

Posting Date: March 23, 2022

# Request for Proposals Notification

**Title:** Johnson County Smith Valley Road - Phase 1 (Des # 2200170) in the Seymour District.

**Response Due Date & Time:** April 6, 2022 at 5:00pm

This Request for Proposals (RFP) is official notification of needed professional services. This RFP is being issued to solicit a letter of Interest (LOI) and other documents from firms qualified to perform engineering work on federal aid projects. A submittal does not guarantee the firm will be contracted to perform any services but only serves notice the firm desires to be considered.

**Contact for Questions:** Joe Bell, P.E. – HWC Engineering  
135 N. Pennsylvania Street, Suite 200  
Indianapolis, IN 46204  
(317) 419-6774  
jbell@hwcengineering.com

## Submittal Requirements:

1.  Letter of Interest – 0 Copies (required content and instructions follow) sent through the U.S. Mail;

OR

- Letter of Interest – submitted electronically (pdf) to **Daniel Johnston, Highway Engineer** at email address **djohnston@co.johnson.in.us**.

AND

2.  One (1) signed Affirmative Action Certification and associated required documents for all items if the DBE goal is greater than 0% sent through the U.S. Mail;

OR

- One (1) signed Affirmative Action Certification and associated required documents for all items if the DBE goal is greater than 0% sent electronically (pdf) to **Daniel Johnston, Highway Engineer** at email address **djohnston@co.johnson.in.us**.

**Submit To:** Daniel Johnston, P.E. – Highway Engineer  
1051 Hospital Road  
Franklin, IN 46131  
(317) 346-4630  
djohnston@co.johnson.in.us

**Selection Procedures:**

Consultants will be selected for work further described herein, based on the evaluation of the Letter of Interest (LOI) and other required documents. The Consultant Selection Rating Form used to evaluate and score the submittals is included for your reference. Final selection ranking will be determined by:

- The weighted score totals with the highest score being the top ranked firm
- Rank totals with the lowest rank total being the top ranked firm

**Requirements for Letters of Interest (LOI)**

## A. General instructions for preparing and submitting a Letter of Interest (LOI).

1. Provide the information, as stated in Item B below, in the same order listed and signed by an officer of the firm. Signed and scanned documents, or electronically applied signatures are acceptable. Do not send additional forms, resumes, brochures, or other material unless otherwise noted in the item description.
2. LOI's shall be limited to twelve (12) 8 ½" x 11" pages that include Identification, Qualifications, Key Staff, and Project Approach.
3. LOI's must be received no later than the "Response Due Date and Time"; as shown in the RFP header above. Responses received after this deadline will not be considered. Submittals must include all required attachments to be considered for selection.

## B. Letter of Interest Content

1. Identification, Qualifications and Key Staff

- a. Provide the firm name, address of the responsible office from which the work will be performed and the name and email address of the contact person authorized to negotiate for the associated work.
- b. List all proposed sub consultants, their DBE status, and the percentage of work to be performed by the prime consultant and each sub consultant. (See Affirmative Action Certification requirements below.) A listing of certified DBE's eligible to be considered for selection as prime consultants or sub-consultants for this RFP can be found at the "Prequalified Consultants" link on the Indiana Department of Transportation (INDOT) Consultants Webpage. (<http://www.in.gov/indot/2732.htm>).
- c. List the Project Manager and other key staff members, including key sub consultant staff, and the percent of time the project manager will be committed for the contract, if selected. Include project engineers for important disciplines and staff members responsible for the work. Address the experience of the key staff members on similar projects and the staff qualifications relative to the required item qualifications.

- d. Describe the capacity of consultant staff and their ability to perform the work in a timely manner relative to present workload.
2. Project Approach
    - a. Provide a description of your project approach relative to the advertised services. For project specific items confirm the firm has visited the project site. For all items address your firm's technical understanding of the project or services, cost containment practices, innovative ideas and any other relevant information concerning your firm's qualifications for the project.

### **Requirements for Affirmative Action Certification**

A completed Affirmative Action Certification form is required for all items that identify a DBE goal greater than "0", in order to be considered for selection. The consultant must identify the DBE firms with which it intends to subcontract.

On the Affirmative Action Certification, include the contract participation percentage of each DBE and list what the DBE will be subcontracted to perform.

If the consultant does not meet the DBE goal, the consultant must provide documentation in additional pages after the form that evidences that it made good faith efforts to achieve the DBE goal.

All DBE subcontracting goals apply to all prime submitting consultants regardless of the prime's status of DBE.

### **INDOT DBE Reciprocity Agreement with KYTC**

An Agreement between INDOT and the Kentucky Transportation Cabinet (KYTC) established reciprocal acceptance of certification of DBE firms in their respective states under the Unified Certification Program (UCP) pursuant to 49 CFR ?26.81(e) and (f).

Copies of the DBE certifications, as issued by INDOT or the Kentucky Transportation Cabinet (KYTC), are to be included as additional pages after the AAC form for each firm listed on the AAC form. The following are DBE Locator Directories for each State Transportation Agency:

**INDOT:** <https://entapps.indot.in.gov/DBELocator/>

**KYTC:** <https://transportation.ky.gov/Civil-Rights-and-Small-Business-Development/Pages/Certified-DBE-Directory.aspx>

Information about the Indiana DBE Program is available at: <https://www.in.gov/indot/2674.htm>.

Information about the KYTC DBE Program is available at: <https://transportation.ky.gov/Civil-Rights-and-Small-Business-Development/Pages/default.aspx>.

## Work item details:

Local Public Agency: Johnson County

Project Location: Smith Valley Road - Phase 1

Project Description: Road Reconstruction from Mullinix Road through and including Morgantown Road Intersection, and replacement of Bridge 73 over Honey Creek

INDOT Des #: 2200170

Phases Included: PE including R/W Engineering

Estimated Construction Amount: \$ 19,377,500

Funding: \$ 1,674,000 Federal

Term of Contract: Until Project Completion

DBE goal: 7%

### Required Prequalification Categories:

- |  |   |
|--|---|
| <input type="checkbox"/> 5.2 Environmental Document Preparation - CE         | <input checked="" type="checkbox"/> 12.1 Project Management for Aquisition Services |
| <input checked="" type="checkbox"/> 6.1 Topographical Survey Data Collection | <input checked="" type="checkbox"/> 12.2 Title Search                               |
| <input type="checkbox"/> 8.1 Non-Complex Roadway Design                      | <input checked="" type="checkbox"/> 12.4 Appraisal                                  |
| <input checked="" type="checkbox"/> 9.1 Level 1 Bridge Design                | <input checked="" type="checkbox"/> 12.5 Appraisal Review                           |
| <input checked="" type="checkbox"/> 11.1 Right of Way Plan Development       | <input type="checkbox"/> 13.1 Construction Inspection                               |
| <input checked="" type="checkbox"/> Additional Categories Listed Below:      |   |

- 2.2 Traffic Forecasting
- 3.1 Non-Complex Traffic Capacity and Operations Analysis
- 7.1 Geotechnical Engineering Services
- 8.2 Complex Roadway Design
- 8.3 Roundabout Design
- 10.1 Traffic Signal Design
- 10.4 Lighting Design
- 12.3 Value Analysis
- 12.6 Negotiation
- 12.7 Closing
- 12.8 Relocation
- 16.1 Utility Coordination
- 17.2 Small Structure and Pipe Design
- 17.3 Storm Sewer and Detention Design
- 17.4 Bridge Hydraulic Design
- 18.1 Pavement Analysis-Design Services

# LPA Consultant Selection Rating Sheet

Sample:

<b>RFP Selection Rating for</b> _____						<b>Des. No.</b> _____	
(City, County, Town) or (Local Public Agency)							
<b>Services Description:</b> _____							
<b>Consultant Name:</b> _____							
Evaluation Criteria to be Rated by Scorers							
Category	Scoring Criteria	Scale	Score	Weight	Weighted Score		
<b>Past Performance</b>	<b>Performance evaluation score averages from historical performance data.</b>						
	Quality score for similar work from performance database.				6		
	Schedule score from performance database.				3		
	Responsiveness score from performance database.				1		
<b>Capacity of Team to do Work</b>	<b>Evaluation of the team's personnel and equipment to perform the project on time.</b>						
	Availability of more than adequate capacity that results in added value.	1			20		
	Adequate capacity to meet the schedule.	0					
	Insufficient available capacity to meet the schedule.	-1					
<b>Team's Demonstrated Qualifications</b>	<b>Technical Expertise: Unique Resources that yield a relevant added value or efficiency to the deliverable.</b>						
	Demonstrated outstanding expertise and resources identified for required services for value added benefit.	2			15		
	Demonstrated high level of expertise and resources identified for required services for value added benefit.	1					
	Expertise and resources at appropriate level.	0					
	Insufficient expertise and/or resources.	-3					
<b>Project Manager</b>	<b>Predicted ability to manage the project, based on: experience in size, complexity, type, subs, documentation skills.</b>						
	Demonstrated outstanding experience in similar type and complexity.	2			20		
	Demonstrated high level of experience in similar type and complexity.	1					
	Experience in similar type and complexity shown in resume.	0					
	Experience in different type or lower complexity.	-1					
	Insufficient experience.	-3					
<b>Approach to Project</b>	<b>Project Understanding and Innovation that provides cost and/or time savings.</b>						
	High level of understanding and viable innovative ideas proposed.	2			15		
	High level of understanding of the project.	1					
	Basic understanding of the project.	0					
	Lack of project understanding.	-3					
<b>Weighted Sub-Total:</b>							
<p>It is the responsibility of scorers to make every effort to identify the firm most capable of producing the highest deliverables in a timely and cost effective manner without regard to personal preference.</p> <p>I certify that I do not have any conflicts of interest associated with this consultant as defined in 49CFR118.36.</p> <p>I have thoroughly reviewed the letter of interest for this consultant and certify that the above scores represent my best judgment of this firm's abilities.</p>							
Signature: _____				Print Name: _____			
Title: _____				Date: _____			
(Form Rev. 4-7-16)							

Des. #: [Click here to enter text.](#)

**Affirmative Action Certification (AAC) for Disadvantaged Business Enterprises (DBE)**

I hereby certify that my company intends to affirmatively seek out and consider Disadvantaged Business Enterprises (DBEs) certified by the State of Indiana’s DBE Program and the Kentucky Transportation Cabinet (KYTC) DBE Program to participate as part of this proposal. An Agreement between INDOT and KYTC established reciprocal acceptance of certification of DBE firms in their respective states under the Unified Certification Program (UCP) pursuant to 49 CFR §26.81(e) and (f).

I acknowledge that this certification is to be made an integral part of this proposal. I understand and agree that the submission of a blank certification may cause the proposal to be rejected. I certify that I have consulted the following DBE websites to confirm that the firms listed below are currently certified DBEs:

INDOT: <https://entapps.indot.in.gov/DBELocator/>

KYTC: <https://transportation.ky.gov/Civil-Rights-and-Small-Business-Development/Pages/Certified-DBE-Directory.aspx>

I certify that I have contacted the certified DBE’s listed below, and if my company becomes the CONSULTANT, these DBEs have tentatively agreed to perform the services as indicated. I understand that neither my company nor I will be penalized for DBE utilization that exceeds the goal. After contract award, any change to the firms listed in this Affirmative Action Certification to be applied toward the DBE goal must have prior approval by INDOT’s Economic Opportunity Division.

**I. DBE Subconsultants to be applied toward DBE goal for the RFP item:**

Certified DBE Name	Service Planned	Estimated Percentage to be Paid*
		%
		%
		%
		%

**II. DBE Subconsultants to be utilized beyond the advertised DBE goal for the RFP item:**

Certified DBE Name	Service Planned	Estimated Percentage to be Paid*
		%
		%
		%
		%

Estimated Total Percentage Credited toward DBE Goal: \_\_\_\_\_

Estimated Percentage of Voluntary DBE Work Anticipated over DBE Goal: \_\_\_\_\_

Company Name: \_\_\_\_\_

Signature: \_\_\_\_\_ Date: \_\_\_\_\_

\* It is understood that these individual firm percentages are estimates only and that percentages paid may be greater or less as a result of negotiation of contract scope of work. My firm will use good faith efforts to meet the overall DBE goal through the use of these or other certified and approved DBE firms.



# Johnson County Highway Department

1051 Hospital Road  
Franklin, IN 46131

(317) 346-4630 Office  
(317) 738-5378 Fax

www.jchighway.com  
highway@co.johnson.in.us

Lucas M. Mastin  
Highway Supervisor

Daniel E. Johnston, P.E.  
Highway Engineer

## Project Information for Request for Proposals

**Project:** Smith Valley Road Reconstruction Phase 1  
Roadway Reconstruction from Mullinix Road through and including Morgantown Road Intersection, and Replacement of Bridge 73 on Smith Valley Road over Honey Creek

**DES:** 2200170

**Funding:** Federal Funding and Local Funding for Construction, Utilities, Construction Inspection, and Right-of-Way Services  
INDOT Local Federal Program funding for PE including R/W Engineering

**Schedule:** Target Contract Letting Date: 12/11/2024  
Estimated Construction Cost: \$19,377,500  
RFP Advertisement Date: 3/23/2022  
\*LOI Submittal Deadline: 4/6/2022  
\*\*Maximum Fee (Federal Participation Cap): \$1,674,000

\*The selection and negotiation process will be completed on an accelerated schedule. Fee proposals are not required with LOI submissions. Selected design team will be expected to provide design fees for negotiation within 1 week of selection notification.

\*\*The selection process will be a Qualification Based Selection (QBS). Local funding for PE beyond the Federal Participation Cap is not anticipated.

## Prequalification Requirements:

2.2	Traffic Forecasting	12.2	Title Research
3.1	Non-Complex Traffic Capacity and Operations Analysis	12.3	Value Analysis
6.1	Topographic Survey Data Collection	12.4	Appraisal
7.1	Geotechnical Engineering Services	12.5	Appraisal Review
8.2	Complex Roadway Design	12.6	Negotiation
8.3	Roundabout Design	12.7	Closing
9.1	Level 1 Bridge Design	12.8	Relocation
10.1	Traffic Signal Design	16.1	Utility Coordination
10.4	Lighting Design	17.2	Small Structure and Pipe Design
11.1	Right of Way Plan Development	17.3	Storm Sewer and Detention Design
12.1	Project Management for Acquisition Services	17.4	Bridge Hydraulic Design
		18.1	Pavement Analysis-Design Services

### Project Summary:

This project will reconstruct Smith Valley Road from the Eastern limit of the INDOT I-69 / Smith Valley Road Interchange project to approximately 500 feet east of the Smith Valley Road and Morgantown Road intersection. Replacement of County Bridge 73 over Honey Creek is also included in this project. This project is one of two construction phases to reconstruct Smith Valley Road between I-69 and SR 135. Refer to the included attachments for graphics of project limits, and the most recent Bridge Inspection Report.

### Project Delivery and Coordination:

Smith Valley Road is projected to receive significant additional traffic in the future and will be widened as part of this roadway reconstruction project. The proposed roadway cross section includes upgrades to pedestrian connectivity, a 4-lane divided highway, and roundabouts at multiple locations. Design teams will be expected to deliver Final Design for contract Letting in accordance with the INDOT Letting Prep Schedule. Project delivery shall be completed in accordance with NEPA requirements, the Indiana Design Manual, INDOT standards and specifications, and will be subject to the INDOT LPA project delivery schedule, procedures, and oversight. Coordination with the Phase 2 design team is expected. Survey is complete and development of the environmental document is underway and will be completed by others. **The project funding for all project funding phases is locked in the specified programmed State Fiscal years, and cannot be altered.**

### Traffic Analysis and Modeling,

Submitting design teams shall demonstrate capacity and expertise for comprehensive Traffic Modeling, complex intersection design (roundabout), and complex roadway design efforts. Determination of capacity and intersection turning movements will be expected of selected design teams as a prerequisite to justification for design. Preliminary design provided is based on current available traffic models and counts. Provided preliminary design and concepts are for information only. Final Design team shall verify and adjust design as necessary.

### Bridge Design

Replacement of the County Bridge #73 over Honey Creek will include removal of existing structure and construction of a new bridge. Preliminary bridge hydraulic analysis and preliminary design are completed and will be provided to the selected team. The most recent Bridge Inspection from 2020 is included in supporting documentation.

### Right of Way Acquisition

Submitting teams shall demonstrate an understanding of the sensitive nature of the right-of-way acquisition required for this project. Design teams should present design strategies and concepts that



will minimize impacts to property owners that will be implemented in design. Property owner impacts are anticipated on this project. Submitting teams shall demonstrate capacity and expertise necessary to deliver a large-scale Public Involvement and R/W Services package on behalf of Johnson County.

\*Submitting teams shall demonstrate capacity to provide turn-key R/W Services per the listed INDOT prequalifications. Preliminary determination of R/W impacts is estimated as follows:

	<b>Mullinix to Morgantown Rd. (Includes Paddock Roundabout, and Honey Creek Bridge)</b>	<b>Morgantown Rd. Roundabout</b>
Estimated No. of Parcels Impacted	38	5
Estimated Relocations	2	0
Estimated Acres	6.0	1.0

Utility Coordination

Significant Utility impacts are anticipated. Submitting teams shall demonstrate capacity and expertise for a comprehensive Utility Coordination effort. Submitting Teams shall demonstrate qualifications including a Certified INDOT Utility Coordinator and support team. Early engagement of affected utilities is expected.

Environmental Documentation and Permitting

Environmental Document Development and services are already in process. The current environmental team will continue with delivery of the Approved Final Environmental Document, and will coordinate related project delivery with the selected design team. Coordination between selected design teams and current Environmental team will be required and should be accounted for in proposals. A CE-4 is anticipated, with multiple relocations, significant R/W acquisition, updates to intersections, access points, and impacts to cultural resources identified. Public Information Exhibits, Public Hearing meeting leadership, and updated design information supporting timely production satisfying the environmental requirements will be expected of the selected design teams.

Geotechnical and Pavement Design

Significant impacts to existing roadbed, new terrain, and surrounding topography are anticipated by this project. A bridge replacement, multiple small structure replacements, and a significantly increased pavement structure are anticipated. Submitting teams shall demonstrate capacity and expertise for delivery of comprehensive Geotechnical Investigation, Report, and Pavement Design efforts.

### Project Management and Design Coordination

Johnson County Board of Commissioners have committed significant investment in preliminary design, and a project management team to serve as oversight in coordination with County throughout design.

### Schedule and Project Delivery Requirements:

Target letting is 12/11/2024, and the related project development schedule will be in accordance with the INDOT Letting Prep Schedule.

INDOT Quarterly Reports will be required with full support from the selected Design Team upon request.

Federal Funding allocation is strictly limited to the current programmed fiscal years of each programmed funding phase. The project schedule and design development submittals will be critical to project success. **The project funding for all project funding phases is locked in the specified programmed State Fiscal years, and cannot be altered.**

### As Built Reference Files (submitting teams may request via INDOT independently)

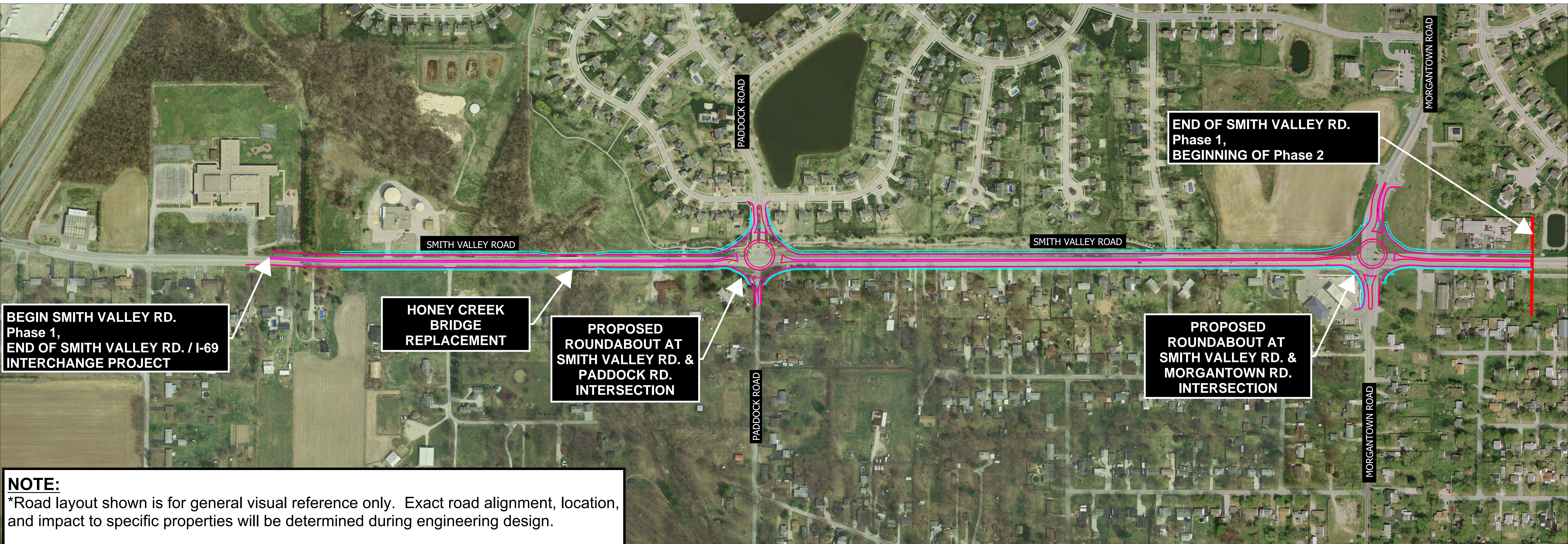
Previous project from SR-37 to Morgantown Rd. was R-28418, Des. 9003745

### Attachments:

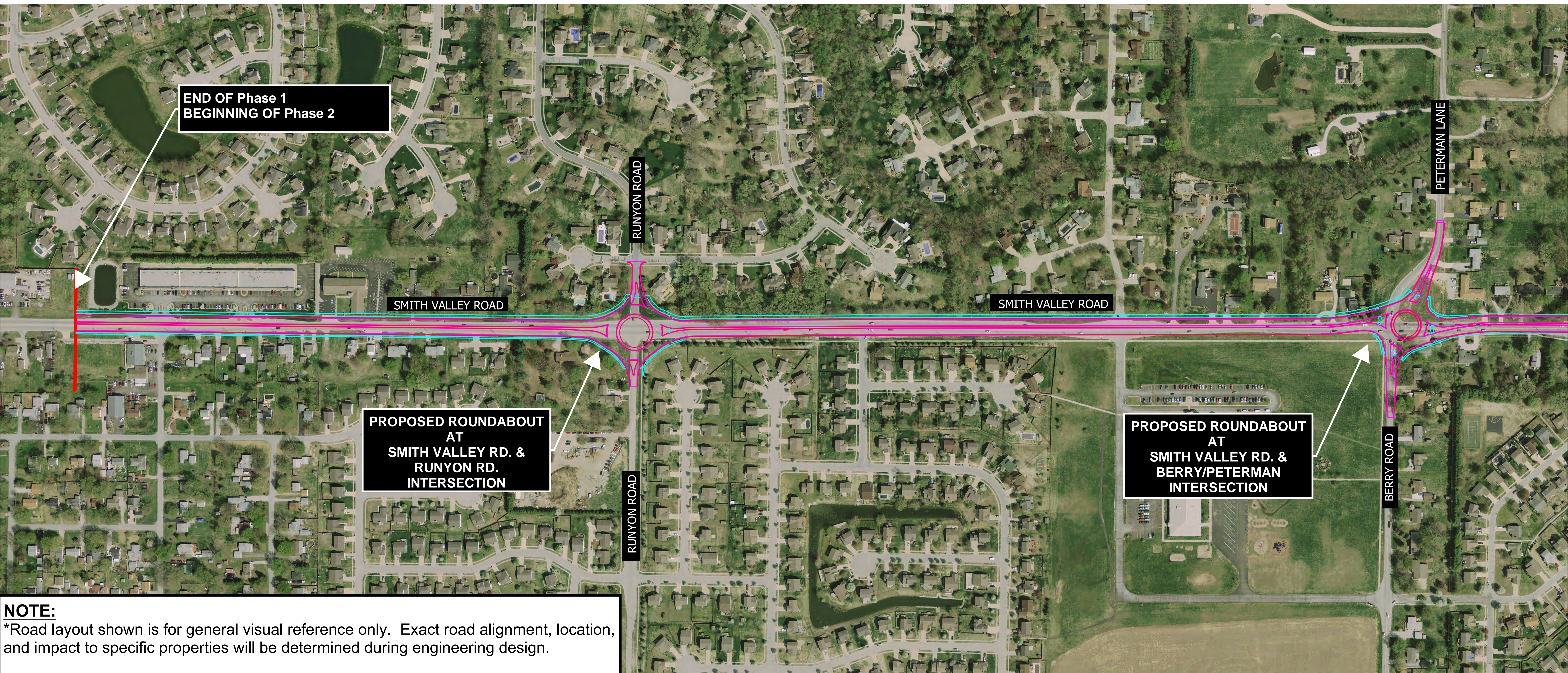
Conceptual Layout of project limits

Bridge #73 – Bridge Inspection Report

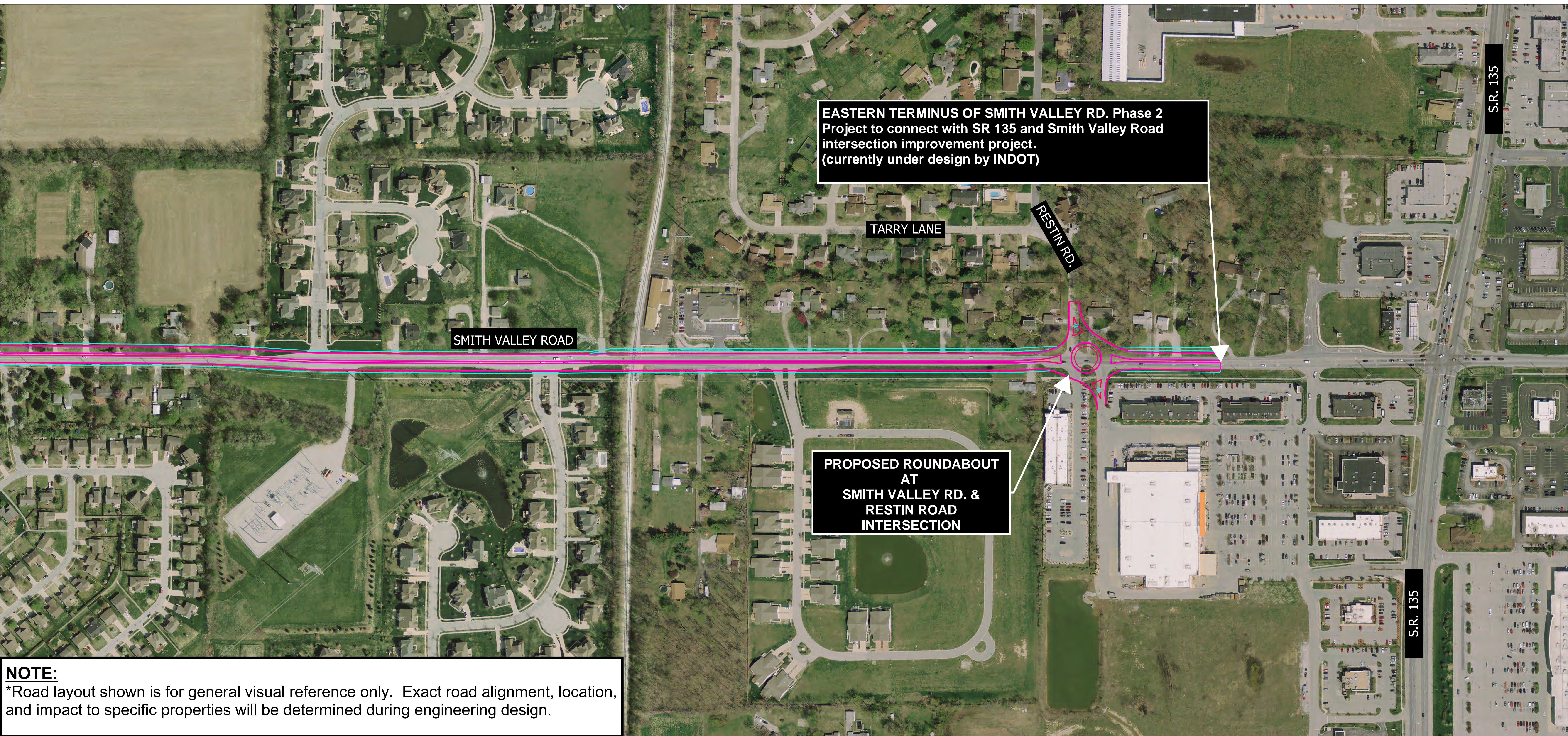
# Johnson County Smith Valley Rd. Reconstruction Contract 1



# Johnson County Smith Valley Rd. Reconstruction Contract 2



# Johnson County Smith Valley Rd. Reconstruction Contract 2





Alignment Looking East



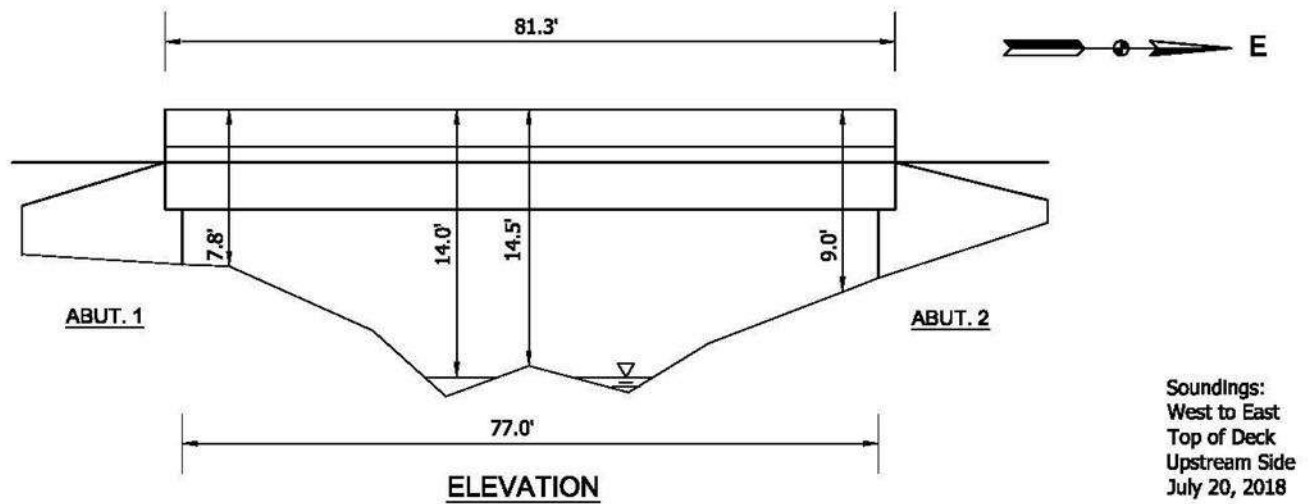
South Elevation



Exposed Steel Bars in Beam 2



Exposed Straps at Beam 3 End



**JOHNSON COUNTY - BRIDGE NO. 41-00073**  
**SMITH VALLEY ROAD over HONEY CREEK**

Inspector: Jacob Gould  
Inspection Date: 07/15/2020

Asset Name: 41-00073  
Facility Carried: SMITH VALLEY  
ROAD

Bridge Inspection Report

OVERALL THE BRIDGE IS IN SATISFACTORY CONDITION. HAIRLINE MAP CRACKS & LONGITUDINAL CRACKS IN DECK. CHANNEL DRIFT & DEBRIS AT BEARING AREAS INDICATES WATER REACHES BOTTOM OF BEAMS. SPALLS WITH EXPOSED STEEL STRAPS ON BOTTOM OF MOST BEAMS AT BEAM ENDS. THERE ARE TWO STEEL BARS EXPOSED IN BEAM 2, THEY DON'T APPEAR TO BE STRANDS. MINOR APPROACH RAIL DAMAGE SOUTHWEST.

RECOMMEND CLEARING OUT DEBRIS FROM BEARING AREAS. CUT BACK VEGETATION AROUND BRIDGE. CONSIDER PATCHING BOTTOM OF BEAMS.

Inspector: Jacob Gould  
 Inspection Date: 07/15/2020

Asset Name: 41-00073  
 Facility Carried: SMITH VALLEY ROAD

Bridge Inspection Report

**IDENTIFICATION**

(1) STATE CODE:	185 - Indiana	(12) BASE HIGHWAY NETWORK:	0
(8) STRUCTURE:	4100057	(13A) INVENTORY ROUTE:	
(5 A-B-C-D-E) INV. ROUTE:	1 - 4 - 1 - 00000 - 0	(13B) SUBROUTE NUMBER:	
(2) HIGHWAY AGENCY DISTRICT:	05 - Seymour	(16) LATITUDE:	39.60498
(3) COUNTY CODE:	041 - JOHNSON	(17) LONGITUDE:	-86.20985
(4) PLACE CODE:	00000 - N/A	(98) BORDER	
(6) FEATURES INTERSECTED:	HONEY CREEK	A) STATE NAME:	
(7) FACILITY CARRIED:	SMITH VALLEY ROAD	B) PERCENT	%
(9) LOCATION:	0.50 E OF SR 37	(99) BORDER BRIDGE STRUCT. NO:	
(11) MILEPOINT:	0000.000		

**STRUCTURE TYPE AND MATERIAL**

(43) STRUCTURE TYPE, MAIN:		(45) NUMBER OF SPANS IN MAIN 001 UNIT:	
A) KIND OF MATERIAL/DESIGN:	5 - Prestressed concrete	(46) NUMBER OF APPROACH SPANS:	0000
B) TYPE OF DESIGN/CONSTR:	02 - Stringer/Multi-beam or Girder	(107) DECK STRUCTURE TYPE:	1 - Concrete Cast-in-Place
(44) STRUCTURE TYPE, APPROACH SPANS:		(108) WEARING SURFACE/PROT SYS:	
A) KIND OF MATERIAL/DESIGN:	0 - Other	A) WEARING SURFACE:	1 - Monolithic Concrete (concurrently placed with structural deck)
B) TYPE OF DESIGN/CONSTR:	00 - Other	B) DECK MEMBRANE:	0 - None
		C) DECK PROTECTION:	1 - Epoxy Coated Reinforcing

**AGE OF SERVICE**

(27) YEAR BUILT:	1962	(28) LANES:	
(106) YEAR RECONSTRUCTED:	2006	A) ON BRIDGE:	02
(42) TYPE OF SERVICE:		B) UNDER BRIDGE:	00
A) ON BRIDGE:	1 - Highway	(29) AVERAGE DAILY TRAFFIC:	007638
B) UNDER BRIDGE:	5 - Water way	(30) YEAR OF AVERAGE DAILY TRAFFIC:	2019
		(109) AVERAGE DAILY TRUCK TRAFFIC:	08 %
		(19) BYPASS DETOUR LENGTH:	002 MI



Inspector: Jacob Gould  
 Inspection Date: 07/15/2020

Asset Name: 41-00073  
 Facility Carried: SMITH VALLEY ROAD

Bridge Inspection Report

**GEOMETRIC DATA**

(48) LENGTH OF MAX SPAN: 00077.0 FT	(35) STRUCTURE FLARED: 0 - No flare
(49) STRUCTURE LENGTH: 00081.3 FT	(10) INV RTE, MIN VERT CLEARANCE: 99.99 FT
(50) CURB/SIDEWALK WIDTHS:	(47) TOT HORIZ CLEARANCE: 038.6 FT
A) LEFT 00.0 FT	(53) VERT CLEAR OVER BR RDWY: 99.99 FT
B) RIGHT: 00.0 FT	(54) MIN VERTICAL UNDERCLEARANCE:
(51) BRDG RDWY WIDTH CURB-TO-CURB: 038.6 FT	A) REFERENCE FEATURE: N
(52) DECK WIDTH, OUT-TO-OUT: 041.6 FT	B) MIN VERT UNDERCLEAR: 0 FT
(32) APPROACH ROADWAY 036.0 FT	(55) LATERAL UNDERCLEARANCE RIGHT:
(33) BRIDGE MEDIAN: 0 - No median	A) REFERENCE FEATURE: N
(34) SKEW: 05 DEG	B) MIN LATERAL UNDERCLEAR: 000.0 FT
	(56) MIN LATERAL UNDERCLEAR ON LEFT: 000.0 FT

**INSPECTIONS**

(90) INSPECTION DATE: 07/15/2020	(91) DESIGNATED INSPECTION FREQUENCY: 24 MONTHS
(92) CRITICAL FEATURE INSPECTION:	(93) CRITICAL FEATURE INSPECTION DATE:
A) FRACTURE CRITICAL REQUIRED/FREQUENCY: N	A) FRACTURE CRITICAL DATE:
B) UNDERWATER INSPECTION REQUIRED/FREQUENCY: N	B) UNDERWATER INSP DATE:
C) OTHER SPECIAL INSPECTION REQUIRED/FREQUENCY: N	C) OTHER SPECIAL INSP DATE:

**CONDITION**

(58) DECK: 7 - Good Condition (some minor problems)	(60) SUBSTRUCTURE: 7 - Good Condition (some minor problems)
(58.01) WEARING SURFACE: 7 - Good Condition	(61) CHANNEL/CHANNEL PROTECTION: 6 - Bank slump. widespread minor damage
(59) SUPERSTRUCTURE: 6 - Satisfactory Condition (minor deterioration)	(62) CULVERTS: N - Not Applicable

**CONDITION COMMENTS**

(58) DECK: 7 - Good Condition (some minor problems)  
 Comments:  
 DECK ON STAY IN PLACE METAL FORMS PERFORMING WELL. HAIRLINE CRACKS IN COPINGS.  
 Material:  
 8" REINFORCED CONCRETE DECK, SIP FORMS

(58.01) WEARING SURFACE: 7 - Good Condition  
 Comments:  
 HAIRLINE CRACKS  
 Material:  
 CONCRETE

Inspector: Jacob Gould  
Inspection Date: 07/15/2020

Asset Name: 41-00073  
Facility Carried: SMITH VALLEY ROAD

Bridge Inspection Report

(59) SUPERSTRUCTURE: 6 - Satisfactory Condition (minor deterioration)

Comments:

SPALLS WITH EXPOSED STEEL STRAPS ON BOTTOM OF MOST BEAMS AT ENDS. SPALL WITH TWO EXPOSED REBARS ON THE BOTTOM OF BEAM 2; THEY DON'T APPEAR TO BE STRANDS.

Material:

7 - 45" PRESTRESSED CONCRETE I-BEAMS @ 6.0'

(60) SUBSTRUCTURE: 7 - Good Condition (some minor problems)

Comments:

NO NOTABLE DEFICIENCIES

Material:

CONCRETE END BENTS

(61) CHANNEL/CHANNEL PROTECTION: 6 - Bank slump. widespread minor damage

Comments:

NO SCOUR. GOOD RIPRAP PROTECTION. DEBRIS TRAPPED AT BEAM SEATS AND LARGE BRANCH WEDGED AGAINST BEAM AT CENTER. SILTING IN MID-CHANNEL PUSHING MAIN FLOW TO EAST.

Material:

RIPRAP/NATURAL

(62) CULVERTS: N - Not Applicable

Comments:

**LOAD RATING AND POSTING**

(31) DESIGN LOAD:	5 - HS 20	(66) INVENTORY RATING:	47
(70) BRIDGE POSTING	5 - Equal to or above legal loads	(65) INVENTORY RATING METHOD: 1 - Load Factor (LF)	
(41) STRUCTURE OPEN/POSTED/CLOSED:	A - Open	(66B) INVENTORY RATING (H):	34
(64) OPERATING RATING:	99	(66C) TONS POSTED :	
(63) OPERATING RATING METHOD:	1 - Load Factor (LF)	(66D) DATE POSTED/CLOSED:	

**APPRAISAL**

SUFFICIENCY RATING:	96.9	(36) TRAFFIC SAFETY FEATURE:	
STATUS:	0	36A) BRIDGE RAILINGS:	1
(67) STRUCTURAL EVALUATION:	6	36B) TRANSITIONS:	1
(68) DECK GEOMETRY:	5	36C) APPROACH GUARDRAIL:	1
(69) UNDERCLEARANCES, VERTICAL & HORIZONTAL:	N	36D) APPROACH GUARDRAIL ENDS:	1

(71) WATERWAY ADEQUACY: 5 - Occasional Overtopping of Approaches - Significant Delays

Comments:

APPEARS SLIGHTLY LOW

(72) APPROACH ROADWAY ALIGNMENT: 8 - Equal to present desirable criteria

Comments:

STRAIGHT, IN CREST CURVE

Inspector: Jacob Gould  
 Inspection Date: 07/15/2020

Asset Name: 41-00073  
 Facility Carried: SMITH VALLEY ROAD

Bridge Inspection Report

(113) SCOUR CRITICAL BRIDGES: 8 - Stable for scour conditions

Comments:  
 NO SCOUR ISSUES OBSERVED, WELL PROTECTED.

**CLASSIFICATION**

(20) TOLL:	3 - On Free Road	(21) MAINT. RESPONSIBILITY:	02 - County Highway Agency
(22) OWNER:	02 - County Highway Agency	(26) FUNCTIONAL CLASS OF INVENTORY RTE:	17 - Urban - Collector
(37) HISTORICAL SIGNIFICANCE:	5 - Not eligible	(100) STRAHNET HIGHWAY:	Not a STRAHNET route
(101) PARALLEL STRUCTURE:	N - No parallel structure	(102) DIRECTION OF TRAFFIC:	2-way traffic
(103) TEMPORARY STRUCTURE:		(104) HIGHWAY SYSTEM OF INVENTORY ROUTE:	0 - Structure/Route is NOT on NHS
(105) FEDERAL LANDS HIGHWAYS:	0-Not Applicable	(110) DESIGNATED NATIONAL NETWORK:	Inventory route not on network
(112) NBIS BRIDGE LENGTH:	Yes		

**NAVIGATION DATA**

(38) NAVIGATION CONTROL:	0 - No navigation control on waterway (bridge permit not required)	(39) NAVIGATION VERTICAL CLEAR:	000.0 FT
(111) PIER OR ABUTMENT PROTECTION:		(116) MINIMUM NAVIGATION VERT. CLEARANCE, VERT. LIFT BRIDGE:	FT
		(40) NAV HORIZONTAL CLEARANCE:	0000.0 FT

**PROPOSED IMPROVEMENTS**

(75A) TYPE OF WORK:		(95) ROADWAY IMPROVEMENT COST:	\$ 000000
(75B) WORK DONE BY:		(96) TOTAL PROJECT COST:	\$ 000000
(76) LENGTH OF IMPROVEMENT:	00000.0 FT	(97) YR OF IMPROVEMENT COST EST:	
(94) BRIDGE IMPROVEMENT COST:	\$ 000000	(114) FUTURE AVG DAILY TRAFFIC:	014473
		(115) YR OF FUTURE ADT:	2039