly as much.

As a federally qualified historical architect, recognized by Indiana Dept. of Historic Preservation, I feel I am qualified to make this observation and to bring this concern to the attention of INDOT. And, as a licensed, practicing architect, I can also suggest that NOW is the perfect time to repair and restore the railings to their correct position.

I believe reasonable, economical and accurate field measurements can be made, along with simple construction documentation for the bridge contractors to remedy this fault. The new cast concrete railings can be designed and constructed with additional height to meet a pre-determined curved datum line such as I have illustrated. The limestone piers can each be reset, individually, at varying heights to meet this curved datum line. I do not believe, nor would I intend for this work to entail any modifications to interior sidewalks, curbs or transportation decking. All of this work can be accomplished, affordably, on the exterior facades as part of the proposed repairs.

Please advise me of the plans to make this restoration to the railings. Further, I would be more than happy to volunteer my professional time to assist INDOT and the their designated design/engineering firm with further consultation on this critical matter. I genuinely believe, with these simple corrective actions, the bridge can return to its true and majestic appearance as originally designed by Charles Cole in 1914.

Sincerely,

Brendan Crumlish, NCARB, CCS
President, and
IN Licensed Architect 870155
Federally Qualified Historical Architect

Copy: Catherine Hostetler, Historic Preservation Commission of South Bend & St. Joseph Cty.
Jessica Clark, PE, County Engineer, St. Joseph County, IN
Honorable Mayor, Peter Buttigieg, City of South Bend

Encl: Photos A & B, SR 933, Michigan Street Bridge, South Bend, IN

3215 Sugar Maple Court, Suite B
South Bend, Indiana 46628
Phone: 574-282-2998 Fax: 574-282-2994

H-32



PHOTO B Michigan Street Bridge South Bend, IN East Facade
crumlish & crumlish architects, inc.

March 28, 2012

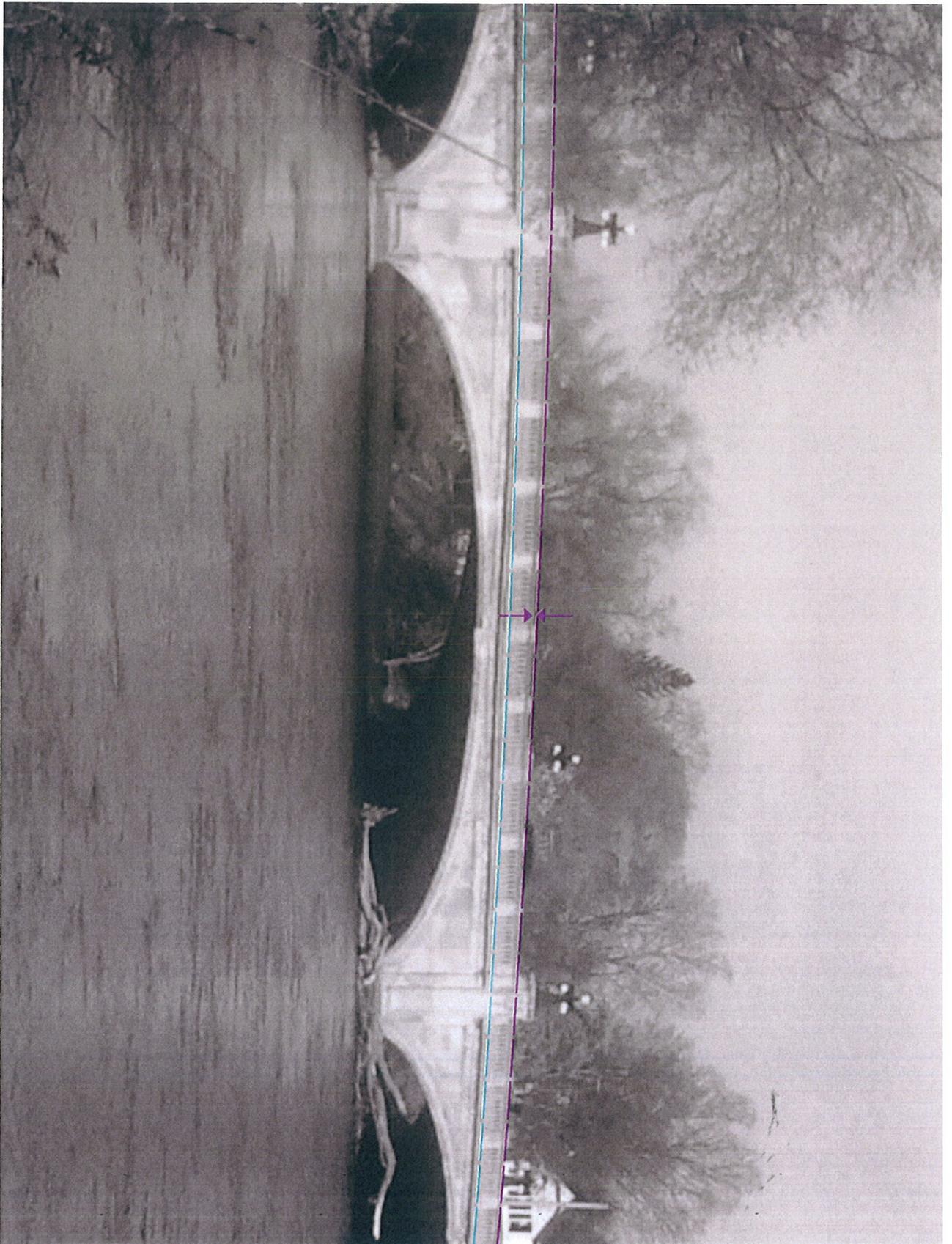


PHOTO A Michigan Street Bridge South Bend, IN East Facade
crumlish & crumlish architects, inc. March 28, 2012

4-3-12

Greetings Rickie:

The tree in the enclosed photo is lodged against the abutment below the Michigan Street Bridge on South Bend. Please forward picture to the appropriate personnel. Thank you!

Sincerely,
Marta + Jerry, Kline

Photo Taken By
Walter Klene
So. Bend, In

Tree lodged
Against abutment
below Michigan Street
Bridge in South
Bend

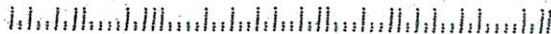
Klene
113 W. North Shore
Dr.
So. Bend, In 46617

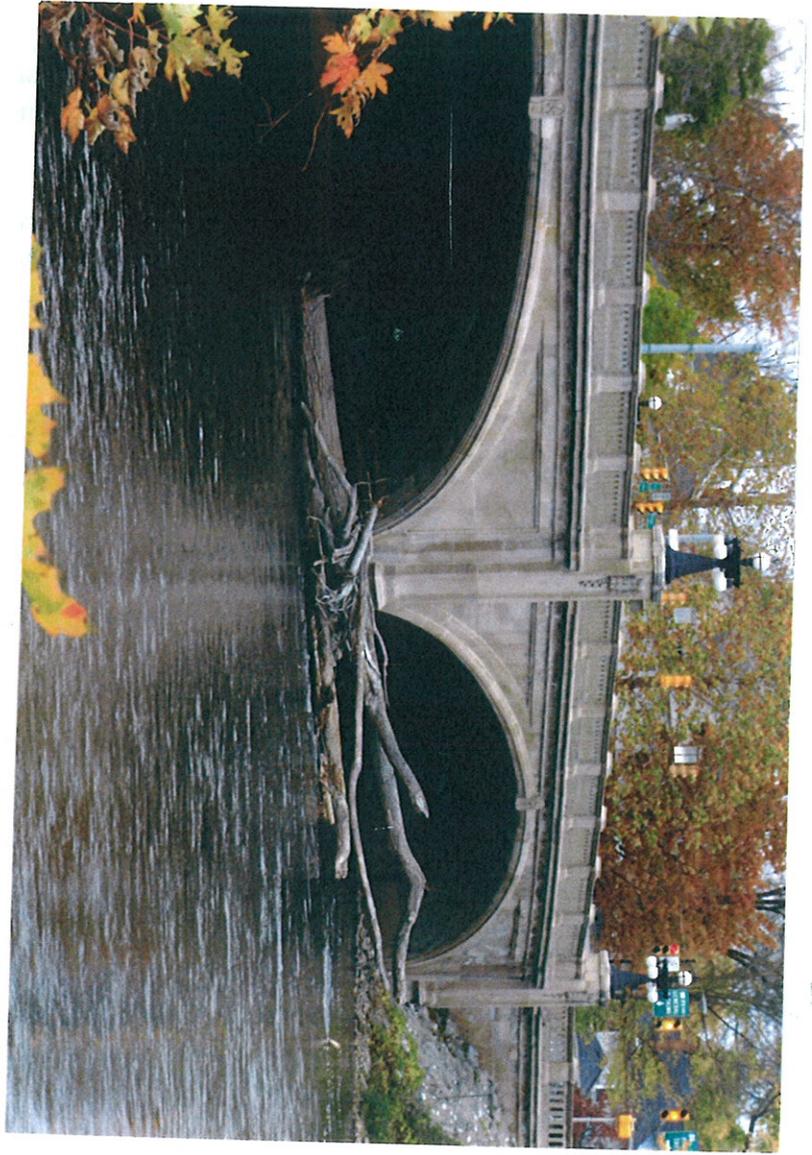
SOUTH BEND IN 466

03 APR 2012 PM 2 L



Rickie Clark
Inst Office of Public
Involvement
100 North State Avenue
Room N 642
Indianapolis, In 46204





Mast, Travis

From: Clark, Rickie
Sent: Monday, April 09, 2012 8:12 AM
To: Eichenauer, Mike; Mast, Travis; Branigin, Susan
Cc: Pinkerton, Jim
Subject: FW: south bend bridge repair

Please add this comment to the SR 933 Bridge hearings transcript.

Thanks,

Rickie

From: aaron frazee [mailto:mudlight@mac.com]
Sent: Monday, April 09, 2012 9:09 AM
To: Clark, Rickie
Subject: south bend bridge repair

Dear INDOT,

I am aware that the Leeper Park bridge is due for some repair work this summer. This is the same bridge that was repaired just a few years ago; it was resurfaced on top, patched below, and new footings were poured. As it supports 933/Michigan Avenue traffic, it is a major north/south artery. This summer, another big construction project will also begin: the reconstruction of SR 23 (Eddie Street), another important north-south route.

Structurally, I would like to know what will be done differently this time to the bridge to ensure long-term use? How will it be better? Were the previous repairs executed poorly, or was the bridge simply not repaired thoroughly enough? Even my small children noticed that after the repairs were completed, the bridge still sagged in the middle. I realize that nothing lasts forever; is it simply time to replace the bridge altogether?

My other questions are logistical. When will the SR 23 and the bridge work begin? What detour route/s are planned? I live in a quiet, residential neighborhood near the bridge. I am concerned about extra traffic here. School will be out, and a lot of my neighbors have young children. Angry, confused or just plain rushed commuters would pose a danger to this otherwise pedestrian-friendly place. In addition, the annual Sunburst race goes through our neighborhood. Each year, through traffic is shut down during that event. Could the repair work begin after the Sunburst?

Sincerely,

Aaron Frazee
South Bend Resident
Vice President, Northshore Triangle Neighborhood Association



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 232-6601
FAX: (317) 232-5349

Mitchell E. Daniels, Jr., Governor
Michael B. Cline, Commissioner

Thank you for attending this Public Hearing regarding the proposed S.R. 933 Bridge Rehabilitation & Repair Project. Please submit any comments regarding this project by using the space provided below. INDOT appreciates your attendance this evening and looks forward to working with the community during the development of this project.

INDOT Office of Public Involvement rclark@indot.in.gov

DATE: Wednesday, March 21, 2012

DES# 1173149

COMMENT:

WHEN REFERRING TO STREET NAMES:
PLEASE CORRECT
NORTHSHORE BOULEVARD
TO
NORTH SHORE DRIVE

SIGNATURE: _____



INDIANA DEPARTMENT OF TRANSPORTATION

Driving Indiana's Economic Growth

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INDOT Office of Public Involvement rclark@indot.in.gov

DATE: Wednesday, March 21, 2012

DES# 1173149

COMMENT:

Trucks often speed at 45 mph going south - There are not
likely local deliveries

Address bank erosion of previous repairs

Old Railroad bridge ^{over 933} could carry a low barrier to reduce
speed or heavy trucks

Large tree at East abutment?

Glass globes fill with water during rain - repair these.

SIGNATURE: *Anne Mowdy*

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Room N642
Indianapolis, Indiana 46204

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INDOT Office of Public Involvement rclark@indot.in.gov

DATE: Wednesday, March 21, 2012

DES# 1173149

COMMENT:

On behalf of the St. Joseph County Board of Commissioners, I offer these comments:

St. Joseph County applauds INDOT for choosing to rehabilitate the structure & preserve the significant historical elements of the bridge & its appearance, as well as phasing or sequencing construction to maintain traffic on the structure during construction. Maintaining traffic through and around downtown South Bend is of utmost priority to St. Joseph County.

The Leeper Bridge is one of the most artistic structures in the state and certainly in St. Joseph County.

We are proud to have some of the most striking ^{wood} bridges in the state, as well as some of the most historically significant both locally and nationally with respect to the design features.

The Leeper Bridge falls in this category and its aesthetic qualities are considerable.

Jessica D. Clark P.E.
St. Joseph County Engineer

SIGNATURE:

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100 North Senate Avenue
Room N642
Indianapolis, Indiana 46204

PHONE: (317) 232-6601
FAX: (317) 232-5349

Mitchell E. Daniels, Jr., Governor
Michael B. Cline, Commissioner

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INDOT Office of Public Involvement rclark@indot.in.gov

DATE: Wednesday, March 21, 2012

DES# 1173149

COMMENT:

It is true, this bridge is essential to the city of Sath Bend. However, it always seems to only receive bandages for repairs. It originally was not built knowing it would be handling the present amount of traffic abuse. Damage to the bridge will continue and necessary maintenance will be required (at a cost to the tax payer) unless changes in traffic flow are made.

1. Elimination of thru semi-truck traffic through the city. (The city bypass was supposedly built to accommodate these thru trucks.)
2. Decrease the weight limit on the bridge.
3. Installation of directional signage would be necessary and placed appropriately. (Trucks coming off the toll road should be directed north to Cleveland Road and onto the bypass and trucks coming from the south must also use the bypass.)

The majority of trucks presently crossing the bridge are thru trucks, not trucks making deliveries to the city. A couple of months with large fines should quickly solve the thru truck problem. Perhaps with the help of modern technology. Cameras could be installed to help catch the violators.

Enforcing the speed limit would further help preserve the bridge by eliminating the vibration that begins when fast moving trucks cross the bridge approach.

Please genuinely consider making the above changes to guarantee this historical bridge a longer and healthier life span and to also save the tax payers money.

SIGNATURE: Kathryn Kerr

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Indianapolis, Indiana 46204

PHONE: (317) 234-0796
FAX: (317) 233-4929

Mitchell E. Daniels, Jr., Governor
Michael B. Cline, Commissioner

DES. #: 1173149

LEGAL NOTICE OF PUBLIC HEARING

The Indiana Department of Transportation will hold a public hearing on **Wednesday, March 21, 2012, at 5:30pm at the St. Joseph County Library, Dickinson Conference Room, 304 South Main Street, South Bend, Indiana.**

The purpose of the public hearing is to offer all interested persons an opportunity to comment on current design plans for the proposed State Road (SR) 933 bridge rehabilitation and repair over the St. Joseph River, approximately 1.59 miles north of SR 23, located in South Bend, St. Joseph County. The total project length is about 0.001 mile.

The preferred alternative for this project will include the removal and in-kind replacement of the existing bridge railing panels in both form and material between Abutment 1 and Abutment 4, the performance of minor repairs to the railing posts with materials to match original; and major repairs to the interior arch ring and restoration of any exterior surfaces. All work will be restricted to the existing bridge structure; no new right-of-way will be acquired. The current bridge is in need of repair based on several deficiencies throughout the structure. The purpose of the project is to correct the deficiencies and ensure a safe transportation route at this location.

The existing structure, Bridge No. (933) 31-71-03690-B/NBI No. 11046, is a 3-span (84 feet X 120 feet X 84 feet) reinforced concrete arch. It was constructed by the City of South Bend in 1914-1915 as part of Leeper Park, and is locally known as the Leeper Bridge. The SR 933/Michigan Street/Leeper Bridge was repaired in 1977 and 2006. The SR 933/Michigan Street/Leeper Bridge over the St. Joseph River (Bridge No. (933)31-71-3690-B/NBI No. 11046) is an "Outstanding" resource in the Leeper Park Historic District (LPHD), which was listed in the National Register of Historic Places (NR) in May, 2000. The bridge is also individually NR-eligible, in part, for its engineering significance. The bridge was classified as "Select" by the 2010 Indiana Department of Transportation (INDOT) Historic Bridge Inventory. "Select" bridges are defined in the Historic Bridge PA as those structures that are excellent examples of a specific bridge type and that are most suitable for preservation. The SR 933/Michigan Street/Leeper Bridge also possesses Local Landmark status as conferred by the City of South Bend. Section 106 of the National Historic Preservation Act (NHPA) requires Federal agencies to take into account the effects of their undertakings on historic properties. Based on the preferred alternative repair of this "Select" bridge, the Federal Highway Administration has issued a "no adverse effect" finding for the project. In accordance with the NHPA, the views of the public are being sought, with the comment period ending, April 4, 2012, regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.6(a) (4), the documentation of "no adverse effect" specified in 36 CFR 800.11 (e) is available for viewing along with preliminary design plans for the project at the locations below:

H-43

1. Hearings Examiner, Room N642, Indiana Government Center North, 100 North Senate Avenue, Indianapolis, Indiana 46204-2216, Phone # (317) 232-6601
2. Indiana Department of Transportation La Porte District office, 315 E. Boyd Blvd, La Porte, Indiana 46350, Phone # 1-855-464-6368
3. St. Joseph County Public Library, Main Branch, 304 South Main Street, South Bend, Indiana, 46601, Phone # (574) 282-4646

Public statements for the record will be taken after the presentation. Individuals interested in participating in the public statement session may sign the speaker's schedule prior to the presentation.

All comments collected before, during and for a period of two (2) weeks after the hearing will be evaluated and addressed in the final design study report. The preliminary plans will be available for anyone interested in talking to the engineers about the project before and after the formal presentation. Conversations will not be part of the official record.

In accordance with the "Americans with Disabilities Act", if you have a disability for which the Indiana Department of Transportation needs to provide accommodations, please call the Public Hearings office at (317) 232-6601, by Wednesday, March 14, 2012.

This notice is published in compliance with Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1) states: "Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program." 23 CFR 450.212(a)(7) states: "Public involvement procedures shall provide for periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all and revision of the process as necessary." Approved by the Federal Highway Administration, U.S. Department of Transportation on April 1, 2009.

INDOT's Public Hearings Office is pleased to announce the availability of public notice via e-mail through our Public Notice List Serve. By subscribing to this service, you will be notified via e-mail each week, of any INDOT project related activities (i.e. public hearings/meetings, any planned highway improvements projects, notices of intent for highway construction and other notices sent through the agency's Public Hearings Office. In order to subscribe, simply visit the INDOT website at www.in.gov/dot and click on the "**Public Involvement / Media**" tab on the left side of the page. You will then click on "**INDOT List Serve Information**" and then scroll down to the "**Public Notices**" link then click to subscribe to this service. Simply enter your e-mail address as instructed then press "**submit**". You will receive a confirmation e-mail stating you have successfully subscribed to the Public Notice List Serve. You may also access the list serve subscription form by directly visiting this page: <http://lists.in.gov/mailman/listinfo/indot-legalnotices>.

INDIANA DEPARTMENT OF TRANSPORTATION

Rickie Clark

Public Hearings Manager

Phone # (317) 232-6601

E-Mail: rclark@indot.IN.gov

(Governmental Unit)
INDOT

.....County, Indiana

To..... Dr
South Bend Tribune
225 West Colfax Ave
South Bend, Indiana 46628

PUBLISHER'S CLAIM

LINE COUNT

Display Master (Must not exceed two actual lines, neither of which shall
total more than four solid lines of the type in which the body of the
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Head -- number of lines

Body -- number of lines

Tail -- number of lines

Total number of lines in notice 284

COMPUTATION OF CHARGES

284 lines, 3 columns wide equals 284 equivalent lines at .3493
cents per line \$148.22

Additional charges for notices containing rule or tabular work (50 per cent
of above amount)

Charge for extra proofs of publication (\$1.00 for each proof in excess
of two)

TOTAL AMOUNT OF CLAIM \$148.22

DATA FOR COMPUTING COST

Width of single column in picas 9.4ems Size of type 7.5 point.

Number of insertions 2

Pursuant to the provisions and penalties of IC 5-11-10-1, I hereby certify that the foregoing account is just and correct, that the amount claimed is legally due, after allowing all just credits, and that no part of the same has been paid.

I also certify that the printed matter attached hereto is a true copy, of the same column width and type size, which was duly published in said paper 2 time(s). The dates of publication being as follows: March 6 & 13, 2012



Date March 13, 2012

Title: President & Publisher

Leslie Ann Winey

Leslie Ann Winey, Notary Public
Resident of St, Joseph County
My Commission expires December 21, 2016

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The purpose of the public hearing is to offer all interested persons an opportunity to comment on current design plans for the proposed State Road (SR) 933 bridge rehabilitation and repair over the St. Joseph River, approximately 1.59 miles north of SR 23, located in South Bend, St. Joseph County. The total project length is about 0.001 mile.

The preferred alternative for this project will include the removal and in-kind replacement of the existing bridge railing panels in both form and material between Abutment 1 and Abutment 4, the performance of minor repairs to the railing posts with materials to match original; and major repairs to the interior arch ring and restoration of any exterior surfaces. All work will be restricted to the existing bridge structure; no new right-of-way will be acquired. The current bridge is in need of repair based on several deficiencies throughout the structure. The purpose of the project is to correct the deficiencies and ensure a safe transportation route at this location.

The existing structure, Bridge No. (933) 31-71-03690-B/NBI No. 11046, is a 3-span reinforced concrete arch. It was constructed by the City of South Bend in 1914-1915 as part of Leeper Park, and locally known as the Leeper Bridge. The SR 933/Michigan Street/Leeper Bridge was repaired in 1977 and 2006. The SR 933/Michigan Street/Leeper Bridge over the St. Joseph River (Bridge No. (933)31-71-3690-B/NBI No. 11046) is an "Outstanding" resource in the Leeper Park Historic District (LPHD), which was

listed in the National Register of Historic Places (NR) in May, 2000. The bridge is also individually NR-eligible, in part, for its engineering significance. The bridge was classified as "Select" by the 2010 Indiana Department of Transportation (INDOT) Historic Bridge Inventory. "Select" bridges are defined in the Historic Bridge PA as those structures that are excellent examples of a specific bridge type and that are most suitable for preservation. The SR 933/Michigan Street/Leeper Bridge also possesses Local Landmark status as conferred by the City of South Bend. Section 106 of the National Historic Preservation Act (NHPA) requires Federal agencies to take into account the effects of their undertakings on historic properties. Based on the preferred alternative repair of this "Select" bridge, the Federal Highway Administration has issued a "no adverse effect" finding for the project. In accordance with the NHPA, the views of the public are being sought, with the comment period ending, April 4, 2012, regarding the effect of the proposed project on the historic elements as per 36 CFR 800.2(d), 800.3(e) and 800.6(a)(4). Pursuant to 36 CFR 800.6(a) (4), the documentation of "no adverse effect" specified in 36 CFR 800.11 (e) is available for viewing along with preliminary design plans for the project at the locations below:

1. Hearings Examiner, Room N642, Indiana Government Center North, 100 North Senate Avenue, Indianapolis, Indiana 46204-2216, Phone # (317) 232-6601
2. Indiana Department of Transportation La Porte District office, 315 E. Boyd Blvd, La Porte, Indiana 46350, Phone # 1-855-464-6368
3. St. Joseph County Public Library, Main Branch, 304 South Main Street, South Bend, Indiana, 46601, Phone # (574) 282-4646

Public statements for the record will be taken after the presentation. Individuals interested in participating in the public statement session may sign the speaker's schedule prior to the presentation.

All comments collected before, during and for a period of two (2) weeks after the hearing will be evaluated and addressed in the final design study report. The preliminary plans will be available for anyone interested in talking to the engineers about the project before and after the formal presentation. Conversations will not be part of the official record.

In accordance with the "Americans with Disabilities Act", if you have a disability for which the Indiana Department of Transportation needs to provide accommodations, please call the Public Hearings office at (317) 232-6601, by Wednesday, March 14, 2012.

This notice is published in compliance with Code of Federal Regulations, Title 23, Section 771 (CFR 771.111(h)(1) states: "Each State must have procedures approved by the FHWA to carry out a public involvement/public hearing program." 23 CFR 450.212(a)(7) states: "Public involvement procedures shall provide for periodic review of the effectiveness of the public involvement process to ensure that the process provides full and open access to all and revision of the process as necessary." Approved by the Federal Highway Administration, U.S. Department of Transportation on April 1, 2009.

INDIANA DEPARTMENT OF TRANSPORTATION
Rickie Clark
Public Hearings Manager
Phone # (317) 232-6601
E-Mail: rclark@indot.IN.gov
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Repairs to South Bend bridge to start in May

Comments 0

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(South Bend Tribune file photo)

By JEFF HARRELL
South Bend Tribune Staff Writer
10:30 p.m. EDT, March 21, 2012
and one lane open to traffic.

Topics

- Heavy Engineering
- Manufacturing and Engineering

Access from the bridge to North Shore Drive will be shut off during

the construction months, said INDOT spokesman Jim Pinkerton.

Residents living near the bridge, however, fear that unless enforcement of INDOT's January ban of all truck traffic on the Michigan Street bridge picks up, the repairs would be a waste of time, money and effort, especially if the truck ban is lifted.

"Heavy truck traffic is not good for the house or for the bridge," said North Shore Drive resident Gerald Kiine.

When another resident asked about lowering the weight limits on the bridge to limit traffic to cars, SUVs and small trucks, Pinkerton likened INDOT to a doctor, and the historic bridge -- built in 1914 -- to "an old patient."

Like doctors who carefully monitor "an old patient," INDOT hopes to continually maintain the bridge's cracks and old-age erosion effects without prohibiting all truck traffic, Pinkerton said.

"I'm an old patient myself," replied the resident, who left before giving his name. "And my doctor tells me to lose pounds."

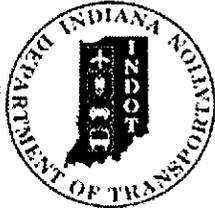
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- Neighbors worry road closure creates danger

Clark, Rickie

From: indot-legalnotices-bounces@lists.in.gov on behalf of INDOT Legal Notices [indot-legalnotices@lists.in.gov]
Sent: Friday, March 09, 2012 5:14 PM
To: indot-legalnotices-bounces@lists.in.gov; INDOT Legal Notices
Cc: Neese, Timothy; Alander, Jennifer; Jessica Clark; chair@indianacat.org; Mast, Travis; Mishler, Ryan; Broden, John; news@chestertontribune.com; comments@hecweb.org; Alderman, Robert; Pinkerton, Jim; Wesco, Tim; Zakas, Joseph; Dvorak, Ryan; Carl Littrell; Bauer, B. Patrick; dennis@indianahighspeedrail.org; Smith, Virginia; Wingfield, Will (INDOT); Niezgodski, David; Seanor, Sandra M; Fry, Craig; Weingardt, Abigail; ggilot@southbendin.gov; McGrannahan-Roberson, Linda; tmaloney@hecweb.org; Arnold, James
Subject: [Indot-legalnotices] INDOT Office of Public Involvement List Serve Announcement
Attachments: ATT00001.txt



INDIANA DEPARTMENT OF TRANSPORTATION
Driving Indiana's Economic Growth

The Indiana Department of Transportation (INDOT) will hold a public hearing on Wednesday, March 21, 2012 at 5:30pm in the St. Joseph County Library Dickinson Conference Room, 304 South Main Street in South Bend, Indiana.

The purpose of the hearing is to offer all interested persons an opportunity to comment on current design plans for the proposed State Road (SR) 933 bridge rehabilitation and repair project over the St. Joseph River, approximately 1.6 miles north of S.R. 23, located in South Bend, St. Joseph County.

The purpose of the project is to correct the deficiencies and ensure a safe transportation route at this location. The public hearing will feature a formal presentation to describe the proposal in detail, a public comment session and an informal public open house session where project officials will be available to address questions and concerns.

Should you have additional questions regarding this public hearing, please contact Rickie Clark, INDOT Office of Public Involvement at (317) 232-6601 or e-mail rclark@indot.in.gov.

Clark, Rickie

From: indot-legalnotices-bounces@lists.in.gov on behalf of INDOT Legal Notices [indot-legalnotices@lists.in.gov]
Sent: Friday, March 16, 2012 3:44 PM
To: INDOT Legal Notices; indot-legalnotices-bounces@lists.in.gov
Cc: Neese, Timothy; Alander, Jennifer; Jessica Clark; chair@indianacat.org; Mast, Travis; Mishler, Ryan; Broden, John; news@chestertontribune.com; comments@hecweb.org; ggilot@southbendin.gov; Pinkerton, Jim; Wesco, Tim; Zakas, Joseph; Dvorak, Ryan; Carl Littrell; Bauer, B. Patrick; dennis@indianahighspeedrail.org; Smith, Virginia; Wingfield, Will (INDOT); Niezgodski, David; Seanor, Sandra M; Fry, Craig; Weingardt, Abigail; Alderman, Robert; McGrannahan-Roberson, Linda; tmaloney@hecweb.org; Arnold, James
Subject: Re: [Indot-legalnotices] INDOT Office of Public Involvement List Serve Announcement
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(ON 0200094)

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DR JAMES COOPER
629 SEMINARY ST
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Des. #1173149
 SR 938/Michigan Street
 Bridge Project
 1.59 mile north of SR 23, over the St. Joseph River
 South Bend, Portage Township, St. Joseph County
 Consulting Parties: Those who responded to early coordination are shaded.

*Rebecca Smith
 and Sandi Markaba
 1201 Central Ave
 4/6/2022*

Title 1	First Name	Last Name	Title	Company Name	Address Line 1	Address Line 2	Address Line 3	City	State	ZIP Code
Mr.	Robert E.	Carter, Jr.	State Historic Preservation Officer	Indiana Department of Natural Resources	Division of Historic Preservation and Archaeology	Indiana Government Center South, Rm. W274		Indianapolis	IN	46204
Mr.	Todd	Zeiger	Director	Indiana Landmarks/Northern R.O.		402 W. Washington St.		South Bend	IN	46601
Mr.	Kurt	Garner	Co-Chair	Historic Michigan Road Byway Committee	12954 6th Rd.			Plymouth	IN	46563
Mr.	John	Kovach	St. Joseph County Historian		7982 Potato Hole Ct.			New Carlisle	IN	46552-9062
Ms.	Sandi	Seanor	Ex. Dir.	Michiana Area Council of Governments (MACOG)	227 W. Jefferson Blvd. Rm. 1120			South Bend	IN	46601
Mr.	Randy	Ray	Director	Center for History	808 W. Washington			South Bend	IN	46601-1439
Ms.	Peggy	Marker	Director	Hannah Lindahl Children's Museum	1402 S. Main St.			Mishawaka	IN	46544-5241
Mr.	Roger	Birdsell	Ex. Director	Michiana Jewish Historical Society	3202 Shalom Way			South Bend	IN	46615-2955
Mr.	Paul	Brandenburg	Chair	Indiana Historic Spans Task Force	5888 Croton Cir.			Indpls.	IN	46254
Dr.	James	L.	Cooper		629 Seminary St			Greencastle	IN	46135

Des. #1173149
 SR 933/Michigan Street
 Bridge Project
 1.59 mile north of SR 23, over the St. Joseph River
 South Bend, Portage Township, St. Joseph County
 Consulting Parties: Those who responded to early coordination are shaded

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Director				Mishawaka-Penn-Harris Public Library	209 Lincolnway E			Mishawaka	IN	46544-2084
Hon.	Stephen	Luecke	Mayor	City of South Bend	227 W. Jefferson Blvd., Ste. 1400 N			South Bend	IN	46601
Mr.	William	Minish	President	South Bend Area Genealogical Society	209 Lincolnway E			Mishawaka	IN	46544-2014
Ms.	Catherine	Hostetler	Director	Historic Preservation Commission of South Bend & St. Joseph Co.	126 S. Lafayette Blvd.			South Bend	IN	46601
	St. Joseph County Comm.				227 W. Jefferson Blvd.			South Bend	IN	46601
Ms.	Jan	Shupert-Arick	President	IN Lincoln Hwy. Assoc., Inc.	402 W. Washington			South Bend	IN	46601
Mr.	Aaron	Perrin	Ex. Dir.	Downtown South Bend, Inc.	205 N. Main St.			South Bend	IN	46601
Mr.	Marco	Mariani	Ex. Dir.	South Bend Heritage Fdn., Inc.	914 Lincolnway West.			South Bend	IN	46601