

**FHWA-Indiana Environmental Document**  
**CATEGORICAL EXCLUSION / ENVIRONMENTAL ASSESSMENT FORM**  
**GENERAL PROJECT INFORMATION**

<b>Road No./County:</b>	Park Road (Canyon Road) / Jefferson County
<b>Designation Number(s):</b>	2200147
<b>Project Description/Termini:</b>	Bridge Rehabilitation on Park Road (Canyon Road) over Little Crooked Creek, approximately 0.40 mile north of Clifty Hollow Road. The project area extends approximately 250 feet northwest and 350 feet southeast of the bridge centerline for a total of 600 feet.

	<b>Categorical Exclusion, Level 2</b> – Required Signatories: INDOT DE and/or INDOT ESD
	<b>Categorical Exclusion, Level 3</b> – Required Signatories: INDOT ESD
<b>X</b>	<b>Categorical Exclusion, Level 4</b> – Required Signatories: INDOT ESD and FHWA
	<b>Environmental Assessment (EA)</b> – Required Signatories: INDOT ESD and FHWA
	<b>Additional Investigation (AI)</b> – The proposed action included a design change from the original approved environmental document. Required Signatories must include the appropriate environmental approval authority.

**Approval**

\_\_\_\_\_  
 INDOT DE Signature and Date                      INDOT ESD Signature and Date

\_\_\_\_\_  
 FHWA Signature and Date

**Release for Public Involvement**

\_\_\_\_\_                      **N/A**                      **ADWP**                      October 8, 2024  
 INDOT DE Initials and Date                      INDOT ESD Initials and Date

**Certification of Public Involvement**

\_\_\_\_\_  
 INDOT Consultant Services Signature and Date

**INDOT DE/ESD Reviewer Signature and Date:** \_\_\_\_\_

**Name and Organization of CE/EA Preparer:** \_\_\_\_\_  
 Raquel Walker – Egis BLN USA Inc. (Egis)

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Note: Refer to the most current INDOT CE Manual, guidance language, and other ESD resources for further guidance regarding any section of this form.

## Part I – Public Involvement

Every Federal action requires some level of public involvement, providing for early and continuous opportunities throughout the project development process. **The level of public involvement should be commensurate with the proposed action.**

	Yes	No
Does the project have a historic bridge processed under the Historic Bridges PA*?	<input checked="" type="checkbox"/>	<input type="checkbox"/>
If No, then: Opportunity for a Public Hearing Required?	<input type="checkbox"/>	<input type="checkbox"/>

\*A public hearing is required for all historic bridges processed under the Historic Bridges Programmatic Agreement between INDOT, FHWA, SHPO, and the ACHP.

Discuss what public involvement activities (legal notices, letters to affected property owners and residents (i.e., notice of entry), meetings, special purpose meetings, newspaper articles, etc.) have occurred for this project.

Notice of Entry letters were not required for this project as all work will take place within the Indiana Department of Natural Resources (IDNR) owned right-of-way (ROW).

### Section 106

To meet the public involvement requirements of Section 106, a legal notice of the Federal Highway Administration's (FHWA) finding of "No Historic Properties Affected" was published in *The Madison Courier* on March 26, 2024, offering the public an opportunity to submit comments pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later on April 26, 2024. The text of the public notice and the affidavit of publication appear in Appendix D, pages 74 and 75. No comments were received in response to the public notice.

### Historic Bridge

Pursuant to the Historic Bridge Programmatic Agreement (PA), a public hearing is required for this project. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled.

## Public Controversy on Environmental Grounds

Discuss public controversy concerning community and/or natural resource impacts, including what is being done during the project to minimize impacts.

At this time, there is no substantial public controversy concerning impacts to the community or natural resources.

## Part II - General Project Identification, Description, and Design Information

Sponsor of the Project: Indiana Department of Natural Resources (IDNR) & Indiana Department of Transportation (INDOT) INDOT District: Seymour

Local Name of the Facility: Park Road (Canyon Road)

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Funding Source (mark all that apply): Federal  State  Local  Other\*

\*If other is selected, please identify the funding source: \_\_\_\_\_

## PURPOSE AND NEED:

*The need should describe the specific transportation problem or deficiency that the project will address. The purpose should describe the goal or objective of the project. The solution to the traffic problem should NOT be discussed in this section.*

**Need:** The need for this project is attributed to deterioration present on the existing structure (P000-39-06876 (National Bridge Inventory (NBI): 060290) conveying the Park Road over Little Crooked Creek that continues to worsen and will eventually compromise the safety of the public. The existing structure was built in 1947, and is listed as a programmatically determined, "Select" historic bridge eligible for the National Register of Historic Places (NHRP) in the *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (2009). The Alternative Analysis Report states the bridge is rated in fair condition (5 out of 9). Condition ratings range from "0" to "9", with "0" being a failed structure and "9" being a structure in excellent condition. There are areas of cracking in the approach slabs and bridge-wearing surface as well as areas of cracking, delamination, and spalling in the spandrel beams, floor beams, columns, and arches. Erosion is present at the footings of the columns and piers. The wearing surface and deck are rated in a satisfactory condition (6 out of 9) but have areas of more significant deterioration, which continue to worsen. The superstructure and substructure are rated in fair condition (5 out of 9), with areas of deterioration limiting the functional life of the bridge. If these areas are not repaired and protected, it is estimated the bridge could have a reduced load capacity in 15 years (Appendix D, pages 41 to 44).

**Purpose:** The purpose of this project is to improve the deteriorated portions of the bridge, including exposed footings, resulting in an overall bridge condition rating of 7 out of 9 (good) or better, and to prolong the life of the bridge by an estimated 25 years while preserving its historical aspects.

## PROJECT DESCRIPTION (PREFERRED ALTERNATIVE):

County: Jefferson Municipality: Madison

Limits of Proposed Work: 250 feet northwest and 350 feet southeast of the bridge centerline for a total of 600 feet

Total Work Length: 0.095 Mile(s) Total Work Area: 0.261 Acre(s)

Is an Interstate Access Document (IAD)<sup>1</sup> required?

If yes, when did the FHWA provide a Determination of Engineering and Operational Acceptability?

Yes <sup>1</sup>	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
Date: _____	

<sup>1</sup>If an IAD is required; a copy of the approved CE/EA document must be submitted to the FHWA with a request for final approval of the IAD.

*Describe location of project including township, range, city, county, roads, etc. Existing conditions should include current conditions, current deficiencies, roadway description, surrounding features, etc. Preferred alternative should include the scope of work, anticipated impacts, and how the project will meet the Purpose and Need. Logical termini and independent utility also need discussed.*

INDOT, in conjunction with the IDNR and with funding from the FHWA, intends to proceed with a bridge rehabilitation project on Park Road over Little Crooked Creek.

### Location:

This project is located on Park Road (also known as Canyon Road) over Little Crooked Creek, approximately 0.4 mile north of Clifty Hollow Road in Clifty Falls State Park, in Madison Township, near the City of Madison, in Jefferson County, Indiana. Specifically, this project is located in Section 33, Township 4 North, Range 10 East, as shown on the Madison West U.S. Geological Survey (USGS)

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7.5 Minute Topographic Map (Appendix B, page 2).

### Existing Conditions:

Bridge No. P000-39-06876 (NBI: 060290) is a single-span, open-spandrel concrete arch bridge with reinforced concrete slab approach spans on each side. The superstructure is comprised of a reinforced concrete deck that spans from floor beam to floor beam and is supported by columns. A reinforced concrete arch supports the main span columns, and spread footings support the approach span columns. The bridge is approximately 176 feet long, with an out-to-out width of 30.33 feet. This bridge has two reinforced concrete slab approach spans on each side, 2.17-foot-wide sidewalks flanking the bridge deck, and 1 foot concrete barriers on each side. Though this structure carries the only southern access road to features and facilities within Clifty Falls State Park, access to the park and the features and facilities will be maintained throughout the duration of the project via the northern park entrance. While the road is closed all features and facilities will still be accessible from the north park entrance. It is listed as a programmatically determined, "Select" historic bridge eligible for the NHRP in the *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (2009). It was built in 1947 and rehabilitated in 1980, 1990, and 2000. Deterioration is present on the existing structure in the form of cracking in the approach slabs and bridge wearing surface as well as areas of cracking, delamination, and spalling in the spandrel beams, floor beams, columns, and arches. Erosion is present at the footings of the columns and piers. The wearing surface and deck have areas of more significant deterioration, which continue to worsen. The superstructure and substructure have areas of deterioration, limiting the functional life of the bridge. If these areas are not repaired and protected, it is estimated the bridge could have a reduced load capacity in 15 years.

Park Road is a two-lane asphalt roadway conveying motorists into the park from the southern entrance of Clifty Falls State Park. The roadway consists of two 10-foot travel lanes, one in each direction, with two-foot shoulders and timber guardrail in the southwest and southeast quadrants of the project area.

The project is located within Clifty Falls State Park, which is afforded protection under Section 4(f) of the U.S. Department of Transportation Act of 1966, as a publicly owned park and is also considered a Section 6(f) Land and Water Conservation Fund (LWCF) site. Surrounding land use is forested, with steep cliffs lining the roadway, and the bridge crosses over Little Crooked Creek.

### Preferred Alternative:

The IDNR and INDOT, with funding from FHWA, intend to proceed with a bridge rehabilitation project on Park Road over Little Crooked Creek. The Alternative Analysis Report for this project evaluated two alternatives, Alternatives A (No-Build) and Alternative B.1 (Rehabilitation for Continued Vehicular Use (Two-Way)), before finding a feasible and prudent alternative that meets the project's purpose and need. A description of the preferred alternative, as noted in the Alternative Analysis Report (Appendix D, pages 37 to 46), can be found below.

### Alternative B.1 Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-Way Option) Meeting Secretary of Interior's Standard (SOIS) for Rehabilitation

The preferred alternative consists of rehabilitating the existing Select bridge for continued vehicular use, meeting the SOIS for Rehabilitation. This alternative addresses the structural deterioration of the bridge in order to prolong the life of the structure.

The rehabilitation efforts will include hydrodemolition of the existing bridge deck and placement of a rigid concrete overlay over the driving surface, curb repair by patching, replacing bridge expansion joints between deck sections with pre-compressed foam joints, and replacing the reinforced concrete approach slabs. The existing approach slabs will be saw cut at the curb face, allowing the existing bridge railing transitions to remain. The new approach slabs will be attached to the remaining portions of the existing approach slabs. Deteriorated portions of the floor beams, piers, spandrel columns, and arches will be repaired by removing the deteriorated concrete and patching the areas with concrete patch and, in some areas, applying a Fiber Reinforced Polymer (FRP) around the repaired area to ensure the patch will not delaminate and fall off the component. The FRP will extend approximately one foot past the repaired area. If the size of the patch warrants, an embedded galvanic anode will be placed in the FRP patched areas to slow the corrosion of the reinforcing steel and prolong the life of the patch. Spalls on the front face of the abutments and wingwalls will be patched. Deteriorated portions of the tie beams between the north and south arches will also be repaired by removing deteriorated portions of the concrete and applying a concrete patch with FRP as needed. Tie Beam B-10 will be replaced due to excessive deterioration. In all areas where FRP is utilized, it will be color-matched to the existing arch. Two locations along the arch and one pedestal have cracking but do not require patching. The cracks will be epoxy injected to prevent the intrusion of water, which could create delamination or spalls. The curb, sidewalk, and railing will be cleaned and sealed, and the northeast side paved



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ditch will be converted from a V-shaped ditch to a flat bottom ditch. Riprap may need to be placed around the piers for scour protection. The existing fence in the northwest and southwest portions of the bridge will be reset as needed. Approximately 10 feet of full-depth patching and 40 feet of resurfacing will be needed on Park Road at each end of the bridge. There are no changes to the Park Road cross-section.

Please refer to Appendix B, pages 17 to 25, for plan sheets illustrating the above scope of work.

This alternative will meet the project's purpose and need by correcting the deteriorated portions of the bridge and prolonging the life of the bridge by an estimated 25 years. The rehabilitation activities will improve the overall condition rating to a seven (good) or better and preserve the existing structure's historical integrity. The estimated cost of this project is currently \$787,000, the estimated cost of the rehabilitation included in the Alternatives Analysis Report was \$825,000 (Appendix D, page 45), and the estimated cost of construction in the Indiana State Transportation Improvement Program (STIP) is \$1,100,000 (Appendix G, page 1). The cost difference between the current estimate, the Alternatives Analysis Report, and the STIP is due to fluctuating construction costs, materials, and labor.

The preferred maintenance of traffic (MOT) is anticipated to be a road closure with a detour route utilizing Clifty Hollow Road, US 421, and SR 56. It would be in place for approximately 60 days or less. Please refer to this document's *Maintenance of Traffic* section for additional details.

**Logical Termini/Independent Utility:**

The project area extends approximately 250 feet northwest and 350 feet southeast of the bridge centerline for a total of 600 feet. The logical termini were developed to minimize impacts on the surrounding environment while including limits necessary to address the project need. Impacts have been minimized to the greatest extent practicable in order to meet the purpose and need of the project. The project has independent utility as the completion will not dictate the outcome of any other projects in the surrounding area and is not dependent on any other planned projects.

**OTHER ALTERNATIVES CONSIDERED:**

*Provide a header for each alternative. Describe all discarded alternatives, including the No Build Alternative. Explain why each discarded alternative was not selected. Make sure to state how each alternative meets or does not meet the Purpose and Need and why.*

**Alternative A: No-Build**

The No-Build/Do-Nothing alternative was considered for the project. This alternative means no federal funds would be expended, and no action would occur. The No-Build/Do-Nothing alternative would not address the purpose of the project, which is to correct the deteriorated portions of the bridge and prolong its service life while preserving its historical aspects. If the No-Build/Do-Nothing alternative is selected, the existing bridge will continue to deteriorate and be unusable for motorists. This alternative does not meet the project's purpose and need; therefore, it is not feasible or prudent.

**The No Build Alternative is not feasible, prudent, or practicable because (Mark all that apply):**

- It would not correct existing capacity deficiencies;
- It would not correct existing safety hazards;
- It would not correct the existing roadway geometric deficiencies;
- It would not correct existing deteriorated conditions and maintenance problems; or
- It would result in serious impacts to the motoring public and general welfare of the economy.
- Other (Describe):

X

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### ROADWAY CHARACTER:

*If the proposed action includes multiple roadways, complete and duplicate for each roadway.*

Name of Roadway	<u>Park Road, also known as Canyon Road</u>			
Functional Classification:	<u>Recreational (primary access)</u>			
Current ADT:	<u>400</u>	<u>VPD (2015)</u>	Design Year ADT:	<u>521</u> <u>VPD (2045)</u>
Design Hour Volume (DHV):	<u>65</u>	Truck Percentage (%)	<u>12.5</u>	
Designed Speed (mph):	<u>15</u>	Legal Speed (mph):	<u>15</u>	

	Existing		Proposed	
Number of Lanes:	2		2	
Type of Lanes:	10-foot travel		10-foot travel	
Pavement Width:	24	ft.		ft.
Shoulder Width:	2.75	ft.	2.75	ft.
Median Width:	0	ft.	0	ft.
Sidewalk Width:	2.17	ft.	2.17	ft.

Setting:	<input type="checkbox"/> Urban	<input type="checkbox"/> Suburban	<input checked="" type="checkbox"/> Rural
Topography:	<input type="checkbox"/> Level	<input type="checkbox"/> Rolling	<input checked="" type="checkbox"/> Hilly

### BRIDGES AND/OR SMALL STRUCTURE(S):

*If the proposed action includes multiple structures, complete and duplicate for each bridge and/or small structure. Include both existing and proposed bridge(s) and/or small structure(s) in this section.*

 Structure/NBI Number(s): P000-39-06876 / NBI: 060290      Sufficiency Rating: 72.8, Alternative Analysis (Appendix D, page 39)  
 (Rating, Source of Information)

	Existing		Proposed	
Bridge/Structure Type:	Open Spandrel Concrete Arch		Open Spandrel Concrete Arch	
Number of Spans:	1		1	
Weight Restrictions:	32	ton	32	ton
Height Restrictions:	N/A	ft.	N/A	ft.
Curb to Curb Width:	24	ft.	24	ft.
Outside to Outside Width:	30.33	ft.	30.33	ft.
Shoulder Width:	1	ft.	1	ft.

*Describe impacts and work involving bridge(s), culvert(s), pipe(s), and small structure(s). Provide details for small structure(s): structure number, type, size (length and dia.), location and impacts to water. Use a table if the number of small structures becomes large. If the table exceeds a complete page, put it in the appendix and summarize the information below with a citation to the table.*

The project involves the rehabilitation of Bridge No. P000-39-06876 (NBI: 060290) is a single-span, open-spandrel concrete arch bridge with approach spans on each side. The bridge was constructed in 1947 and rehabilitated in 1980, 1990 and 2000. The superstructure is comprised of a reinforced concrete deck that spans from floor beam to floor beam and is supported by columns. A reinforced concrete arch supports the main span columns, and spread footings support the approach span columns. The bridge is approximately 176 feet long, with an out-to-out width of 30.33 feet. This bridge has two reinforced concrete slab approach spans on each side, 2.17-foot-wide sidewalks flanking the bridge deck, and 1-foot concrete barriers on each side. It is listed as a programmatically determined, "Select" historic bridge, eligible for the National Register of Historic Places (NHRP) in the *Indiana Historic Bridge Inventory Volume 2: Listing of Historic and Non-Historic Bridges* (2009). The scope of work will include replacing the

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existing bridge deck, replacing bridge expansion joints between deck sections with pre-compressed foam joints, and replacing the reinforced concrete approach slabs. Deteriorated portions of the floor beams, piers, spandrel columns, and arches will be repaired by removing the deteriorated concrete and patching the areas with concrete patch and, in some areas, applying a Fiber Reinforced Polymer (FRP) around the repaired area to ensure the patch will not delaminate and fall off the component. Where needed, an embedded galvanic anode will be placed in the FRP patched areas to slow the corrosion of the reinforcing steel and prolong the life of the patch. Spalls on the front face of the abutments and wingwalls will be patched. Deteriorated portions of the tie beams between the north and south arches will also be repaired by removing deteriorated portions of the concrete and applying a concrete patch with FRP as needed. Tie Beam B-10 will be replaced due to excessive deterioration. In all areas where FRP is utilized, it will be color-matched to the existing arch. The cracks will be epoxy injected to prevent the intrusion of water.

No other structures are within the project area.

### MAINTENANCE OF TRAFFIC (MOT) DURING CONSTRUCTION:

	Yes	No
Is a temporary bridge proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is a temporary roadway proposed?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project involve the use of a detour or require a ramp closure? (describe below)	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for access by local traffic and so posted.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made for through-traffic dependent businesses.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Provisions will be made to accommodate any local special events or festivals.	<input checked="" type="checkbox"/>	<input type="checkbox"/>
Will the proposed MOT substantially change the environmental consequences of the action?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Is there substantial controversy associated with the proposed method for MOT?	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Will the project require a sidewalk, curb ramp, and/or bicycle lane closure? (describe below)	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Provisions will be made for access by pedestrians and/or bicyclists, and so posted (describe below).	<input checked="" type="checkbox"/>	<input type="checkbox"/>

*Discuss closures, detours, and/or facilities (if any) that will be provided for maintenance of traffic. Any known impacts from these temporary measures should be quantified to the extent possible, particularly with respect to properties such as Section 4(f) resources and wetlands. Discuss any pedestrian/bicycle closures. Any local concerns about access and traffic flow should be detailed as well.*

The preferred MOT for the project is anticipated to be a full road closure with a detour route utilizing Clifty Hollow Road, US 421, and SR 56. This detour adds 5.1 miles for motorists and will be in place for approximately 60 days or less (Appendix B, Page 16). The project is anticipated to be completed in late Spring 2026, with the road being closed in March and April to have the least amount of inconvenience to park visitors. Access to the park and features will be maintained throughout the duration of the project via the northern park entrance.

The closures/lane restrictions will pose a temporary inconvenience to traveling motorists (including school buses and emergency services); however, no significant delays are anticipated, and all inconveniences and delays will cease upon project completion.

### ESTIMATED PROJECT COST AND SCHEDULE:

Engineering: \$ 393,500 (2023 & 2026\*) Right-of-Way: \$ 0 (20--) Construction: \$ 1,100,000 (2026)

\*Engineering costs were included in the previous STIP under the year 2022. If necessary, the INDOT Project Manager will ensure the STIP will be updated prior to the Ready-for-Contract (RFC) date to reflect increasing costs. This has been added as a firm commitment in the *Environmental Commitments* section of this document.

Anticipated Start Date of Construction: Spring 2026

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### RIGHT OF WAY:

Land Use Impacts	Amount (acres)	
	Permanent	Temporary
Residential	0	0
Commercial	0	0
Agricultural	0	0
Forest	0	0
Wetlands	0	0
Other:	0	0
Other:	0	0
<b>TOTAL</b>	<b>0</b>	<b>0</b>

*Describe both Permanent and Temporary right-of-way and describe their current use. Typical and Maximum right-of-way widths (existing and proposed) should also be discussed. Any advance acquisition, reacquisition, or easements, either known or suspected, and their impacts on the environmental analysis should be discussed.*

This project occurs within the IDNR publicly owned Clifty Falls State Park property. This project will occur within the existing IDNR-owned ROW. No permanent or temporary ROW will be required for this project.

If the scope of work or permanent or temporary right-of-way amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately.

## Part III – Identification and Evaluation of Impacts of the Proposed Action

### SECTION A - EARLY COORDINATION:

*List the date(s) coordination was sent and all resource agencies that were contacted as a part of the development of this Environmental Study. Also, include the date of their response or indicate that no response was received.*

Early coordination letters were sent on May 12, 2023 (Appendix C, pages 1 to 2).

<u>Agency</u>	<u>Date Sent</u>	<u>Response Received</u>	<u>Appendix</u>
Indiana Geological and Water Survey (IGWS) - Electronic	May 12, 2023	May 12, 2023	C4 to C6
INDOT, Office of Aviation	May 12, 2023	May 15, 2023	C7 to C8
Jefferson County Emergency Management	May 12, 2023	May 15, 2023	C9
IDNR- Division of State Parks	May 12, 2023	May 12, 2023 & October 30, 2023	C10 to C12
Indiana Department of Natural Resources, Division of Fish and Wildlife (IDNR, DFW)	May 12, 2023	June 6, 2023	C13 to C14
FHWA	May 12, 2023	No Response	-
INDOT, Seymour District	May 12, 2023	No Response	-
U.S. Fish and Wildlife Service (USFWS)	May 12, 2023	No Response	-
U.S. Army Corps of Engineers (USACE)	May 12, 2023	No Response	-

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INDOT, Project Manager	May 12, 2023	No Response	-
National Park Service	May 12, 2023	No Response	-
U.S. Department of Housing and Urban Development	May 12, 2023	No Response	-
Jefferson County Highway Department	May 12, 2023	No Response	-
Jefferson County Surveyor's Office	May 12, 2023	No Response	-
Jefferson County Commissioner's Office	May 12, 2023	No Response	-
IDNR, Division of Engineering Project Manager	May 12, 2023	No Response	-
Clifty Falls State Park, Property Manager	May 12, 2023	No Response	-
Area Plan Commission of Tippecanoe County (APC)	May 12, 2023	No Response	-
IDNR, Division of Law Enforcement	May 12, 2023	No Response	-
INDOT, Cultural Resources Office Historic Bridge Specialist	May 12, 2023	No Response	-

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

### SECTION B – ECOLOGICAL RESOURCES:

#### Streams, Rivers, Watercourses & Other Jurisdictional Features

- Federal Wild and Scenic Rivers
- State Natural, Scenic or Recreational Rivers
- Nationwide Rivers Inventory (NRI) listed
- Outstanding Rivers List for Indiana
- Navigable Waterways

#### Presence

X

#### Impacts

Yes	No
	X

Total stream(s) in project area: 135 Linear feet      Total impacted stream(s): 0 Linear feet

Stream Name	Classification	Total Size in Project Area (linear feet)	Impacted linear feet	Comments (i.e., location, flow direction, likely Water of the US, appendix reference)
Little Crooked Creek	Intermittent	135	0	Little Crooked Creek flows northeast to southwest through the project area and is a likely jurisdictional Water of the U.S. (Appendix F, page 3).

*Describe all streams, rivers, watercourses, and other jurisdictional features adjacent to or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if the streams or rivers are listed on any federal or state lists for Indiana. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the Red Flag Investigation (RFI) report (Appendix E, pages 1 to 10), there are five streams, rivers, watercourses, or other jurisdictional features within the 0.5-mile search radius. There is one stream within or adjacent to the project area. This number was confirmed during the site visit on May 31, 2023, by Egis.

A *Waters of the U.S. Determination/Wetland Delineation Report* was approved by the INDOT Ecology, Waterway Permitting, and Stormwater Office on March 1, 2024. Please refer to Appendix F, pages 1 to 15, for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that one likely jurisdictional waterway, Little Crooked Creek, is within the project area. USACE makes all final determinations regarding jurisdiction.

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**Little Crooked Creek**

Little Crooked Creek flows northeast to southwest through the project area under Park Road and is depicted as a dotted blue line on the USGS topographic map, indicating an intermittent stream (Appendix B, page 2). Approximately 135 feet of Little Crooked Creek is within the project area. The riparian corridor within the project area consists of forested cover along each side of the stream with steep slopes. Little Crooked Creek has a defined bed and bank throughout the project limits, with an upstream ordinary high-water mark (OHWM) of approximately 8 feet wide and 0.3 feet deep and a downstream OHWM of approximately 10 feet wide and 0.3 feet deep. Riffles and pooling were present, and the substrate consisted of rock within the channel. The quality of the stream is considered average. A review of the USGS National Hydrography Dataset revealed Little Crooked Creek flows into the Ohio River, a traditional navigable water (TNW); therefore, it would likely be considered a jurisdictional Water of the U.S.

Little Crooked Creek is not listed as a navigable waterway on the Indiana Department of Natural Resources (IDNR) Listing of Outstanding Rivers and Streams, is not listed as a Federal, Wild, and Scenic River, State Natural, Scenic, and Recreational River; or on the National Rivers Inventory, nor are there any present in or adjacent to the project area. No impacts to Little Crooked Creek are anticipated. Due to the topography of the area, the stream bed of Crooked Creek is at a lower elevation than the bridge, and outside of the construction limits. Any riprap placed around the piers for scour protection will occur above the OHWM.

IDNR-DFW responded on June 8, 2023, with recommendations to avoid and minimize impacts to Little Crooked Creek (Appendix C, pages C13 to C14). All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

<b>Open Water Feature(s)</b>	<b><u>Presence</u></b>	<b><u>Impacts</u></b>	
		<b>Yes</b>	<b>No</b>
Reservoirs	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Lakes	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Farm Ponds	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Retention/Detention Basin	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Storm Water Management Facilities	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Other: _____	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

*Describe all open water feature(s) identified adjacent to or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages 1 to 10), there are three open water features within the 0.5-mile search radius. There are no open water features within or adjacent to the project area, which was confirmed by the site visit on May 31, 2023, by Egis. Therefore, no impacts are expected.

A *Waters of the U.S. Determination / Wetland Delineation Report* was approved by the INDOT Ecology, Waterway Permitting, and Stormwater Office on March 1, 2024. Please refer to Appendix F, pages 1 to 15, for the *Waters of the U.S. Determination / Wetland Delineation Report*. It was determined that no open water features are within the project area. USACE makes all final determinations regarding jurisdiction.

<b>Wetlands</b>	<b><u>Presence</u></b>	<b><u>Impacts</u></b>	
		<b>Yes</b>	<b>No</b>
	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total wetland area: 0 Acre(s)      Total wetland area impacted: 0 Acre(s)

(If a determination has not been made for non-isolated/isolated wetlands, fill in the total wetland area impacted above.)

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Wetland No.	Classification	Total Size (Acres)	Impacted Acres	Comments (i.e., location, likely Water of the US, appendix reference)
N/A	N/A	N/A	N/A	N/A

**Wetlands** (Mark all that apply)

- Wetland Determination
- Wetland Delineation
- USACE Isolated Waters Determination

**Documentation**

X

**ESD Approval Dates**

March 1, 2024

**Improvements that will not result in any wetland impacts are not practicable because such avoidance would result in** (Mark all that apply and explain):

- Substantial adverse impacts to adjacent homes, businesses, or other improved properties;
- Substantially increased project costs;
- Unique engineering, traffic, maintenance, or safety problems;
- Substantial adverse social, economic, or environmental impacts, or
- The project is not meeting the identified needs.


*Describe all wetlands identified adjacent to or within the project area. Include whether or not impacts (both permanent and temporary) will occur to the features identified. Include if features are likely subject to federal or state jurisdiction. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on the desktop review, the aerial map of the project area, and the RFI report (Appendix E, pages 1 to 10), there is one wetland within the 0.5-mile search radius. There are no wetlands within or adjacent to the project area, which was confirmed by the site visit on May 31, 2023, by Egis. Therefore, no impacts are expected.

A *Waters of the U.S. Determination/Wetland Delineation Report* was approved by the INDOT Ecology, Waterway Permitting, and Stormwater Office on March 1, 2024. Please refer to Appendix F, pages 1 to 15, for the *Waters of the U.S. Determination/Wetland Delineation Report*. It was determined that no wetland features are within the project area. USACE makes all final determinations regarding jurisdiction.

**Terrestrial Habitat**

**Presence**

X
---

**Impacts**

Yes	No
X	

Total terrestrial habitat in project area: 0.092 Acre(s)      Total tree clearing: 0.073 Acre(s)

*Describe types of terrestrial habitat (i.e. forested, grassland, farmland, lawn, etc) adjacent or within the project area. Include whether or not impacts will occur to habitat identified. Include total terrestrial habitat impacted and total tree clearing that will occur. Discuss measures to avoid, minimize, and mitigate if impacts will occur.*

Based on a desktop review, a site visit on May 31, 2023, by Egis, and the aerial map of the project area (Appendix B, page 3), there is forested riparian habitat within the project area. Vegetation within the project area includes slippery elm (*Ulmus rubra*), American beech (*Fagus grandifolia*), yellowwood (*Cladrastis kentuckea*), Virginia creeper (*Parthenocissus quinquefolia*), tree of heaven (*Ailanthus altissima*), Eastern redbud (*Cercis canadensis*), black locust (*Robinia pseudoacacia*), tulip tree (*Liriodendron tulipifera*) and garlic mustard (*Alliaria petiolata*). Suitable summer habitat exists within the project area, and approximately 0.092 acre of terrestrial disturbance, including 0.073 acre of tree and shrub clearing, is expected for the placement of riprap. Any disturbed areas will be restored following the completion of construction activities. Anticipated tree clearing and replanting will be coordinated with Clifty Falls Property Management (Appendix C, page 10). This is included as a firm commitment in the *Environmental Commitments*

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section of this CE document. No mitigation is anticipated.

The IDNR-DFW responded on June 8, 2023, with several recommendations to avoid or minimize impacts on terrestrial habitat. These recommendations included developing a mitigation plan for any unavoidable habitat impacts as well as recommendations for installing bank stabilization measures, revegetating all bare and disturbed areas, and installing appropriate erosion and sediment control measures. In their response letter, the IDNR-DFW also indicated that if impacts to non-wetland riparian forest are less than one acre, the non-wetland forest removed in a rural setting should be replaced at a 1:1 ratio based on area (Appendix C, pages 13 to 14). Any disturbed areas will be restored following the completion of construction activities.

All applicable recommendations are included in the *Environmental Commitments* section of this CE document.

**Protected Species**

**Federally Listed Bats**

Information for Planning and Consultation (IPaC) determination key completed  
 Section 7 informal consultation completed (IPaC cannot be completed)  
 Section 7 formal consultation Biological Assessment (BA) required

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Determination Received for Listed Bats from USFWS:            NE             NLAA             LAA

**Other Species not included in IPaC**

Additional federal species found in project area (based on IPaC species list)  
 State species (not bird) found in project area (based upon consultation with IDNR)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

**Migratory Birds**

Known usage or presence of birds (i.e., nests)  
 State bird species based upon coordination with IDNR

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

*Discuss IDNR coordination and species identified. Describe USFWS Section 7 consultation and determination received for Indiana bat and northern long-eared bat impacts. Discuss if other federally listed species were identified. If so, include consultation that has occurred and the determination that was received. Discuss if migratory birds have been observed and any impacts.*

Based on a desktop review and the RFI report (Appendix E, pages 1 to 10), completed by Egis on March 20, 2023, the IDNR Jefferson County Endangered, Threatened and Rare (ETR) Species List has been checked. According to the IDNR-DFW early coordination response letter dated June 8, 2023 (Appendix C, pages 13 to 14), the Natural Heritage Program's Database has been checked, and to date, no plant or animal species listed as states or federally endangered or rare have been reported to occur in the project vicinity. An INDOT 0.5-mile bat review occurred on November 16, 2022, and did not indicate the presence of endangered bat species within 0.5 mile of the project area.

Project information was submitted through the USFWS's Information for Planning and Consultation (IPaC) portal, and an official species list was generated (Appendix C, pages 15 to 28). The project is within range of the federally endangered Indiana bat (*Myotis sodalis*) and Northern Long-eared bat (NLEB) (*Myotis septentrionalis*). Other species were generated in the IPaC species list, along with the Indiana bat and NLEB. Refer to the paragraph below.

The project qualifies for the *Range-wide Programmatic Informal Consultation for the Indiana bat and Northern Long-eared bat (NLEB)*, dated May 2016 (revised February 2018), between FHWA, Federal Railroad Administration (FRA), Federal Transit Administration (FTA), and USFWS. An effect determination key was completed on March 1, 2023, and based on the responses provided, the project was found to "Not Likely to Adversely Affect" the Indiana bat and/or the NLEB (Appendix C, pages 29 to 44). INDOT reviewed and verified the effect finding on March 1, 2024, and requested USFWS's review of the finding. No response was received from USFWS within the 14-day review period; therefore, it was concluded they concur with the finding. The USFWS



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recommends general lighting hibernacula and tree removal Avoidance and Minimization Measures (AMMs). The AMMs and/or commitments are included as firm commitments in the *Environmental Commitments* section of this document.

A bridge inspection occurred on September 28, 2022, which found signs of bats to be present. A subsequent bridge inspection on May 31, 2023, by Egis did not indicate bats or signs of bats were present (Appendix C, page 45). Bridge/Structure Assessments are only valid for two years. If construction will begin after May 31, 2025, an inspection of the structure by a qualified individual must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT District Environmental Manager must be contacted immediately. This firm commitment is included in the *Environmental Commitments* of this document.

The official species list generated from IPaC indicated six other species present within the project area. The endangered Gray bat (*Myotis grisescens*), the proposed endangered Tricolored Bat (*Perimyotis subflavus*), the experimental population, non-essential Whooping Crane (*Grus americana*), the candidate Monarch Butterfly (*Danaus plexippus*), and the endangered Pink Mucket (*Lampsilis abrupta*) and Sheepnose (*Plethobasus cyphus*) mussel species are all listed as potentially within the project area. No further coordination was required for the Whooping Crane and Monarch Butterfly as experimental and candidate species, as opposed to threatened or endangered. The IPaC determination does not cover the Tricolored Bat as a "proposed" species and is not yet afforded protection under Section 7. Previously noted AMMs for protected bat species will reduce concerns over potential impacts that may occur to the Tricolored Bat and Gray bat. Since no work will occur in the stream channel, there are no potential impacts to the mussel species will occur.

Additional coordination with USFWS occurred on September 9, 2024 (Appendix C, pages 46 to 49), due to the project area being within the expanded Gray bat region. USFWS responded by requesting additional information regarding the presence or absence of bats. An additional inspection of the bridge will occur, and if any guano is detected samples will be taken and processed to determine which bat species may be utilizing the bridge. This environmental document will be updated after the public involvement process to include these results and any additional coordination that occurs.

Bridge No. P000-39-06876 over Little Crooked Creek and the project's surrounding habitat is conducive for use (i.e., nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Prior to the start of nesting season (May 1), the structure must be inspected for birds or signs of birds. If birds or signs of birds are found during the inspection, avoidance, and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure" Recurring Special Provision (RSP). This firm commitment is included in the *Environmental Commitments* section of this document.

The IDNR-DFW responded on June 8, 2023, stating, "If any bird nests with eggs or young are found on the existing structures, do not work on the bridges from March 15 through September 7. If construction is planned during this time and active nests are present, prior approval from the USDA must be secured by contacting Wildlife Services State Director, USDA Wildlife Services, 901 W. State Street, W. Lafayette, IN 47907; (765) 494-6229; request Form 37 and any other required documentation and follow the USDA's instructions" (Appendix C, pages 13 to 14). This has been added as a firm commitment in the *Environmental Commitments* section of this CE document.

This precludes the need for further consultation on this project as required under Section 7 of the Endangered Species Act, as amended. If new information on endangered species at the site becomes available or if project plans are changed, USFWS will be contacted for consultation.

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### Geological and Mineral Resources

Project located within the Indiana Karst Region  
 Karst features identified within or adjacent to the project area  
 Oil/gas or exploration/abandoned wells identified in the project area

Yes	No
<b>X</b>	
	<b>X</b>
	<b>X</b>

Date Karst Evaluation reviewed by INDOT EWPO (if applicable): \_\_\_\_\_

*Discuss if project is located in the Indiana Karst Region and if any karst features have been identified in the project area (from RFI). Discuss response received from IGWS coordination. Discuss if any mines, oil/gas, or exploration/abandoned wells were identified and if impacts will occur. Include discussion of karst study/report was completed and results. (Karst investigation must comply with the current Protection of Karst Features during Planning and Construction guidance and coordinated and reviewed by INDOT EWPO)*

Based on a desktop review and the Indiana Karst Region map, the project is located in the designated Indiana Karst Region as outlined in the most current *Protection of Karst Features during Project Development and Construction*. According to the topo map of the project area (Appendix B, page 2) and the RFI report (Appendix E, pages 1 to 10), no karst features are identified within or adjacent to the project area.

In the early coordination response dated May 12, 2023, the Indiana Geological and Water Survey (IGWS) did not indicate that karst features exist in the project area (Appendix C, pages 4 to 6). Their response indicated a high liquefaction potential, the project area is within a floodway, a high potential to encounter bedrock resources, and a low potential to encounter sand and gravel resources. Their response also indicated that no active and/or abandoned mineral resource extraction sites (i.e., petroleum exploration wells, underground coal mines, and surface coal mines) have been documented in the area. The IGWS response was communicated to the designer on May 12, 2023. No impacts are expected.

### SECTION C – OTHER RESOURCES

#### Drinking Water Resources

Wellhead Protection Area(s)  
 Source Water Protection Area(s)  
 Water Well(s)  
 Urbanized Area Boundary  
 Public Water System(s)

<u>Presence</u>	<u>Impacts</u>	
	Yes	No
<b>X</b>		<b>X</b>

Is the project located in the St. Joseph Sole Source Aquifer (SSA):  
 If Yes, is the FHWA/EPA SSA MOU Applicable?  
 If Yes, is a Groundwater Assessment Required?

Yes	No
	<b>X</b>

*Check the appropriate boxes and discuss each topic below. Provide details about impacts and summarize resource-specific coordination responses and any mitigation commitments. Reference responses in the Appendix.*

#### Sole Source Aquifer

The project is located in Jefferson County, which is not located within the area of the St. Joseph Sole Source Aquifer, the only legally designated sole source aquifer in the state of Indiana. Therefore, the FHWA/EPA/INDOT Sole Source Aquifer Memorandum of Understanding (MOU) is not applicable to this project, a detailed groundwater assessment is not needed, and no impacts are expected.

#### Wellhead Protection Area and Source Water

The Indiana Department of Environmental Management's Wellhead Proximity Determinator website (<http://www.in.gov/idem>)

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[/cleanwater/pages/wellhead/](#)) was accessed on April 13, 2023, by Egis. This project is located within a Wellhead Protection Area but is not within a Source Water Area. However, there is no known involvement of utility facilities located within the project area. Therefore, no impacts are expected.

**Water Wells**

The IDNR Water Well Record Database website (<https://www.in.gov/dnr/water/3595.htm>) was accessed on April 13, 2023, by Egis. No wells are located near this project. Therefore, no impacts are expected.

**Urban Area Boundary**

Based on a desktop review of IDEM's MS4 Boundaries Map for Indiana website (<https://www.in.gov/idem/cleanwater/ms4s-boundaries-map-for-indiana/>) by Egis on April 13, 2023, this project is not located in an Urban Area Boundary. No impacts are expected.

**Public Water System**

Based on a desktop review, a site visit on May 31, 2023, by Egis, and the aerial map of the project area (Appendix B, page 3), no public water systems were identified. Therefore, no impacts are expected.

<b>Floodplains</b>	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Project located within a regulated floodplain	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Longitudinal encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Transverse encroachment	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Homes located in floodplain within 1000' up/downstream from project	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

If applicable, indicate the Floodplain Level.

Level 1     Level 2     Level 3     Level 4     Level 5

*Use the IDNR Floodway Information Portal to help determine potential impacts. Include floodplain map in appendix. Discuss impacts according to the classification system. If encroachment on a flood plain will occur, coordinate with the Local Flood Plain Administrator during design to insure consistency with the local flood plain planning.*

The Indiana Department of Natural Resources Indiana Floodway Information Portal website (<http://dnrmmaps.dnr.in.gov/appsphp/fdms/>) was accessed on November 3, 2023, by Egis. This project is not located in a regulatory floodplain as determined from approved IDNR floodplain maps (Appendix F, page 6). Therefore, it does not fall within the guidelines for the implementation of 23 CFR 650, 23 CFR 771, and 44 CFR. No impacts are expected.

<b>Farmland</b>	<u>Presence</u>	<u>Impacts</u>	
		Yes	No
Agricultural Lands	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
Prime Farmland (per NRCS)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

Total Points (from Section VII of CPA-106/AD-1006\*) \_\_\_\_\_  
*\*If 160 or greater, see CE Manual for guidance.*

*Discuss existing farmland resources in the project area, impacts that will occur to farmland, and mitigation and minimization measures considered.*

Based on a desktop review, a site visit on May 31, 2023, by Egis, and the aerial map of the project area (Appendix B, page 3), there

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is no land that meets the definition of farmland under the Farmland Protection Policy Act (FPPA) within or adjacent to the project area. The requirements of the FPPA do not apply to this project; therefore, no impacts are expected.

### SECTION D – CULTURAL RESOURCES

**Minor Projects PA** Category(ies) and Type(s) INDOT Approval Date(s) N/A

**Full 106 Effect Finding**  
 No Historic Properties Affected  No Adverse Effect  Adverse Effect

**Eligible and/or Listed Resources Present**  
 NRHP Building/Site/District(s)  Archaeology  NRHP Bridge(s)

Documentation Prepared (mark all that apply)	ESD Approval Date(s)	SHPO Approval Date(s)
APE, Eligibility and Effect Determination	X June 7, 2023	July 5, 2023
800.11 Documentation	X March 20, 2024	April 16, 2024
Historic Properties Report or Short Report	X June 7, 2023	July 5, 2023
Archaeological Records Check and Assessment	X June 7, 2023	July 5, 2023
Archaeological Phase Ia Survey Report		
Archaeological Phase Ic Survey Report		
Other: Historic Bridge Alternative Analysis	X June 7, 2023	July 5, 2023

Memorandum of Agreement (MOA)  **MOA Signature Dates** (List all signatories)

*If the project falls under the MPPA, describe the category(ies) that the project falls under and any approval dates. If the project requires full Section 106, use the headings provided. The completion of the Section 106 process requires that a Legal Notice be published in local newspapers. Please indicate the publication date, name of the paper(s), and the comment period deadline. Include any further Section 106 work which must be completed at a later date, such as mitigation from a MOA or avoidance commitments.*

INDOT, acting on behalf of the FHWA, is required to comply with Section 106 of the National Historic Preservation Act of 1966 as amended (Section 106) and its implementing federal regulation, 36 CFR 800. Section 106 and 36 CFR 800 outline a process that requires INDOT to evaluate the effects of its undertakings on properties that are listed on or eligible for listing on the National Register of Historic Places (NRHP). The following information summarizes the steps INDOT took to identify the cultural resources listed on or eligible for listing on the NRHP and the expected impacts the proposed project would have on those resources. Per the terms of the "Programmatic Agreement Regarding Management and Preservation of Indiana's Historic Bridges" (Historic Bridges PA), the Federal Highway Administration-Indiana Division (FHWA) will satisfy its Section 106 responsibilities involving "Select" and "Non-Select" bridges through the Project Development Process (PDP) of the Historic Bridges PA (Stipulation III). Bridge No. P000-39-06876B (NBI No. 60290) has been classified as a "Select" bridge by the INDOT Historic Bridge Inventory. Thus, the procedures outlined in Stipulation III.A. of the Historic Bridges PA will be followed to fulfill FHWA's Section 106 responsibilities for the bridge. Additionally, because the rehabilitation of the bridge is the preferred alternative, the standard treatment approach described in Attachment B of the Historic Bridges PA (Standard Treatment Approach for Historic Bridges) will be followed.

**Area of Potential Effect (APE):**

Per 36 CFR 800.9(a), the Area of Potential Effect (APE) is defined as the "geographic area or areas within which an undertaking may directly or indirectly cause alterations in the character or use of historic properties if any such properties exist." The project APE

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includes all properties adjacent to the project and those with a proximate viewshed of the project. The APE for this project was created to take into account construction staging and traffic. A map illustrating the APE limits as described is provided in Appendix D, pages 27 to 28.

**Coordination with Consulting Parties:**

Section 106 of the National Historic Preservation Act (NHPA) of 1966, as amended, requires Federal Agencies or their representatives to take into account the effects of their undertakings on historic properties. In accordance with 36 CFR 800.2(c) and the INDOT Cultural Resources Manual, consulting parties were invited to participate in efforts to identify historic properties potentially affected by this undertaking, assess its effects, and seek ways to avoid, minimize, or mitigate any adverse effects on historic properties. On February 21, 2023, the following consulting parties were sent project information and invited to participate in the Section 106 evaluation process (Appendix D, pages 1 to 7. The State Historic Preservation Officer (SHPO) is an automatic consulting party due to their mandatory or designated roles as specified in 36 C.F.R. § 800.2. Other parties that accepted consulting party status are shown in boldface type below.

<b>Consulting Party</b>	<b>Response</b>
<b>Indiana SHPO</b>	<b>March 23, 2023</b>
Clifty Falls State Park	No Response
Cornerstone Society	No Response
Historic Bridge Foundation	No Response
HistoricBridges.org	No Response
Historic Hoosier Bridges	No Response
Historic Madison Foundation, Inc.	No Response
<b>Historic SPANs Task Force</b>	<b>February 21, 2023</b>
Indiana Department of Natural Resources (IDNR)	No Response
Indiana Landmarks-Southern Regional Office	No Response
Jefferson County Auditor	No Response
Jefferson County Commissioners	No Response
Jefferson County Genealogical Society	No Response
Jefferson County Highway Supervisor	No Response
Jefferson County Historian	No Response
Jefferson County Historical Society Museum	No Response
Jefferson County Historic Preservation Council, Inc.	No Response
Jefferson County Public Library – Genealogy and Local History	No Response
Southern Indiana Regional Planning Commission	No Response
Delaware Tribe of Indians	No Response
<b>Eastern Shawnee Tribe of Oklahoma</b>	<b>April 13, 2023</b>
<b>Miami Tribe of Oklahoma</b>	<b>March 2, 2023</b>
Osage Nation	No Response
Peoria Tribe of Indians of Oklahoma	No Response
Pokagon Band of Potawatomi Indians	No Response
<b>Shawnee Tribe</b>	<b>April 20, 2023</b>
United Keetoowah Band of Cherokee Indians	No Response

In response to the early coordination invitation letter on February 21, 2023, the Historic SPANs Task Force responded to the early coordination letter via an email, accepting consulting party status (Appendix D, pages 8 to 11).

In a letter dated March 2, 2023, the Miami Tribe of Oklahoma responded accepting consulting party status. Additionally, they “offer no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site.” If any remains are found, they wish to be notified immediately (Appendix D, page 12).

On March 23, 2023, SHPO staff responded to the early coordination letter. They stated that they will resume identification and

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evaluation procedures for this project once more information about the "Select" bridge has been received by their office. Additionally, they noted that "...we have not identified any currently known archaeological resources listed in or eligible for inclusion in the National Register of Historic Places (NRHP) within the proposed project area" (Appendix D, pages 13 to 15).

On April 13, 2023, the Eastern Shawnee Tribe of Oklahoma responded to the early coordination letter. They stated, "...we find our people occupied these areas historically and/or prehistorically. However, the project proposes No Adverse Effect or endangerment to known sites of interest to the Eastern Shawnee Tribe. Please continue the project as planned. However, should this project inadvertently discover an archeological site or object(s), we request that you immediately contact the Eastern Shawnee Tribe, as well as the appropriate state agencies (within 24 hours). We also ask that all ground disturbing activity stop until the Tribe and State agencies are consulted. Please note that any future changes to this project will require additional consultation" (Appendix D, page 16).

In an email dated April 20, 2023, the Shawnee Tribe responded to the early coordination letter stating, "The Shawnee Tribe's Tribal Historic Preservation Department concurs that no known historic properties will be negatively impacted by this project. However, there is still potential for the discovery of unknown resources." If any archaeological materials are found, they wish to be notified immediately (Appendix D, page 17).

### Archaeology:

An archaeological records review of SHAARD records was completed in January 2023 by CRA. Ground disturbances associated with the project as planned are on existing paved areas, previously disturbed soils, or steeply sloping landforms on the side and underneath the bridge. As such, an archaeological survey was not required for the project. A review of SHAARD records revealed no previously recorded archaeological sites within or near the current project area.

### Historic Properties:

A Historic Property Short Report (HPSR) was completed by CRA (Reynolds, May 23, 2023). CRA recommended that Bridge P000-39-06876B, a "Select" bridge, continues to be eligible for listing in the NRHP under Criterion C for its unique architectural and engineering design. No additional resources were identified as a result of survey efforts. The summary of the HPSR can be found in Appendix D, pages 22 to 37.

An Alternatives Analysis Report was completed by Egis (formerly Beam, Longest, & Neff, LLC) and approved by the INDOT on April 6, 2023. Two alternatives for the proposed bridge project were discussed: Alternative A: The No-Build/Do-Nothing Alternative and Alternative B.1: Rehabilitation of the Existing Bridge for Continued Vehicular Use (Two-Way Option) Meeting Secretary of Interior's Standards (SOIS) for Rehabilitation. Alternative B.1 was recommended as the preliminary preferred alternative by Egis, which proved to be both feasible and prudent, resulting in extending the life of the bridge. A description of the rehabilitation work described in Section 1 of the Alternatives Analysis Report can be found in Appendix D, pages 44 to 46.

On June 7, 2023, CRA distributed the HPSR and Alternatives Analysis Report via email and the INDOT Section 106 Consultation and Outreach Portal Enterprise (IN SCOPE) to consulting parties for a 30-day review and comment period (Appendix D, pages 18 to 21).

The Historic Spans Task Force responded to the HPSR and Alternatives Analysis Report in an email dated June 9, 2023. They had no concerns or additional comments in relation to the rehabilitation treatment of the bridge (Appendix D, pages 48 to 50).

In a letter dated June 30, 2023, the Miami Tribe of Oklahoma responded, stating they "offer no objection to the above-referenced project at this time, as we are not currently aware of existing documentation directly linking a specific Miami cultural or historic site to the project site." If any remains are found, they wish to be notified immediately (Appendix D, page 51).

The SHPO responded to the HPSR and Alternatives Analysis Report in a letter dated July 5, 2023. They agreed with the size of the APE, the archaeological assessment, and the recommendation that Bridge P000-39-06876B continues to be eligible for listing in the NRHP. They also agreed with the results of the HBAA, recognizing that Alternative B.1 is both feasible and prudent (Appendix D, pages 52 to 54).

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### Documentation Findings:

On March 20, 2024, INDOT, on behalf of FHWA, signed the 800.11(e) finding of "No Historic Properties Affected," and it was distributed to consulting parties (Appendix D, pages 61 to 73). In a letter dated April 16, 2024, SHPO concurred with the "No Historic Properties Affected" determination. They also stated they appreciated the 60% plans provided and had no comment on this set of plans but look forward to receiving the final rehabilitation plans. They also requested the bridge be photographically documented, including color digital images providing overviews of the resource, along with detailed shots of the character-defining features. In addition, they would like a photo log corresponding to the photographs, a photo key, and an overview thumbnail sheet. They requested a draft copy of this documentation be submitted on a CD, flash drive, or any other previously approved storage device for their review and approval. Upon approval, the documentation should be provided to a public or not-for-profit organization willing to accept the documentation to be made available to the public (Appendix D, pages 76 to 78). This has been added as a firm commitment in the *Environmental Commitments* section of this CE document.

Per the Historic Bridge PA, the 30%, 60%, and 90% plan sets are required to be submitted to SHPO for review. The 30% plans were sent to consulting parties and SHPO on October 30, 2023. The SHPO responded to the 30% plans in a letter dated November 27, 2023, stating they did not have any comments on the 30% plans (Appendix D, pages 58 to 60). The 60% plans were submitted to SHPO for review with the 800.11(e) documentation on March 20, 2024 (Appendix D, pages 61 to 73). The 90% plan set will be submitted to SHPO prior to Ready-for Contracts (RFC). This has been added as a firm commitment in the *Environmental Commitments* section of this CE document.

With regard to state law, pursuant to Section 11.5(f) of the rule governing dual review, at the conclusion of the SHPO's review of the final plans, it is anticipated that the Division of Historic Preservation and Archaeology's (DHPA) Division Director would issue a letter of clearance exempting this project from obtaining a Certificate of Approval (COA) from the Indiana Historic Preservation Review Board (Review Board) under IC 14-21-1-18. Obtaining a letter of clearance before environmental consultation form (ECF) approval is added as a firm commitment in the *Environmental Commitments* section of this CE document.

### Public Involvement:

In accordance with 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4), the views of the public were sought regarding the effect of the proposed project. An advertisement was placed in *The Madison Courier* newspaper in Madison, Indiana, on March 26, 2024, offering the public an opportunity to submit comments pursuant to 36 CFR 800.2(d), 800.3(e), and 800.6(a)(4). The public comment period closed 30 days later, on April 26, 2024. The text of the public notice and the affidavit of publication appear in Appendix D, pages 74 to 75. No comments were received in response to the public notice.

Per Stipulation III of the Historic Bridges PA, INDOT will hold a public hearing for the project prior to the completion of NEPA studies. All consulting parties will be notified of the public hearing. A legal notice will appear in a local publication contingent upon the release of this document for public involvement. This document will be revised after the public involvement requirements are fulfilled. FHWA's final approval of the environmental document will affirm that all Historic Bridges PA requirements have been fully addressed and will serve to confirm that FHWA has concluded its responsibilities under Section 106.

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<b>SECTION E – SECTION 4(f) RESOURCES/ SECTION 6(f) RESOURCES</b>
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	<u>Presence</u>	<u>Use</u>	
		<u>Yes</u>	<u>No</u>
<b>Parks and Other Recreational Land</b>			
Publicly owned park	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Publicly owned recreation area	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Other (school, state/national forest, bikeway, etc.)	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>Wildlife and Waterfowl Refuges</b>			
National Wildlife Refuge	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
National Natural Landmark	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Wildlife Area	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
State Nature Preserve	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b>Historic Properties</b>			
Site eligible and/or listed on the NRHP	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>
<b><u>Evaluations</u></b>			
<b><u>Prepared</u></b>			
Programmatic Section 4(f)	<input type="checkbox"/>		
“De minimis” Impact	<input type="checkbox"/>		
Individual Section 4(f)	<input type="checkbox"/>		
Any exception included in 23 CFR 774.13	<input checked="" type="checkbox"/>		

Discuss Programmatic Section 4(f) and “de minimis” Section 4(f) impacts in the discussion below. Individual Section 4(f) documentation must be included in the appendix and summarized below. Discuss proposed alternatives that satisfy the requirements of Section 4(f). FHWA has identified various exceptions to the requirement for Section 4(f) approval. Refer to 23 CFR § 774.13 - Exceptions.

Section 4(f) of the U.S. Department of Transportation Act of 1966 prohibits the use of certain public and historic lands for federally funded transportation facilities unless there is no feasible and prudent alternative. The law applies to significant publicly owned parks, recreation areas, wildlife/waterfowl refuges, and NRHP-eligible or listed historic properties regardless of ownership. Lands subject to this law are considered Section 4(f) resources.

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, pages 1 to 10), there is one recreational facility, six trail segments, and two managed lands located within the 0.5-mile search radius. Based on the site visit on May 31, 2023, by Egis, there are two (2) 4(f) resources within or adjacent to the project area. Bridge No. P000-39-06876 and Clifty Falls State Park are within the project area. A detailed discussion of these resources is provided below.

**Bridge No. P000-39-06876**

Bridge No. P000-39-06876B (NBI No. 60290) on Park Road over Little Crooked Creek is listed in the *Indiana Historic Bridge Inventory*, completed in 2010, as eligible for listing in the NRHP. It was also determined to be a “Select” bridge, which are those that are most suitable for preservation and/or are excellent examples of a given type of historic bridge. Per FHWA’s Section 4(f) guidance, a proposed action will “use” a bridge that is on or eligible for inclusion on the NRHP when the action will impair the historic integrity of the bridge either by rehabilitation or demolition. Rehabilitation that does not impair the historic integrity of the bridge as determined by procedures implementing the Nation Historic Preservation Act of 1966, as amended, is not subject to Section 4(f).

Because the preferred alternative is to rehabilitate the existing bridge for continued vehicular use that meets the Secretary of the Interior’s Standards for Rehabilitation (Appendix D, pages 43 to 46), the project will not impair the historical integrity of the bridge. As a result, the rehabilitation of the bridge is not considered a “use” and is not subject to Section 4(f). On June 7, 2023, CRA distributed the HPSR and Alternatives Analysis via email and the INDOT Section 106 Consultation and Outreach Portal Enterprise (IN SCOPE) to consulting parties for a 30-day review and comment period. In a letter dated July 5, 2023, SHPO acknowledged receipt of the HPSR, an archaeological assessment, and Alternatives Analysis Report and stated they agreed with the conclusions of the



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Alternatives Analysis that Alternative B.1: Rehabilitation for Continued Vehicular Use, meeting the Secretary of the Interior's Standards for Rehabilitation is the preferred alternative for this project compared to the No Build Alternative (Appendix D, pages 52 to 54).

The historical integrity of the bridge will be maintained through coordination and consultation with the SHPO during the design phase of the project with the required plan submittals pursuant to the Historic Bridge PA. Therefore, no Section 4(f) use of the bridge will occur. FHWA approval of this CE document confirms that there is no Section 4(f) use of the bridge.

### Clifty Falls State Park

The project area is located within Clifty Falls State Park, which is owned and operated by the IDNR Division of State Parks. Based on its public ownership status as a park, Clifty Falls State Park is afforded protection under Section 4(f). This project qualifies for the Section 4(f) exemption under 23 CFR 774.13(g) as a transportation enhancement activity (TEA). A TEA is one of the specific types of activities set forth by statute at 23 U.S.C. § 101(a)(35). TEAs often involve the enhancement of an activity, feature, or attribute on a property that qualifies as a Section 4(f) property. The project site is not adjacent to public recreational amenities and will not disrupt public recreational and Section 4(f) attributes.

In order to qualify as an exempt TEA project, two conditions must be met:

1. The use of the Section 4(f) property is solely for the purpose of preserving or enhancing an activity, feature, or attribute that qualifies the property for Section 4(f) protection and
2. The official(s) with jurisdiction (OWJ) over the Section 4(f) resource agrees in writing to paragraph (g)(1)

This transportation project meets condition (g)(1) of 23 CFR 774.13(g), as this project serves to enhance Bridge No. P000-39-06876B by correcting deterioration that is present on the structure. The existing bridge carries Park Road over Little Crooked Creek within Clifty Falls State Park. This structure is important to the activities, features, and attributes that qualify Clifty Falls State Park as a Section 4(f) property, as it is the only southern access road to park features and facilities. While the road is closed, all features and facilities will still be accessible from the north park entrance. Though this project is related to surface transportation, its intent is to enhance the park by providing a structurally sound bridge to convey motorists and pedestrians over Little Crooked Creek.

The OWJ of the Section 4(f) property, Clifty Falls State Park, acknowledged and agreed that this project meets condition (g)(1) of 23 CFR 774.13(g) in that its intent is to enhance the park by providing a structurally sound bridge to convey motorists and pedestrians over Little Crooked Creek, and that the structure is important to the activities, features, and attributes that qualify Clifty Falls State Park as a Section 4(f) property (Appendix H, pages 2 to 3).

### Section 6(f) Involvement

#### Presence

#### Use

### Section 6(f) Property

*Discuss Section 6(f) resources present or not present. Discuss if any conversion would occur as a result of this project. If conversion will occur, discuss the conversion approval.*

The U.S. Land and Water Conservation Fund Act of 1965 established the Land and Water Conservation Fund (LWCF), which was created to preserve, develop, and assure accessibility to outdoor recreation resources. Section 6(f) of this Act prohibits conversion of lands purchased with LWCF monies to non-recreation use.

A review of 6(f) properties on the INDOT Environmental Services Division (ESD) website revealed a total of 10 properties in Jefferson County (Appendix H, page 1). The project is located within Clifty Falls State Park, a Section 6(f) property. The IDNR, Division of State Parks responded on May 12, 2023, and stated, "The project as submitted takes place within an LWCF site, Clifty Falls State Park, but does not negatively impact outdoor recreation as it is repairing the existing structure. Please coordinate tree clearing with property management. Any tree clearing for temporary use must be replanted" (Appendix C, pages 10 to 12). Anticipated tree clearing and replanting will be coordinated with Clifty Falls Property Management. This has been added as a firm commitment in the Environmental Commitments section of this CE document. No permanent or temporary ROW will be acquired for

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this project, and this project will not convert land to non-recreation use. Therefore, no impacts are expected.

In the early coordination response dated June 8, 2023 the IDNR-DFW had several recommendations to avoid or minimize impacts on terrestrial habitat. These recommendations included developing a mitigation plan for any unavoidable habitat impacts as well as recommendations for installing bank stabilization measures, revegetating all bare and disturbed areas, and installing appropriate erosion and sediment control measures. In their response letter, the IDNR-DFW also indicated that if impacts to non-wetland riparian forest are less than one acre, the non-wetland forest removed in a rural setting should be replaced at a 1:1 ratio based on area of impact. They also stated seeding and stabilizing disturbed areas is required regardless of impact amount or location (Appendix C, pages 13 to 14). Any disturbed areas will be restored following the completion of construction activities. This is included as a firm commitment in the Environmental Commitments section of this CE document.

### SECTION F – Air Quality

#### STIP/TIP and Conformity Status of the Project

Is the project in the most current STIP/TIP? **Yes**

**No**

Is the project located in an MPO Area?

Is the project in an air quality non-attainment or maintenance area?

If Yes, then:

Is the project in the most current MPO TIP?

Is the project exempt from conformity?

If No, then:

Is the project in the Transportation Plan (TP)?

Is a hot spot analysis required (CO/PM)?

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Location in STIP:

Fiscal Year (FY) Statewide Transportation Improvement Program (STIP) 2024-2028 Page 139

Name of MPO (if applicable):

N/A

Location in TIP (if applicable):

N/A

Level of MSAT Analysis required?

Level 1a  Level 1b  Level 2  Level 3  Level 4  Level 5

*Describe if the project is listed in the STIP and if it is in a TIP. Describe the attainment status of the county(ies) where the project is located. Indicate whether the project is exempt from a conformity determination. If the project is not exempt, include information about the TP and TIP. Describe if a hot spot analysis is required and the MSAT Level.*

This project is included in the FY 2024-2028 Statewide Transportation Improvement Program (STIP) (Appendix G, page 1).

#### Attainment Status

This project is located in Jefferson County, which is currently in attainment for all criteria pollutants according to the Environmental Protection Agency (EPA) Green Book website ([Nonattainment Areas for Criteria Pollutants \(Green Book\) | US EPA](#)). Therefore, the conformity procedures of 40 Code of Federal Regulations (CFR) Part 93 do not apply.

#### MSAT

This project qualifies as a categorical exclusion (Group 1) under 23 CFR 771.1171 or exempt under the Clean Air Act conformity rule under 40 CFR 93.126. As such, a Mobile Source Air Toxics analysis is not required.

If necessary, the INDOT Project Manager will ensure the STIP will be updated prior to the Ready-for-Contract (RFC) date to reflect increasing costs. This has been added as a firm commitment in the Environmental Commitments section of this document.

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### SECTION G - NOISE

**Noise**

**Yes      No**

Is a noise analysis required in accordance with FHWA regulations and INDOT's traffic noise policy?       

Date Noise Analysis was approved/technically sufficient by INDOT ESD: \_\_\_\_\_

*Describe if the project is a Type I or Type III project. If it is a Type I project, describe the studies completed to date and if noise impacts were identified. If noise impacts were identified, describe if abatement is feasible and reasonable and include a statement of likelihood.*

This project is a Type III project. In accordance with 23 CFR 772 and the current *Indiana Department of Transportation Traffic Noise Analysis Procedure*, this action does not require a formal noise analysis.

### SECTION H – COMMUNITY IMPACTS

**Regional, Community & Neighborhood Factors**

- Will the proposed action comply with the local/regional development patterns for the area?
- Will the proposed action result in substantial impacts to community cohesion?
- Will the proposed action result in substantial impacts to local tax base or property values?
- Will construction activities impact community events (festivals, fairs, etc.)?
- Does the community have an approved transition plan?  
If No, are steps being made to advance the community's transition plan?
- Does the project comply with the transition plan? (explain in the discussion below)

Yes	No
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>
<input checked="" type="checkbox"/>	<input type="checkbox"/>

*Discuss how the project complies with the area's local/regional development patterns; whether the project will impact community cohesion; and impact community events. Discuss how the project conforms with the ADA Transition Plan.*

The proposed project will benefit the community by rehabilitating the deteriorating bridge on Park Road over Little Crooked Creek and maintaining safe access to Clifty Falls State Park. Construction will occur between March and April in Spring 2026 in order to have the least amount of inconvenience to park visitors. The project is not anticipated to impact the tax base for the area or result in a division of the community. While there may be temporary inconveniences associated with construction, such as possible construction noise and fugitive dust, there are no long-term, foreseeable economic impacts from the project. According to a review of the website Find Art Shows, Craft Shows, and Festivals Near You (<https://www.fairsandfestivals.net/>), an online resource for local fairs and festivals, there are no scheduled festivals or other public events that will be impacted as a result of the project.

As required by the Americans with Disabilities Act (ADA), Madison, Indiana, has developed an ADA Transition Plan dated 2013 ([https://www.madison-in.gov/egov/documents/1613331581\\_4626.pdf](https://www.madison-in.gov/egov/documents/1613331581_4626.pdf)). The project will be designed in accordance with this plan and all applicable ADA requirements.

Indirect impacts are effects caused by the action and are later in time or farther removed in distance but are still reasonably foreseeable. Indirect effects may include growth-inducing effects and other effects related to induced changes in the pattern of land use, population density, or growth rate. Cumulative impacts affect the environment, which result from the incremental impact of the action when added to other past, present, and reasonably foreseeable future actions, regardless of what agency or person undertakes such actions. This project will not contribute to or stimulate an increase in commercial or residential development in the project area or cause additional impacts in the foreseeable future. This project is not anticipated to result in substantial impacts to community cohesion because it will not change access to properties within the area or divide existing communities. The project is not expected to impact the surrounding community or cause economic impacts on the surrounding area. No indirect or cumulative

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impacts are expected as a result of the project.

**Public Facilities and Services**

*Discuss what public facilities and services are present in the project area and impacts (such as MOT) that will occur to them. Include how the impacts have been minimized and what coordination has occurred. Some examples of public facilities and services include health facilities, educational facilities, public and private utilities, emergency services, religious institutions, airports, transportation, or public pedestrian and bicycle facilities.*

Based on a desktop review, the aerial map of the project area (Appendix B, page 3), and the RFI report (Appendix E, pages 1 to 10), there is one recreational facility, six trails, and two managed lands located within 0.5 mile of the project area. The project area is within one managed land, Clifty Falls State Park. That number was confirmed by the site visit on May 31, 2023, by Egis.

The project involves the rehabilitation of Bridge No. P000-39-06876, which conveys Park Road, the southern entrance into Clifty Falls State Park, over Little Crooked Creek. Due to the nature of the work, the bridge will be closed for approximately 60 days, requiring park visitors to detour to the north access point into the park. However, the detour will be temporary and is needed in order to address the purpose and need of the project.

IDNR, Division of State Parks responded on May 12, 2023, and stated, "The project as submitted takes place within a Land and Water Conservation Fund (LWCF) site, Clifty Falls State Park, but does not negatively impact outdoor recreation as it is repairing the existing structure" (Appendix C, pages 11 to 12).

It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access.

**Environmental Justice (EJ)** (Presidential EO 12898)

During the development of the project, were EJ issues identified?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Does the project require an EJ analysis?

If YES, then:

Are any EJ populations located within the project area?

<input type="checkbox"/>	<input type="checkbox"/>
<input type="checkbox"/>	<input type="checkbox"/>

Will the project result in adversely high and disproportionate impacts to EJ populations?

*Indicate if EJ issues were identified during project development. If an EJ analysis was not required, discuss why. If an EJ analysis was required, describe how the EJ population was identified. Include if the project has a disproportionately high or adverse effect on EJ populations and explain your reasoning. If yes, describe actions to avoid, minimize, and mitigate these effects.*

Under FHWA Order 6640.23A, FHWA and the project sponsor, as a recipient of funding from FHWA, are responsible to ensure that their programs, policies, and activities do not have a disproportionately high and adverse effect on minority or low-income populations. This project will have no relocations and require no additional permanent right-of-way; therefore, an EJ analysis is not required per the current INDOT Categorical Exclusion Manual.

**Relocation of People, Businesses or Farms**

Will the proposed action result in the relocation of people, businesses, or farms?

Yes	No
<input type="checkbox"/>	<input checked="" type="checkbox"/>
<input type="checkbox"/>	<input checked="" type="checkbox"/>

Is a BIS or CSRS required?

Number of relocations:      Residences:   0        Businesses:   0        Farms:   0        Other:   0  

*Discuss any relocations that will occur due to the project. If a BIS or CSRS is required, discuss the results in the discussion below.*

No relocations of people, businesses, or farms will take place as a result of this project.

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## SECTION I – HAZARDOUS MATERIALS & REGULATED SUBSTANCES

**Hazardous Materials & Regulated Substances** (Mark all that apply)

- Red Flag Investigation (RFI)
- Phase I Environmental Site Assessment (Phase I ESA)
- Phase II Environmental Site Assessment (Phase II ESA)
- Design/Specifications for Remediation required?

**Documentation**

X

Date RFI concurrence by INDOT SAM (if applicable): March 20, 2023

*Include a summary of the potential hazardous material concerns found during review. Discuss in depth sites found within, directly adjacent to, or ones that could impact the project area. Refer to current INDOT SAM guidance. If additional documentation (special provisions, pay quantities, etc.) will be needed, include in discussion. Include applicable commitments.*

Based on a review of Geographic Information Systems (GIS) and available public records, the RFI was completed on March 20, 2023, and INDOT Site Assessment and Management (SAM) provided their concurrence on March 20, 2023 (Appendix E, pages 1 to 10). One Underground Storage Tank (UST) site, one National Pollutant Discharge Elimination System (NPDES) site, and one NPDES Pipe Location are located within 0.5 mile of the project area. None of the hazmat sites identified will impact the project. Further investigation for hazardous material concerns is not required at this time.

## **Part IV – Permits and Commitments**

### PERMITS CHECKLIST

**Permits** (mark all that apply)

**Likely Required**

**Army Corps of Engineers (404/Section 10 Permit)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Other

**IN Department of Environmental Management (401/Rule 5)**

- Nationwide Permit (NWP)
- Regional General Permit (RGP)
- Individual Permit (IP)
- Isolated Wetlands
- Rule 5
- Other

**IN Department of Natural Resources**

- Construction in a Floodway
- Navigable Waterway Permit
- Other

**Mitigation Required**

- US Coast Guard Section 9 Bridge Permit**
- Others (Please discuss in the discussion below)**

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List the permits likely required for the project and summarize why the permits are needed, including permits designated as "Other."

No permits are anticipated to be needed for this project.

Applicable recommendations provided by resource agencies are included in the *Environmental Commitments* section of this document. If permits are found to be necessary, the conditions of the permit will be requirements of the project and will supersede these recommendations.

It is the responsibility of the project sponsor to identify and obtain all required permits.

### ENVIRONMENTAL COMMITMENTS

List all commitments and include the name of agency/organization requesting/requiring the commitment(s). Listed commitments should be numbered.

#### Firm:

1. If the scope of work or permanent or temporary ROW amounts change, the INDOT Environmental Services Division (ESD) and the INDOT District Environmental Section will be contacted immediately. (INDOT ESD and INDOT Seymour District)
2. It is the responsibility of the project sponsor to notify school corporations and emergency services at least two weeks prior to any construction that would block or limit access. (INDOT ESD)
3. A USFWS Bridge/Structure Assessment shall take place no earlier than two (2) years prior to the start of construction. If construction will begin after May 31, 2025, an inspection of the structure by a qualified individual must be performed. Inspection of the structure should check for presence of bats/bat indicators and/or presence of birds. The results of the inspection must indicate no signs of bats or birds. If signs of bats or birds are documented during this inspection, the INDOT Crawfordsville District Environmental Manager must be contacted immediately. (INDOT ESD)
4. Bridge No. P000-39-06876 over Little Crooked Creek and the project's surrounding habitat is conducive for use (i.e., nests) by a bird species protected under the Migratory Bird Treaty Act (MBTA). Avoidance and minimization measures must be implemented prior to the start of and during the nesting season. Nests without eggs or young should be removed prior to construction during the non-nesting season (September 8 – April 30) and during the nesting season if no eggs or young are present. Nests with eggs or young cannot be removed or disturbed during the nesting season (May 1 – September 7). Nests with eggs or young should be screened or buffered from active construction. Details of the required procedures are outlined in the "Potential Migratory Bird on Structure" Recurring Special Provision (RSP). (INDOT EWSP0)
5. If necessary, the INDOT Project Manager will ensure the STIP will be updated prior to the Ready-for-Contract (RFC) date. (INDOT ESD)
6. Hibernacula AMM 1: For projects located within karst areas, on-site personnel will use best management practices, secondary containment measures, or other standard spill prevention and countermeasures to avoid impacts to possible hibernacula. Where practicable, a 300-foot buffer will be employed to separate fueling areas and other major containment risk activities from caves, sinkholes, losing streams, and springs in karst topography. (USFWS)
7. General AMM 1: Ensure all operators, employees, and contractors working in areas of known or presumed bat habitat are aware of all FHWA/FRA/FTA (Transportation Agencies) environmental commitments, including all applicable AMMs. (USFWS)
8. Lighting AMM 1: Direct temporary lighting away from suitable habitat during the active season. (USFWS)
9. Tree Removal AMM 1: Modify all phases/aspects of the project (e.g., temporary work areas, alignments) to avoid tree removal. (USFWS)
10. Tree Removal AMM 2: Apply time-of-year restrictions for tree removal when bats are not likely to be present (April 1 to September 30), or limit tree removal to 10 or fewer trees per project at any time of year within 100 feet of existing road/rail surface and outside of documented roosting/foraging habitat or travel corridors; visual emergence survey must be conducted with no bats observed. Do not cut any trees suitable for Indiana bat or Northern Long-eared bat roosting (greater than 5 inches dbh, living or dead, with loose hanging bark, or with cracks, crevices, or cavities). (USFWS and IDNR-DFW)
11. Tree Removal AMM 3: Ensure tree removal is limited to the specified in project plans and ensure that contractors understand clearing limits and how they are marked in the field (e.g., install bright colored flagging/fencing prior to any tree clearing to ensure contractors stay within clearing limits). (USFWS)
12. Tree Removal AMM 4: Do not remove documented Indiana bat or NLEB roosts that are still suitable for roosting, or trees within 0.25 miles of roosts, or documented foraging habitat any time of year. (USFWS)

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13. Anticipated tree clearing and replanting will be coordinated with Clifty Falls Property Management (Cory Duff, Assistant Property Manager, 812-273-8885, CDuff1@dnr.IN.gov). (IDNR, Division of State Parks)
14. Any disturbed areas will be restored following the completion of construction activities. (IDNR- Division of State Parks)
15. SHPO has requested that the bridge be photographically documented, including color digital images that provide overviews of the resource, along with detailed shots of the character-defining features. In addition, they would like a photo log that corresponds to the photographs, a photo key, and an overview thumbnail sheet. A draft copy of this documentation should be submitted on a CD, flash drive, or any other previously approved storage device for their review and approval. Upon approval, the documentation should be provided to a public or not-for-profit organization willing to accept the documentation, to be made available to the public. SHPO should be informed of which not-for-profit accepts the documentation. This should be completed prior to ECF approval. (SHPO)
16. The 90% plan sets need to be approved by SHPO prior to ECF approval. (SHPO)
17. In order to comply with IC 14-21-1-18, a COA must be obtained from the Review Board, or a director's letter of clearance must be obtained from the Indiana DHPA, exempting the project from obtaining a COA. This must be completed before ECF approval. (SHPO)
18. If any bird nests with eggs or young are found on the existing structures, do not work on the bridges from March 15 through September 7. If construction is planned during this time and active nests are present, prior approval from the USDA must be secured by contacting Wildlife Services State Director, USDA Wildlife Services, 901 W. State Street, W. Lafayette, IN 47907; (765) 494-6229; request Form 37 and any other required documentation and follow the USDA's instructions. (IDNR-DFW)

**For Further Consideration:**

19. Impacts to non-wetland forest of one (1) acre or more in a rural or urban area should be mitigated at a minimum 2:1 ratio based on area of impact. Impacts to non-wetland forest under one (1) acre but at least 0.10 acre in a rural or urban area should be mitigated at a minimum 1:1 ratio based on area of impact. Impacts under 0.10 acre in a rural area typically do not require mitigation or additional plantings beyond seeding and stabilizing disturbed areas, though there are exceptions for high-quality habitat sites. Impacts under 0.10 acre in an urban area should be mitigated by replacing trees that are 10" diameter-at-breast height (dbh) or greater by planting five trees, 1" to 2" in dbh, for each tree which is removed that is 10" dbh or greater. (IDNR-DFW)
20. Limit impacts to wildlife passage and movement from any riprap placed against the piers by only placing the minimum width of riprap protection needed and leaving most of the slope as a natural, riprap-free surface. (IDNR-DFW)

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# **Appendix A:**

## **INDOT Supporting Documentation**

## Categorical Exclusion Level Thresholds

	PCE	Level 1	Level 2	Level 3	Level 4 <sup>1</sup>
<b>Section 106</b>	Falls within guidelines of Minor Projects PA	“No Historic Properties Affected”	“No Adverse Effect”	-	“Adverse Effect” Or Historic Bridge involvement <sup>2</sup>
<b>Stream Impacts<sup>3</sup></b>	No construction in waterways or water bodies	< 300 linear feet of stream impacts	≥ 300 linear feet of stream impacts	-	USACE Individual 404 Permit <sup>4</sup>
<b>Wetland Impacts<sup>3</sup></b>	No adverse impacts to wetlands	< 0.1 acre	-	< 1.0 acre	≥ 1.0 acre
<b>Right-of-way<sup>5</sup></b>	Property acquisition for preservation only or none	< 0.5 acre	≥ 0.5 acre	-	-
<b>Relocations<sup>6</sup></b>	None	-	-	< 5	≥ 5
<b>Threatened/Endangered Species (Species Specific Programmatic for Indiana bat &amp; northern long eared bat)*</b>	“No Effect”, “Not likely to Adversely Affect” (With select AMMs <sup>7</sup> )	“Not likely to Adversely Affect” (With any AMMs or commitments)	-	“Likely to Adversely Affect”	Project does not fall under Species Specific Programmatic <sup>8</sup>
<b>Threatened/Endangered Species (Any other species)*</b>	Falls within guidelines of USFWS 2013 Interim Policy or “No Effect”	“Not likely to Adversely Affect”	-	-	“Likely to Adversely Affect”
<b>Environmental Justice</b>	No disproportionately high and adverse impacts	-	-	-	Potential <sup>9</sup>
<b>Sole Source Aquifer</b>	No Detailed Groundwater Assessment	-	-	-	Detailed Groundwater Assessment
<b>Floodplain</b>	No Substantial Impacts	-	-	-	Substantial Impacts
<b>Section 4(f) Impacts</b>	None	-	-	-	Any <sup>10</sup>
<b>Section 6(f) Impacts</b>	None	-	-	-	Any
<b>Permanent Traffic Alteration</b>	None	-	-	-	Any
<b>Noise Analysis Required</b>	No	-	-	-	Yes
<b>Air Quality Analysis Required</b>	No	-	-	-	Yes <sup>11</sup>
<b>Approval Level</b>					
<ul style="list-style-type: none"> <li>• District Env. (DE)</li> <li>• Env. Serv. Div. (ESD)</li> <li>• FHWA</li> </ul>	Concurrence by DE or ESD	DE or ESD	DE or ESD	DE and/or ESD	DE and/or ESD; and FHWA

<sup>1</sup> Coordinate with INDOT Environmental Services Division. INDOT will then coordinate with the appropriate FHWA Environmental Specialist.

<sup>2</sup> Any involvement with a bridge processed under the Historic Bridge Programmatic Agreement.

<sup>3</sup> Total permanent impacts to streams (linear feet) and wetlands (acres).

<sup>4</sup> US Army Corps of Engineers Individual 404 Permit

<sup>5</sup> Total permanent and temporary right-of-way. This does not include reacquisition of existing apparent right-of-way.

<sup>6</sup> If any relocations are within an area with a known or suspected Environmental Justice (EJ) or disadvantaged population, or has greater than 5 relocations, a conversation with FHWA, through INDOT ESD, is needed to confirm NEPA classification and outreach plan for the project.

<sup>7</sup> Avoidance and Mitigation Measures (AMMs) determined by the IPAC determination key to be required that are not tree AMMs, bridge AMMs, or structure AMMs.

<sup>8</sup> Projects that do not fall under a Species Specific Programmatic and results in a “Likely to Adversely Affect”. Other findings can be processed as a lower-level CE.

<sup>9</sup> Potential for causing a disproportionately high and adverse impact.

<sup>10</sup> Section 4(f) use resulting in an Individual, Programmatic, or *de minimis* evaluation. The only exception is a *de minimis* evaluation for historic properties (Effective January 2, 2020). If a historic property *de minimis* and no other use, mark the *None* column.

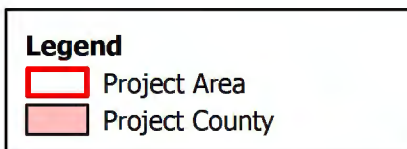
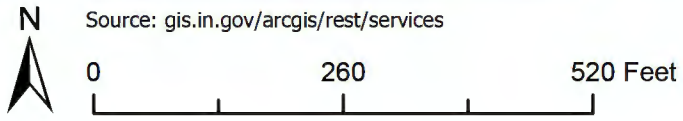
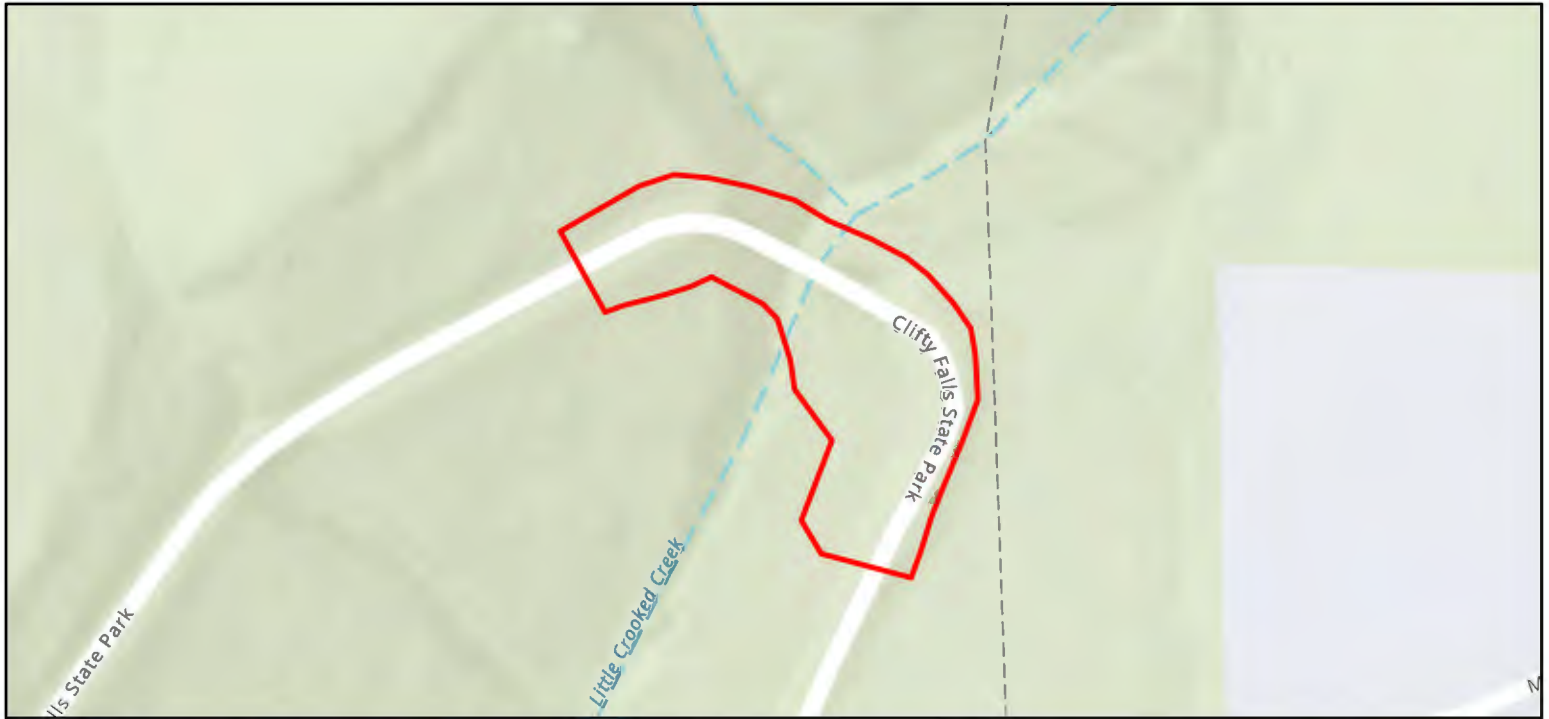
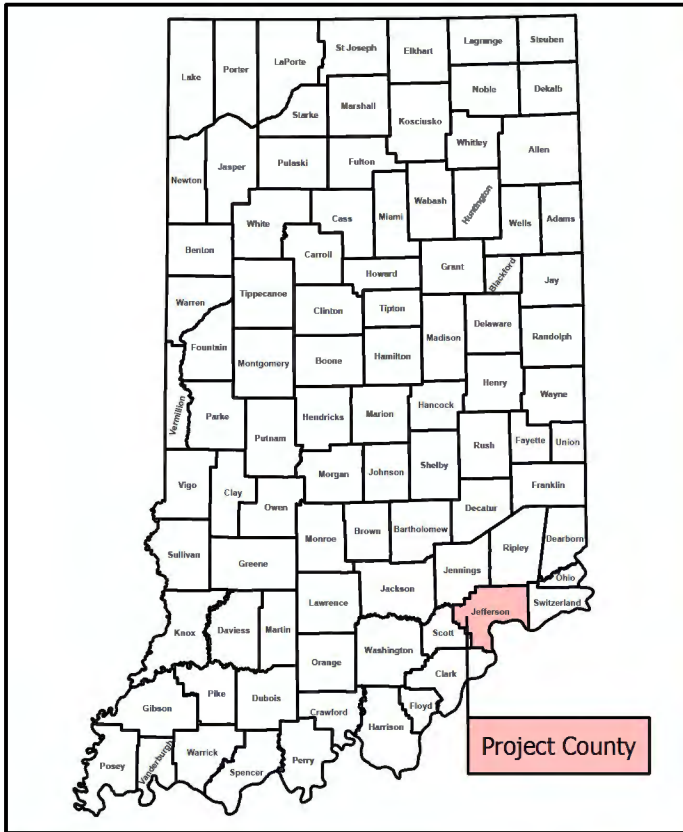
<sup>11</sup> Hot Spot Analysis and/or MSAT Quantitative Emission Analysis.

\* Includes the threatened/endangered species critical habitat

Note: Substantial public or agency controversy may require a higher-level NEPA document.

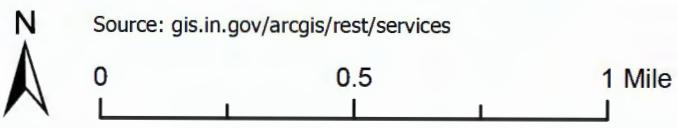
# **Appendix B:**

# **Graphics**



**Figure 1: Project Location Map**  
 Bridge Project  
 Park Road over Little Crooked Creek  
 Jefferson County, Indiana  
 Des 2200147  
 Author: Raquel Walker





**Legend**

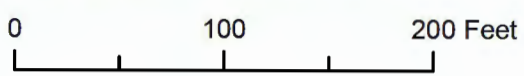
Project Area

**Figure 2: USGS Topo Map**  
 Bridge Project  
 Park Road Over Little Crooked Creek  
 Jefferson County, Indiana  
 Des. No. 2200147  
 Author: Raquel Walker






Source: [gis.in.gov/arcgis/rest/services](https://gis.in.gov/arcgis/rest/services)



**Legend**

 Project Area

**Figure 3: Aerial Map**  
Bridge Project  
Park Road Over Little Crooked Creek  
Jefferson County, Indiana  
Des. No. 2200147  
Author: Raquel Walker







**Photo 1: Facing southwest toward Park Road west of the bridge.**



**Photo 2: Facing northeast toward Park Road west of the bridge.**



**Photo 3: Facing northeast on Park Road west of the bridge.**



**Photo 4: Facing east toward the bridge from Park Road.**





Photo 5: Facing east toward the bridge just south of Park Road.



Photo 6: Facing southeast toward Park Road from the northwest side of bridge.



Photo 7: Facing southeast from the center of Park Road the bridge.

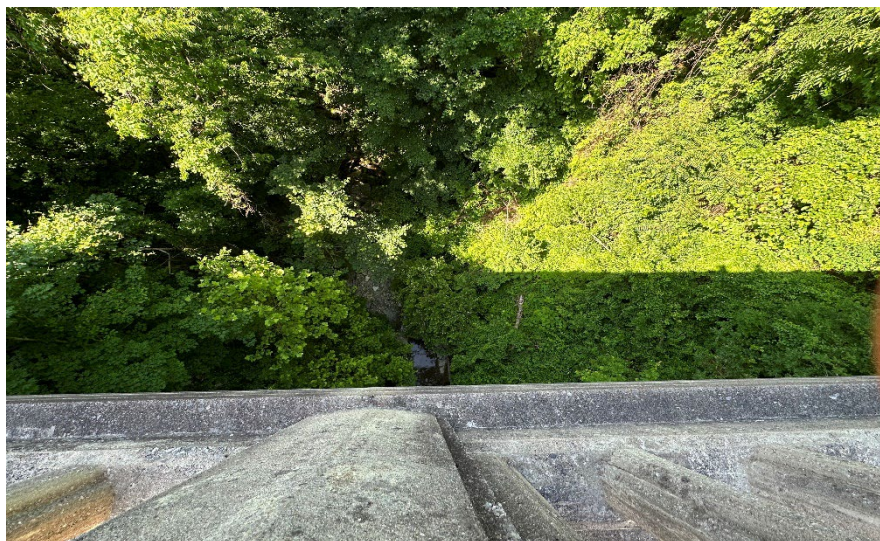


Photo 8: Facing southwest, downstream, looking down at Little Crooked Creek from the center of the bridge.





Photo 9: Facing southwest looking at the steep slopes from the center of the bridge.



Photo 10: Facing north from the center of the bridge looking at the steep slopes and an erosional feature outside of the project area.



Photo 11: Facing northeast, upstream, looking at the rocky substrate of Little Crooked Creek from the center of the bridge.

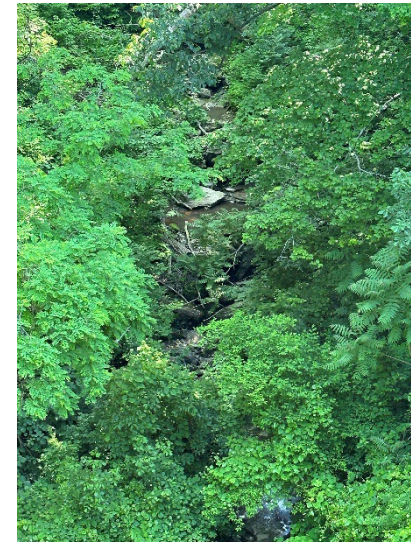


Photo 12: Facing northeast, upstream, looking at Little Crooked Creek from beneath the bridge.



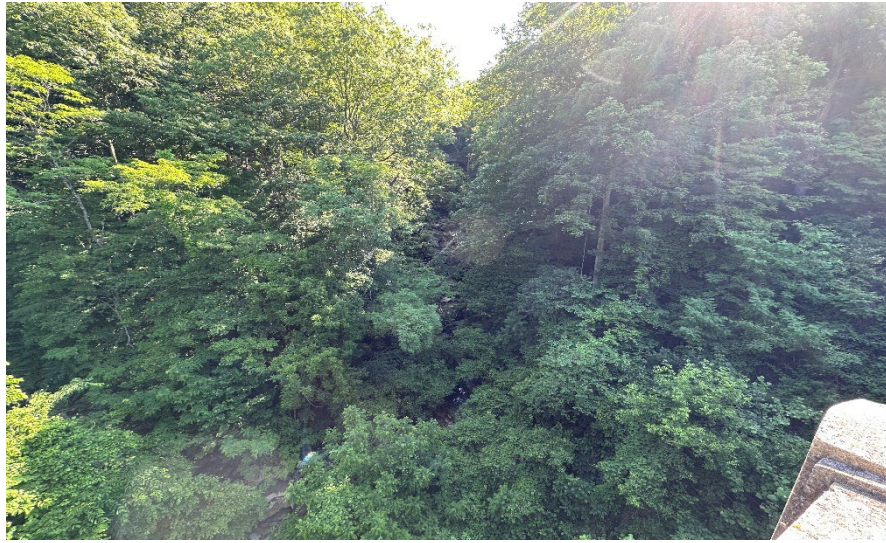


Photo 13: Facing northeast looking upstream towards Little Crooked Creek, which travels up the slope from the center of the bridge on Park Road.



Photo 14: Facing east looking at a drain and a concrete roadside ditch (RSD) on the northeast side of bridge.



Photo 15: Facing northwest toward the bridge from Park Road.



Photo 16: Facing east looking at a drainage feature on the north side of Park Road, east of the bridge.





Photo 17: Facing northwest toward the bridge from the center of Park Road.



Photo 18: Facing north toward a grassy area on the southeast side of the bridge.



Photo 19: Facing south looking at a grassy area on the southeast side of the bridge.

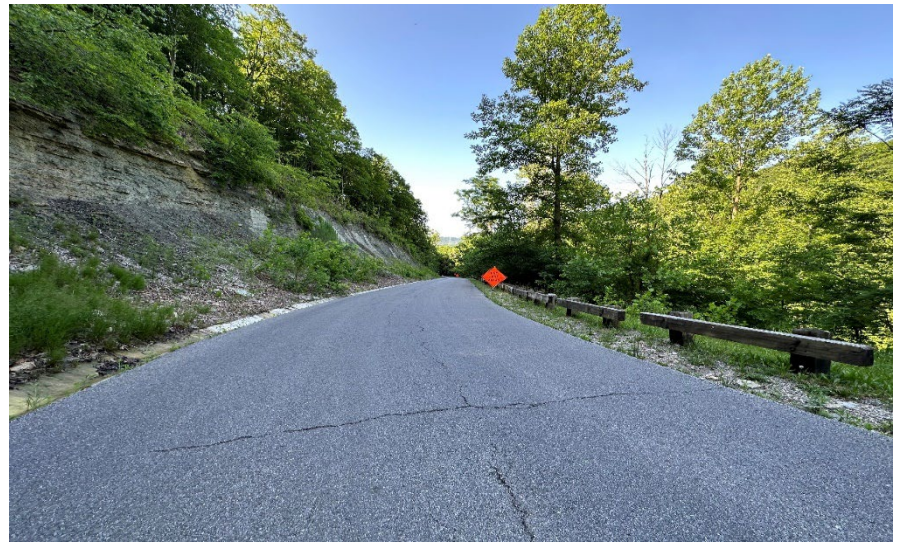


Photo 20: Facing south toward the RSD and Park Road east of the bridge.





Photo 21: Facing south toward riprap lined slopes southeast of the bridge.



Photo 22: Facing north looking at the steep riprap lined slopes southeast of the bridge.



Photo 23: Facing northwest looking up at the bridge beneath Park Road.



Photo 24: Facing northwest from beneath the bridge.





**Photo 25: Facing south looking at the bridge abutment on the north side of the bridge.**



**Photo 26: Facing southwest looking down at animal tracks observed below the bridge.**



**Photo 27: Facing northwest looking down at ground vegetation and erosion on the northeast side below the bridge.**



**Photo 28: Facing southwest from beneath the bridge.**





**Photo 29: Facing northeast, looking down at Little Crooked Creek from the center of the bridge.**



**Photo 30: Facing northwest, looking down towards Little Crooked Creek from the southeast corner of the bridge.**



PROJECT	DESIGNATION
2200147	2200147
CONTRACT	BRIDGE FILE
B-44217	P000-39-06876 C

STRUCTURE INFORMATION				
STRUCTURE	TYPE	SPAN AND SKEW	OVER	STATION
P000-39-06876 C	OPEN SPANDREL ARCH	3 SPANS: 21'-2 5/8", 119'-8 1/2", 20'-1 1/8" SKEW: 0°	LITTLE CROOKED CREEK	Q STRUCTURE STA.100+00 "A"

# INDIANA DEPARTMENT OF TRANSPORTATION



## BRIDGE PREVENTIVE MAINTENANCE PLANS

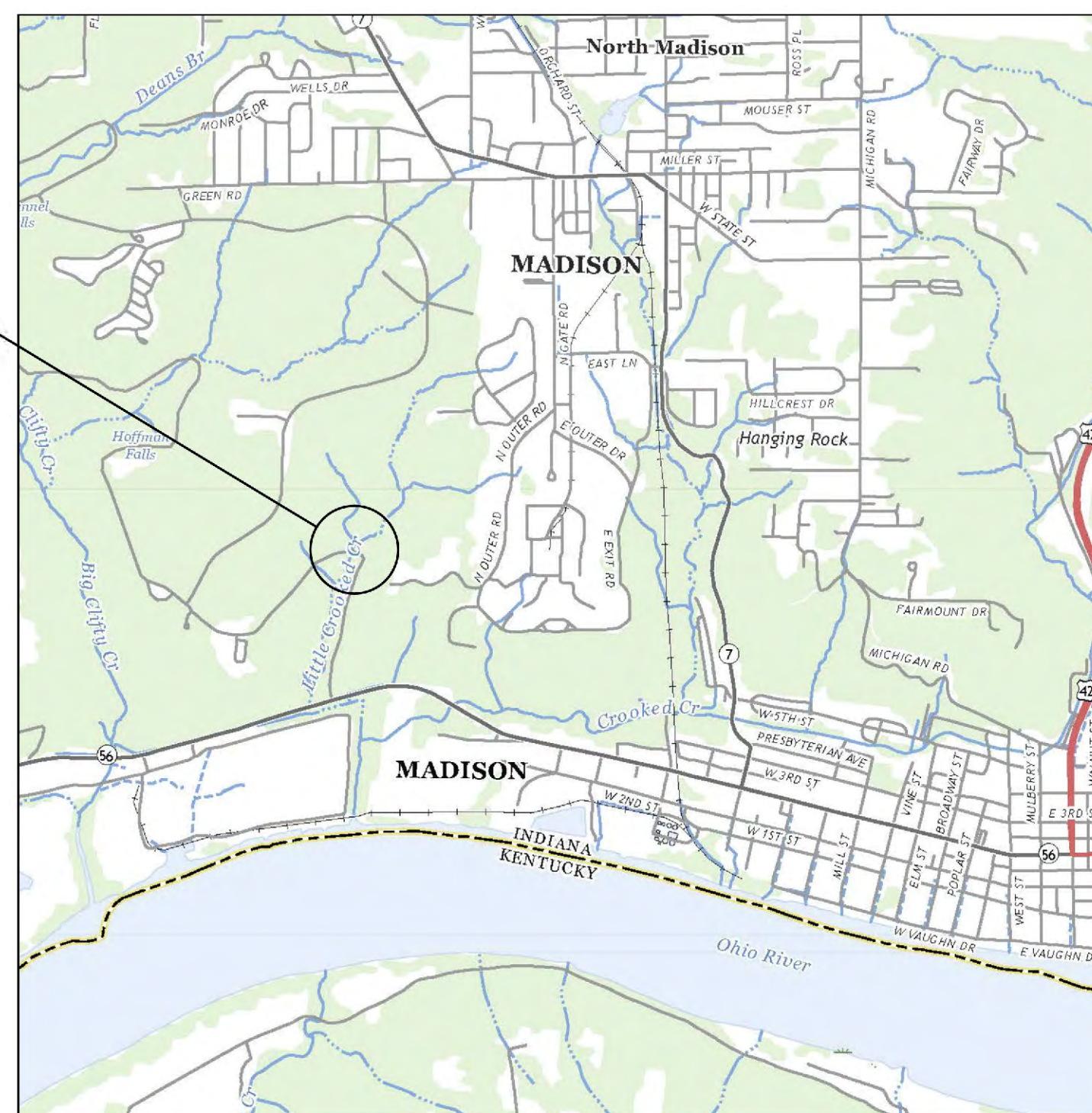
FOR SPANS OVER 20 FEET  
ROUTE: PARK ROAD (CANYON ROAD)

PROJECT NO. 2200147 P.E.  
R/W  
2200147 CONST.

NO ADDITIONAL RIGHT-OF-WAY  
REQUIRED FOR THIS PROJECT

Bridge Deck Overlay and Repair on Park Road (Canyon Road) over Little Crooked Creek  
Located 0.4 Miles North of Clifty Hollow Road (Old SR 56) within Clifty Falls State Park  
Section 33, T-4-N, R-10-E, Madison Township, Jefferson County, Indiana

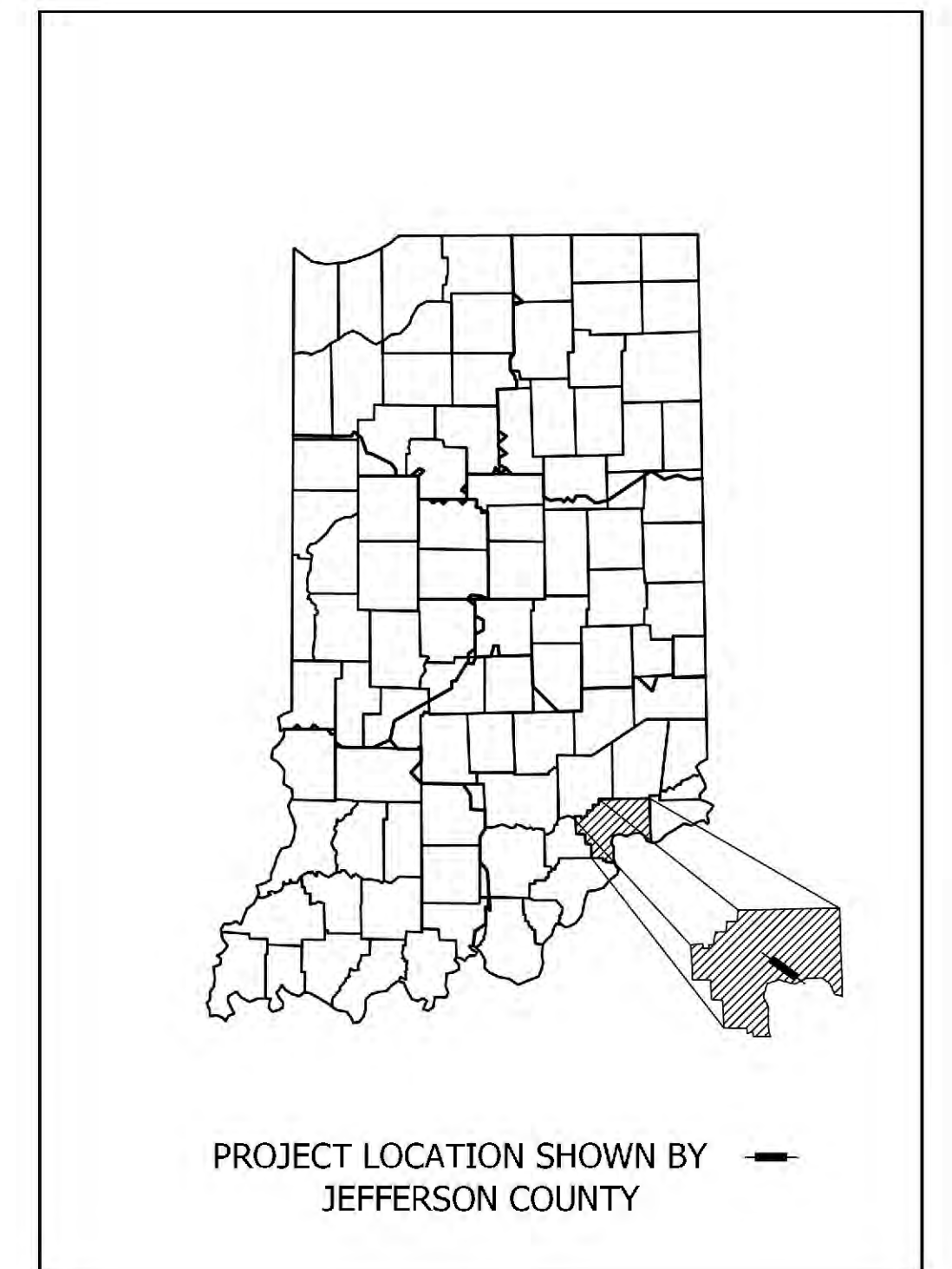
**PROJECT LOCATION**  
Begin Project-Sta.98+84.93 "A"  
End Project-Sta.101+12.95 "A"



LOCATION MAP  
SCALE: 1" = 2000'

TRAFFIC DATA		
A.A.D.T.	(2025)	440 V.P.D.
A.A.D.T.	(2045)	521 V.P.D.
D.H.V	(2045)	65 V.P.H.
DIRECTIONAL DISTRIBUTION		50 %
TRUCKS		12.5 % A.A.D.T. 20 % D.H.V.

DESIGN DATA	
DESIGN SPEED	15 M.P.H.
PROJECT DESIGN CRITERIA	RECREATIONAL ROAD
FUNCTIONAL CLASSIFICATION	PRIMARY ACCESS
RURAL/URBAN	RURAL
TERRAIN	ROLLING
ACCESS CONTROL	NONE



LATITUDE: 38°44'51.79" N LONGITUDE: 85°24'41.83" W

BRIDGE LENGTH:	0.033	MI.
ROADWAY LENGTH:	0.010	MI.
TOTAL LENGTH:	0.043	MI.
MAX. GRADE:	6.80	%

HUC 12: 051401010302  
HUC 14: 05140101040040

INDIANA DEPARTMENT OF TRANSPORTATION  
STANDARD SPECIFICATIONS DATED 2024  
TO BE USED WITH THESE PLANS.



PLANS PREPARED BY: BEAM, LONGEST & NEFF, LLC (317)849-5832 PHONE NUMBER

CERTIFIED BY: \_\_\_\_\_ DATE

APPROVED FOR LETTING: \_\_\_\_\_ DATE  
INDIANA DEPARTMENT OF TRANSPORTATION

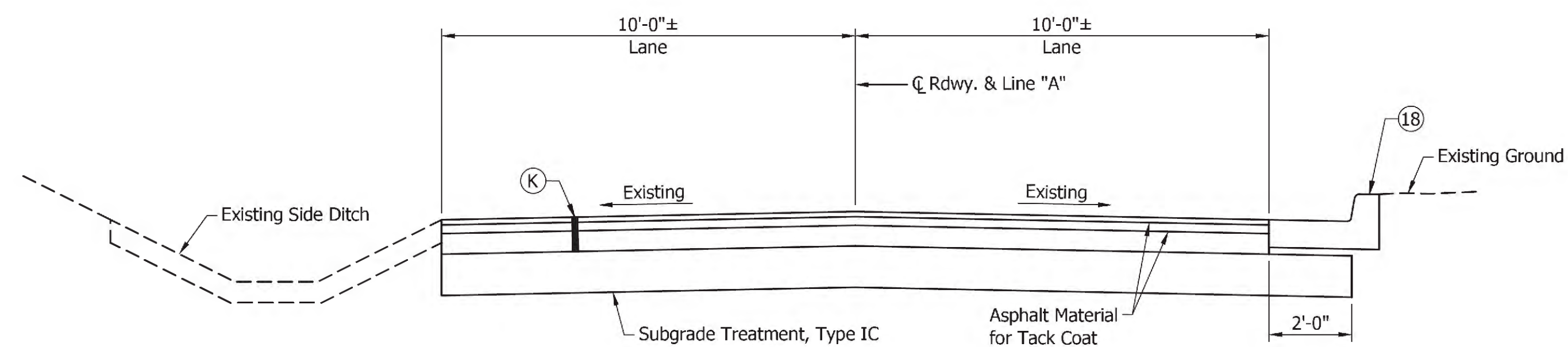
BRIDGE FILE	
P000-39-06876 C	
DESIGNATION	
2200147	
DRAWING NO.	SHEETS
1	of 14
CONTRACT	
B-44217	
PROJECT	
2200147	





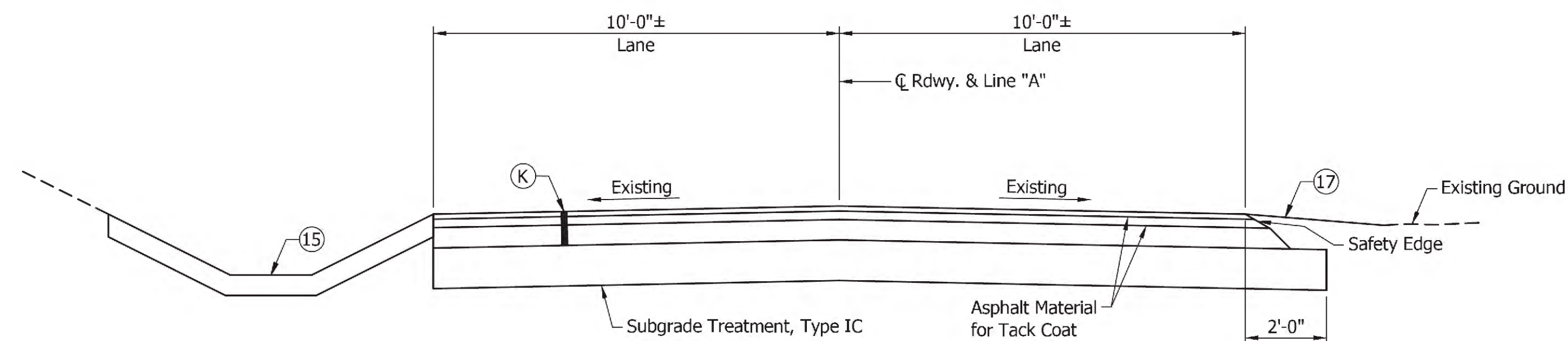
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- (K) 165 lbs/Syd. QC/QA HMA, 3, 64, Surface, 9.5mm on 275 lbs/Syd. QC/QA HMA, 3, 64, Intermediate, 19.0mm on 660 lbs/Syd. QC/QA HMA, 3, 64, Base, 25.0mm on Subgrade Treatment, Type IC
- (R) Transition Milling w/ 165 lbs/Syd. QC/QA HMA, 3, 64, Surface, 9.5mm
- (15) Paved Side Ditch, F
- (17) Linear Grading
- (18) Curb & Gutter, C, Modified (Match existing shape)



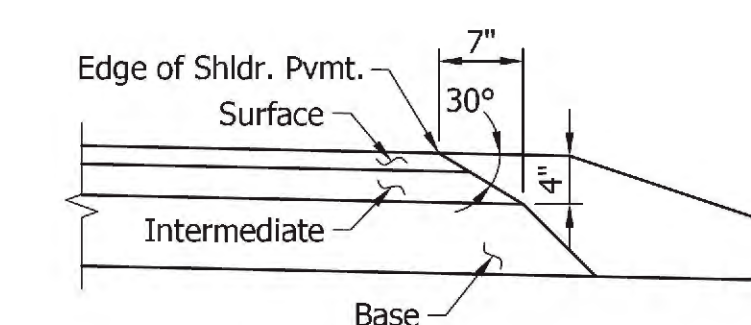
**TYPICAL SECTION - FULL DEPTH PAVEMENT**

Sta.98+84.93 "A" to Sta.98+94.93 "A"  
Scale: 3/8" = 1'-0"



**TYPICAL SECTION - FULL DEPTH PAVEMENT**

Sta.101+02.96 "A" to Sta.101+12.95 "A"  
Scale: 3/8" = 1'-0"

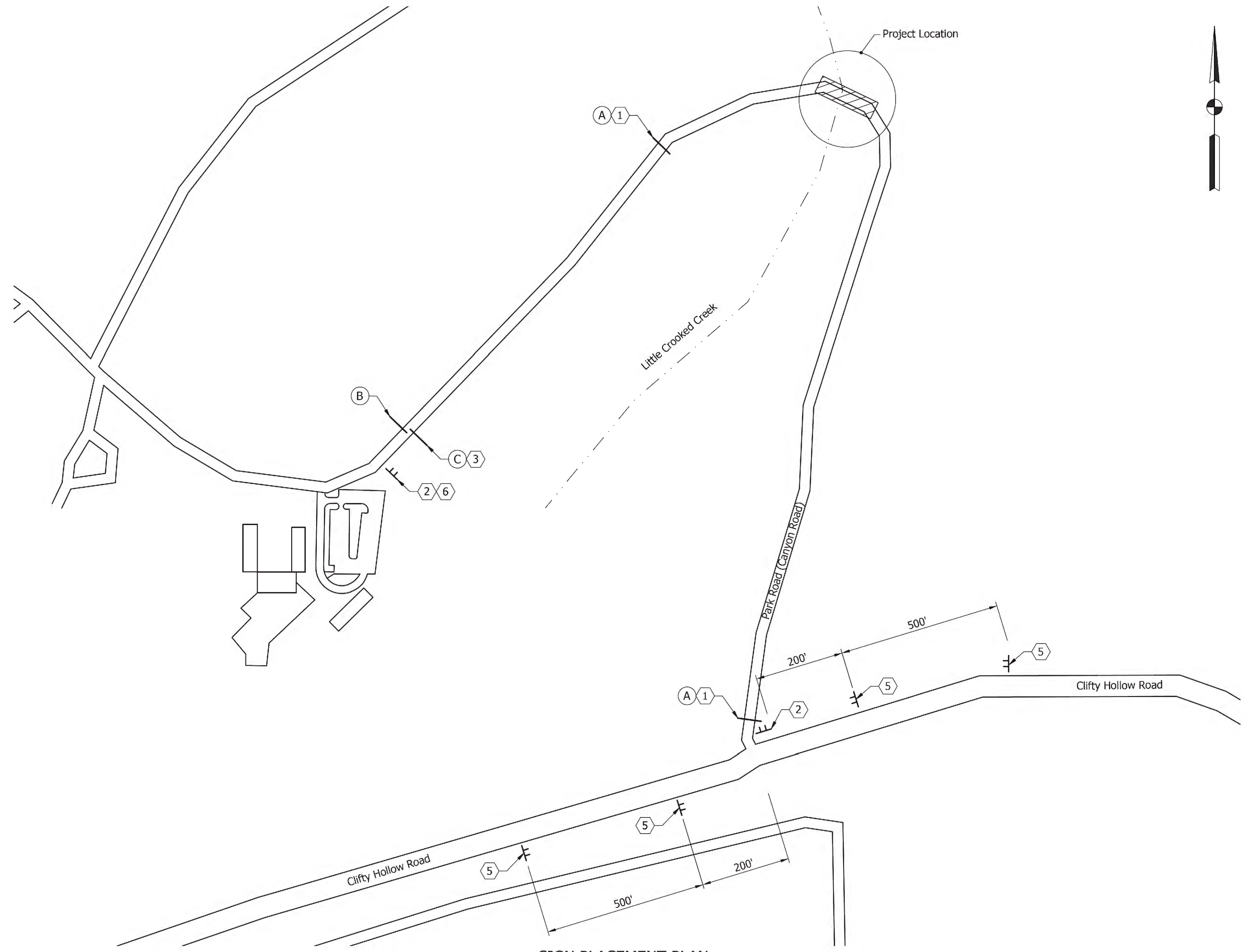


**30° SAFETY EDGE**

Scale: 3/4" = 1'-0"

	RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	INDIANA DEPARTMENT OF TRANSPORTATION	<table border="1" style="width: 100%; border-collapse: collapse;"> <tr> <td>HORIZONTAL SCALE</td> <td>BRIDGE FILE</td> </tr> <tr> <td>3/8" = 1'-0"</td> <td>P000-39-06876 C</td> </tr> <tr> <td>VERTICAL SCALE</td> <td>DESIGNATION</td> </tr> <tr> <td>3/8" = 1'-0"</td> <td>2200147</td> </tr> <tr> <td>DRAWING NO.</td> <td>SHEETS</td> </tr> <tr> <td></td> <td>3 of 14</td> </tr> <tr> <td>CONTRACT</td> <td>PROJECT</td> </tr> <tr> <td>B-44217</td> <td>2200147</td> </tr> </table>	HORIZONTAL SCALE	BRIDGE FILE	3/8" = 1'-0"	P000-39-06876 C	VERTICAL SCALE	DESIGNATION	3/8" = 1'-0"	2200147	DRAWING NO.	SHEETS		3 of 14	CONTRACT	PROJECT	B-44217	2200147
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3/8" = 1'-0"	P000-39-06876 C																		
VERTICAL SCALE	DESIGNATION																		
3/8" = 1'-0"	2200147																		
DRAWING NO.	SHEETS																		
	3 of 14																		
CONTRACT	PROJECT																		
B-44217	2200147																		
B-15	DESIGNED: TSW CHECKED: AE	DRAWN: LLB CHECKED: TSW	TYPICAL CROSS SECTIONS																





- LEGEND**
- (A) Barricade Type III-A & Road Closure Sign Assembly
  - (B) Barricade Type III-B
  - (C) Barricade Type III-B & Road Closure Sign Assembly
  - ▨ Project Location

- CONSTRUCTION SIGNS TYPE "A"**
- ① R11-2 Road Closed
  - ② X620-5 Road Closed on or After XX/XX/XX
  - ③ XW20-3 Road Closed Ahead

- CONSTRUCTION SIGNS TYPE "C"**
- ⑤ Park Entrance Closed Use North Entrance
  - ⑥ Park Exit Closed Use North Exit

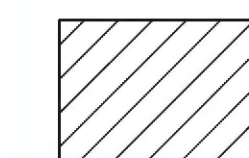
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Barricade, Type III-A	LFT	
Barricade, Type III-B	LFT	
Construction Sign, A	EACH	
Road Closure Sign Assembly	EACH	
Detour Route Marker Assembly	EACH	

**SIGN PLACEMENT PLAN**  
No Scale

SIGN NUMBER	IMUTCD CODE	SIGN MESSAGE	POST DESIGN		SIGN SIZE	SIGN COLOR		BORDER WIDTH	MARGIN WIDTH	LETTER HEIGHT SERIES - LINE 1	LETTER HEIGHT SERIES - LINE 2	LETTER HEIGHT SERIES - LINE 3	LETTER HEIGHT SERIES - LINE 4	WORD OR LINE	PCT	CORNER RADIUS	NO. OF POSTS	
			4'X4' WOOD	STEEL		BACKGROUND	COPY										1	2
5		SOUTH PARK ENTR. CLOSED USE NORTH ENTR. ON SR 56	*	B	60 x 36	Orange	Black	1/2	3/8	6 - Series C	6 - Series C	4 - Series C	4 - Series C			1 1/2		X
6		SOUTH PARK EXIT CLOSED USE NORTH EXIT ON SR 56	*	B	60 x 36	Orange	Black	1/2	3/8	6 - Series C	6 - Series C	4 - Series C	4 - Series C			1 1/2		X

RECOMMENDED FOR APPROVAL _____ DESIGN ENGINEER DATE	<b>INDIANA DEPARTMENT OF TRANSPORTATION</b>  <b>MAINTENANCE OF TRAFFIC</b>	HORIZONTAL SCALE	BRIDGE FILE
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		VERTICAL SCALE	DESIGNATION
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CHECKED: AE	CHECKED: TSW	4	of 14
B-16		CONTRACT	PROJECT
		B-44217	2200147





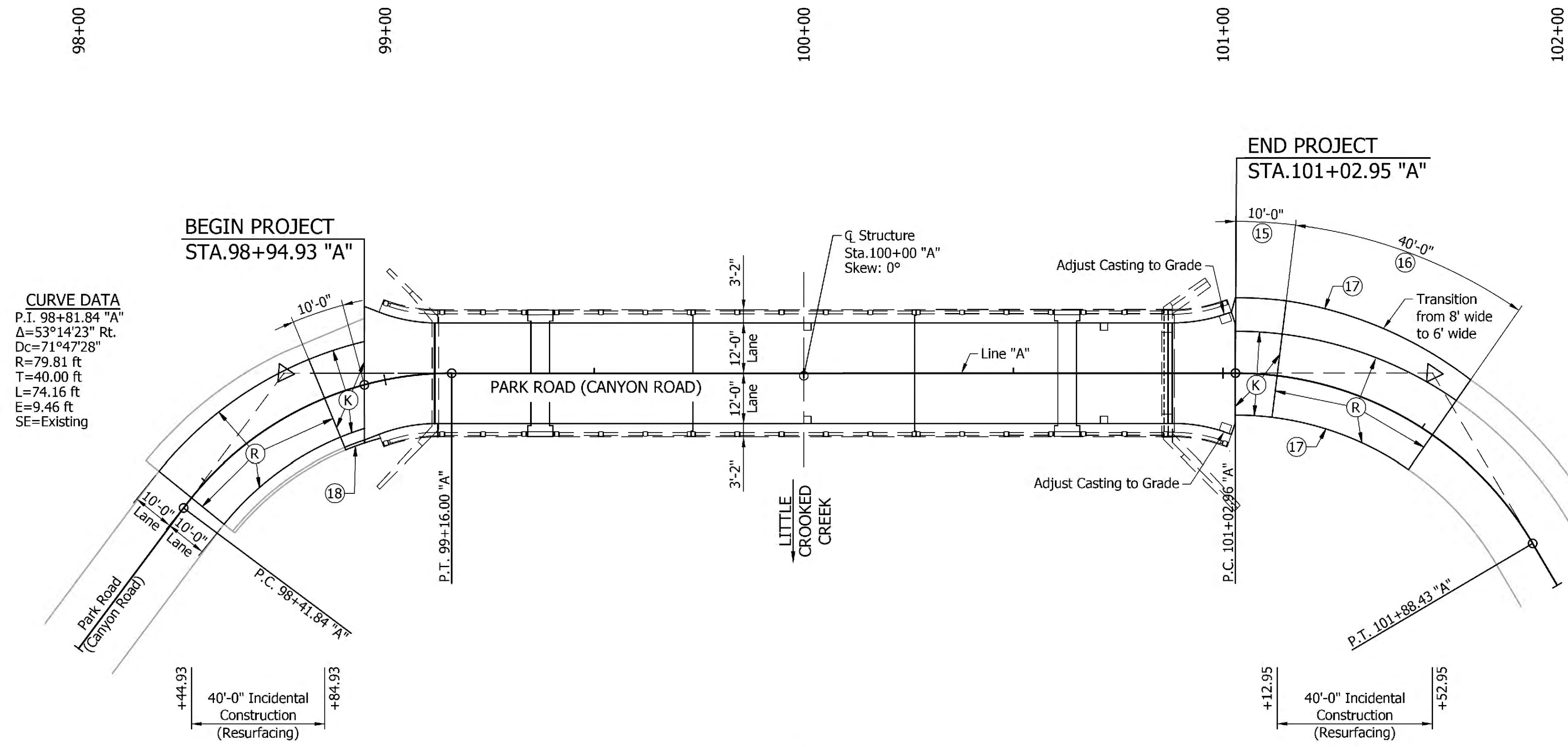
Anticipated Construction Access Route.

Notes:

The Contractor shall make every effort to minimize distance to trees and vegetation along the Construction Access Route.  
Any tree cleared along Construction Access Route shall be replanted. (See Special Provisions)

		RECOMMENDED FOR APPROVAL _____	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE	BRIDGE FILE	
		DESIGNED: TSW		DRAWN: LLB	1" = 40'	P000-39-06876 C
		CHECKED: AE		CHECKED: TSW	VERTICAL SCALE	DESIGNATION
					1" = 40'	2200147
	B-17		CONSTRUCTION ACCESS	DRAWING NO.	SHEETS	
				CONTRACT	5 of 14	
				B-44217	PROJECT 2200147	

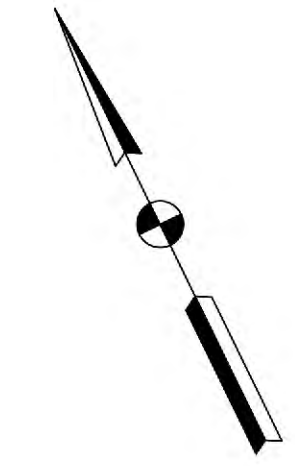




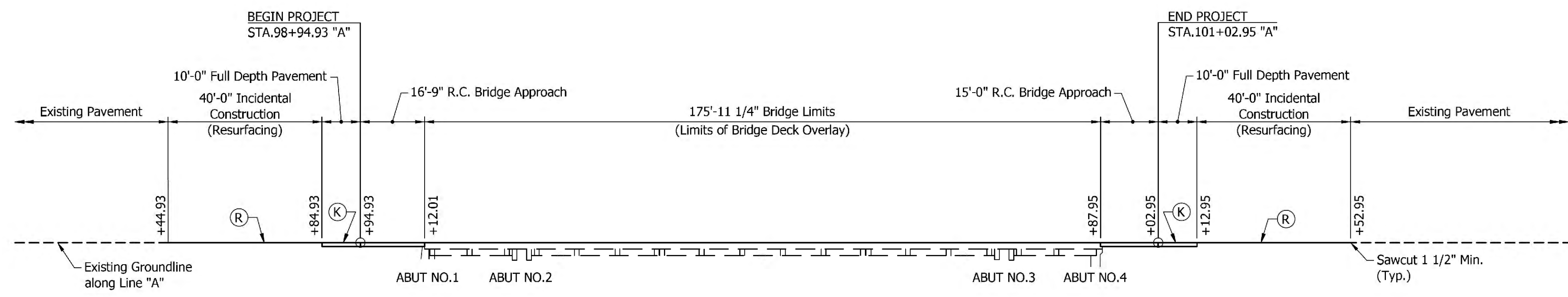
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 $D_c=71^{\circ}47'28''$   
 $R=79.81$  ft  
 $T=40.00$  ft  
 $L=74.16$  ft  
 $E=9.46$  ft  
 SE=Existing

**CURVE DATA**  
 P.T. 101+50.00 "A"  
 $\Delta=59^{\circ}30'00''$  Rt.  
 $D_c=69^{\circ}36'57''$   
 $R=82$  ft  
 $T=47.04$  ft  
 $L=85.47$  ft  
 $E=12.49$  ft  
 SE=Existing

- LEGEND**
- (K) 165 lbs/Syd. QC/QA HMA, 3, 64, Surface, 9.5mm on 275 lbs/Syd. QC/QA HMA, 3, 64, Intermediate, 19.0mm on 660 lbs/Syd. QC/QA HMA, 3, 64, Base, 25.0mm on Subgrade Treatment, Type IC
  - (R) Transition Milling w/ 165 lbs/Syd. QC/QA HMA, 3, 64, Surface, 9.5mm
  - (15) Paved Side Ditch, F
  - (16) Paved Side Ditch, F, Modified Transition from 2' flat bottom to V-Ditch to match existing.
  - (17) Linear Grading
  - (18) Curb & Gutter, C, Modified (Match existing shape)



**PLAN**  
 Scale: 1" = 20'



**LONGITUDINAL SECTION**  
 Scale: 1" = 20'

Note:  
 For General Notes & Typical Sections, see Dwg.C3.  
 The alignment and stations shown are based on the 2000 plan set (Contract #B-24095) and it was extended to the west utilizing aerial photography.  
 For Transition Milling Details, see Std.Dwg.No.E306-TMPT-01

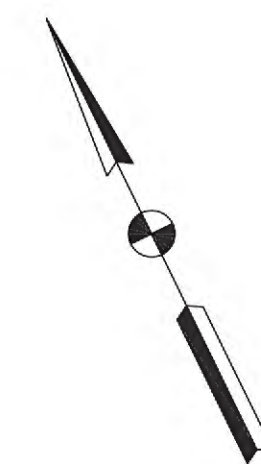
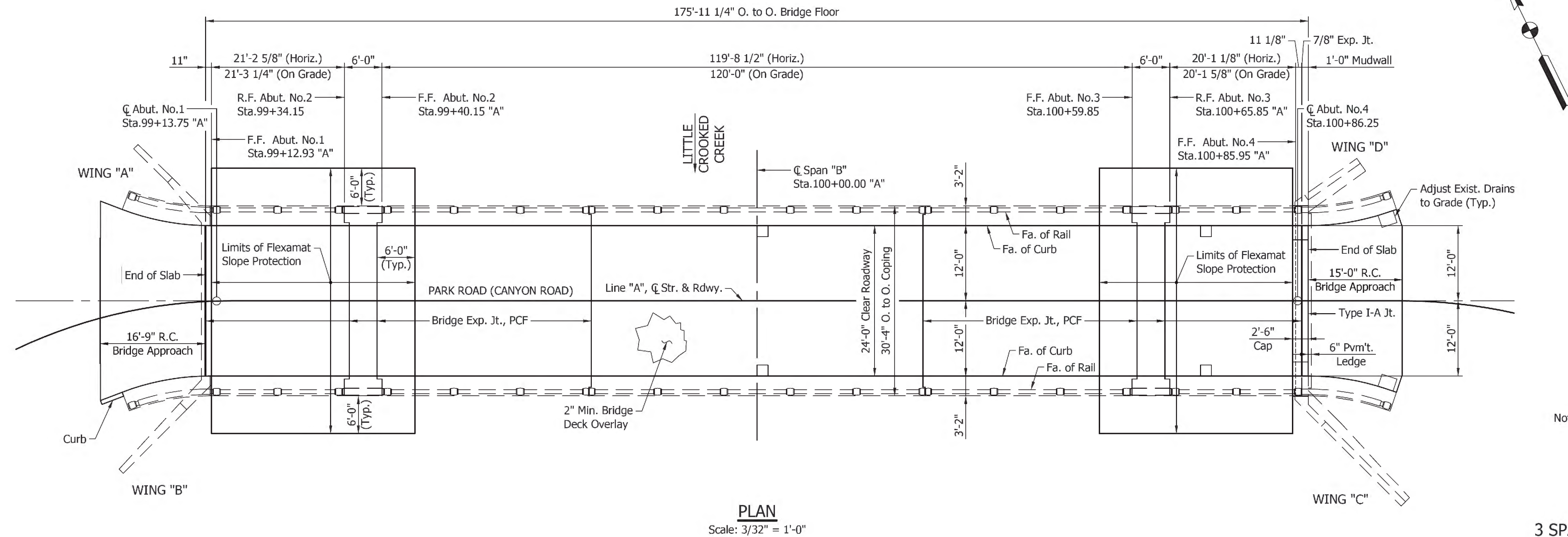
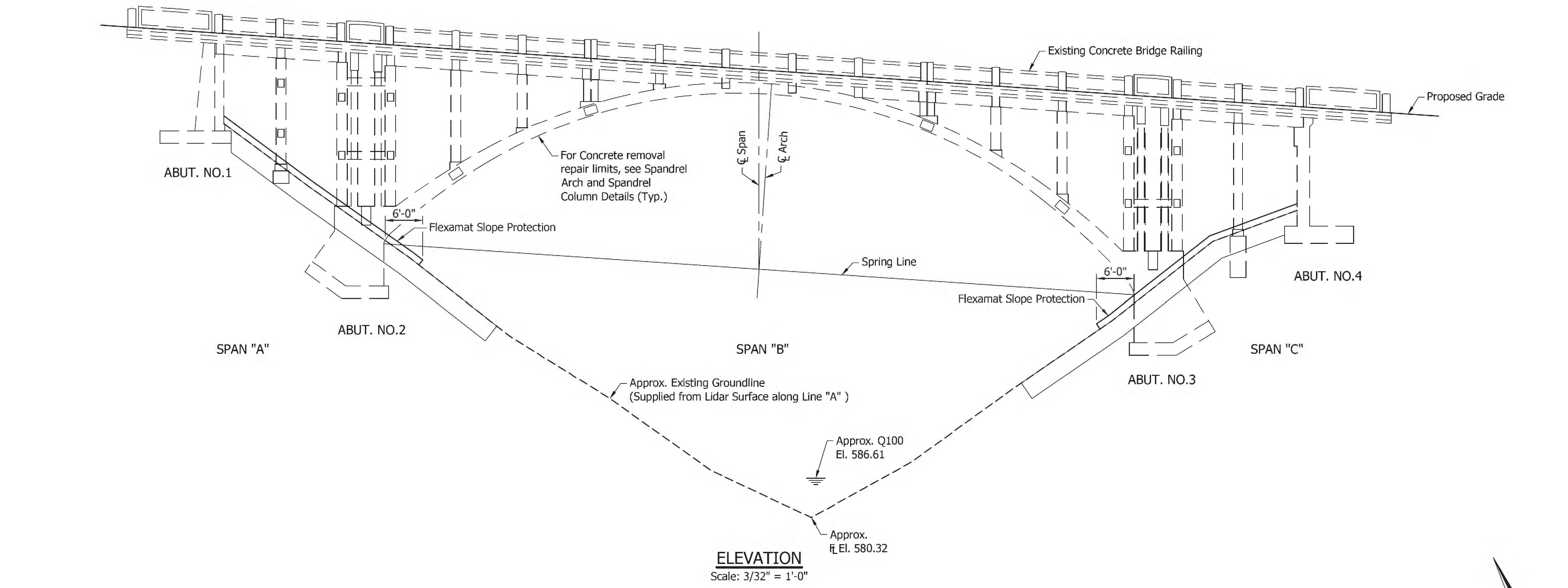
	RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1" = 20'	BRIDGE FILE P000-39-06876 C	
	DESIGNED: TSW	DRAWN: LLB			CONSTRUCTION LAYOUT DETAILS	VERTICAL SCALE 1" = 20'	DESIGNATION 2200147
	CHECKED: AE	CHECKED: TSW				DRAWING NO.	SHEETS
						CONTRACT B-44217	6 of 14 PROJECT 2200147







EXISTING STRUCTURE BUILT TO A -6.8% GRADE



Notes:  
 For General Notes & Typical Sections, see Dwg.C3.  
 The alignment and stations shown are based on the 2000 plan set (Contract #B-24095) and it was extended to the west utilizing aerial photography.

**OPEN SPANDREL ARCH BRIDGE**  
 3 SPAN: 21'-2 5/8", 119'-8 1/2" & 20'-1 1/8"  
 24'-0" CLEAR ROADWAY SKEW: 0°  
 PARK ROAD (CANYON ROAD) OVER LITTLE CROOKED CREEK  
 JEFFERSON COUNTY

RECOMMENDED FOR APPROVAL _____ DESIGNED: TSW CHECKED: AE	DESIGN ENGINEER _____ DRAWN: LLB CHECKED: TSW	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 3/32" = 1'-0"	BRIDGE FILE P000-39-06876 C
			VERTICAL SCALE 3/32" = 1'-0"	DESIGNATION 2200147
GENERAL PLAN PROPOSED		DRAWING NO. C2 of C7	SHEETS 8 of 14	
		CONTRACT B-44217	PROJECT 2200147	



**GENERAL NOTES**

Reinforcing Steel covering shall be 2 1/2" in top and 1" minimum in the bottom of the floor slab, and 2" in all other parts, unless noted.

The face of curb, top of sidewalk and concrete railing to be sealed in accordance with Article 702.21 of the Specifications. (Estimated Quantity = XXXX Sft.)

Where new work is to be fitted to the old work, the Contractor shall check and verify all dimensions, elevations and conditions in the field and report any errors or discrepancies to the Engineer and assume responsibility for their correctness and the fit of the new construction to the existing structure.

Plans for original structure and subsequent rehabs are on file at the Indiana Department of Natural Resources and are available upon request.

**DESIGN DATA**

**LIVE LOAD**

The design load for the original portions of the bridge is unknown. The portions of the bridge built as part of the 1980 rehabilitation were designed for HS 20-44 loading in accordance with the 1977 AASHTO Bridge Design Specifications.

**DESIGN STRENGTHS**

**CONCRETE:**

- Class "A":  $f_c=3,500$  psi
- Class "B":  $f_c=3,000$  psi
- Class "C":  $f_c=4,000$  psi

**REINFORCING STEEL:**

- Grade 60:  $f_y=60,000$  psi

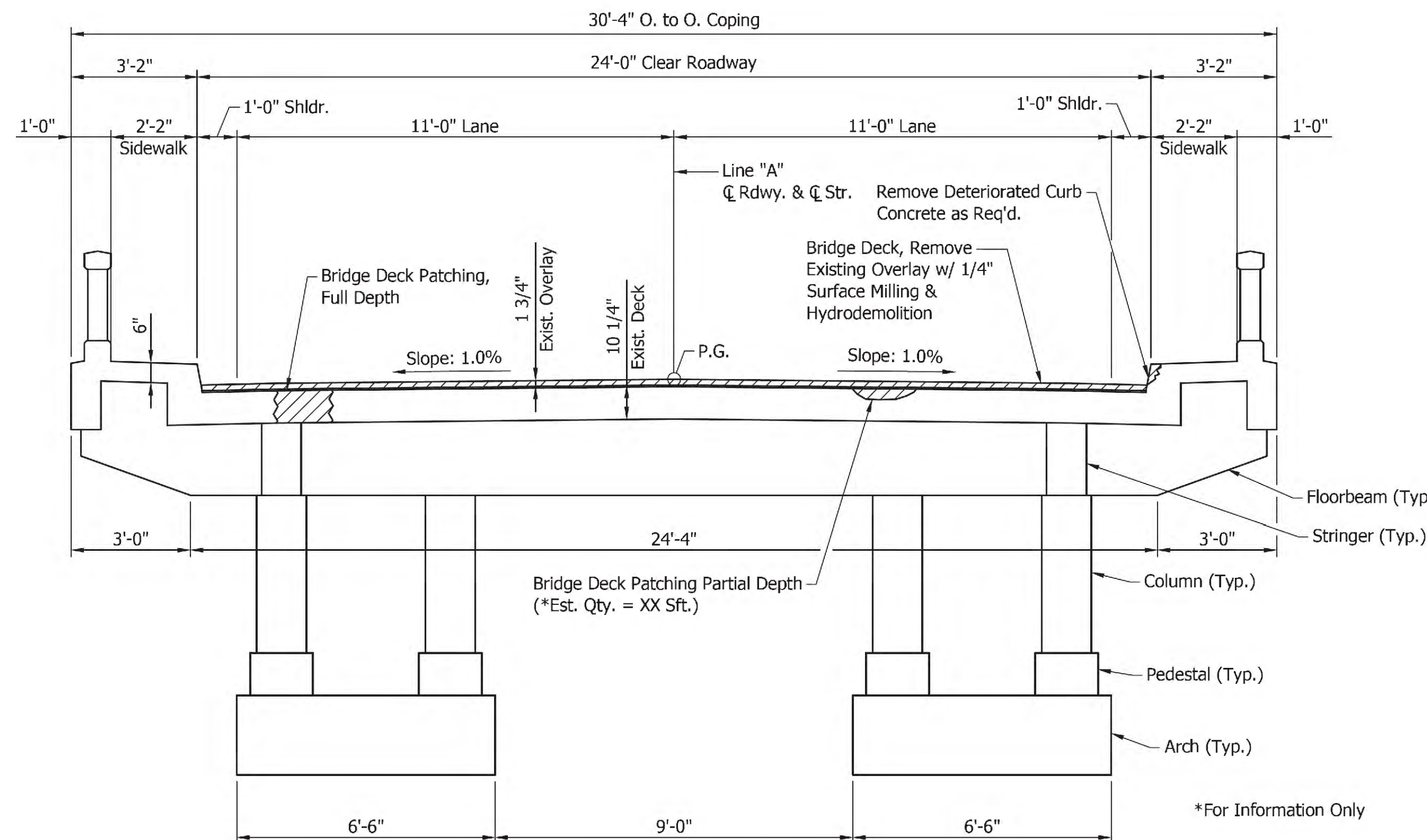
**MATERIAL NOTES**

**BRIDGE DECK OVERLAY**

2" Latex Modified Portland Cement Concrete or 2" Silica Fume Modified Structural Concrete.

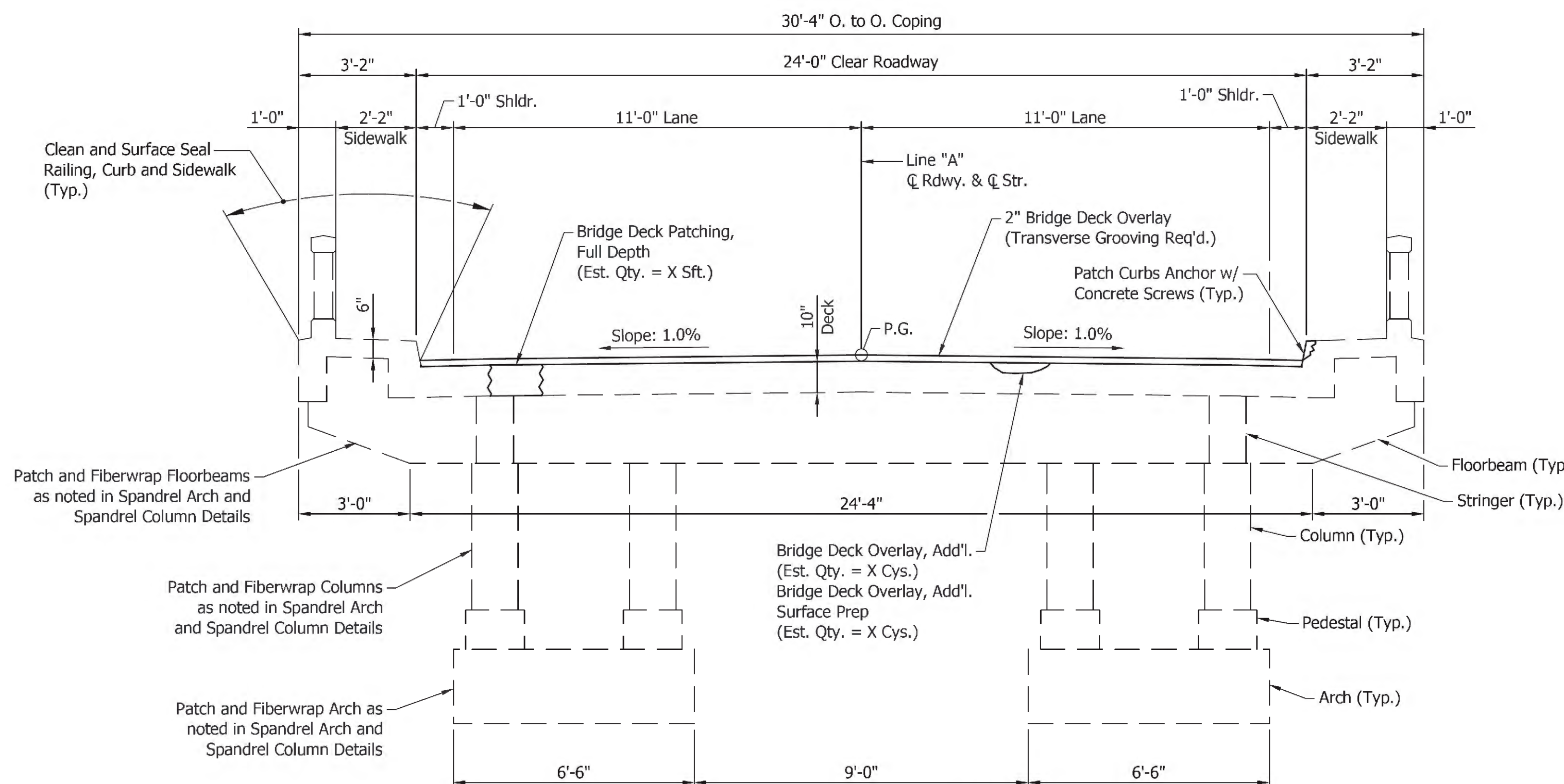
**HYDRAULIC DATA**

Drainage Area	0.32 Sq Mi
Design Discharge, Q100	360 cfs
High Water Elevation, Q100 (Scour Analysis)	El.586.61
Contraction Scour, Q100	0.20 ft.
Total Scour, Q100	12.57 ft.
Flowline Elevation	El. 580.32
Low Scour Elev., Q100	El. 567.75
Max. Velocity, Q100	7.28 ft/sec.
Avg. Velocity, Q100	5.93 Ft/sec



**TYPICAL SECTION - EXISTING**

Scale: 3/8" = 1'-0"



**TYPICAL SECTION - PROPOSED**

Scale: 3/8" = 1'-0"

- Notes:
- For Plan & Elevation, see Dwgs.C1 & C2.
  - Hatched area indicates portions to be removed.
  - For Spandrel Arch and Spandrel Column Details, see Dwgs.C4-C7.

**OPEN SPANDREL ARCH BRIDGE**  
3 SPAN: 21'-2 5/8", 119'-8 1/2" & 20'-1 1/8"  
24'-0" CLEAR ROADWAY SKEW: 0°  
PARK ROAD (CANYON ROAD) OVER LITTLE CROOKED CREEK  
JEFFERSON COUNTY

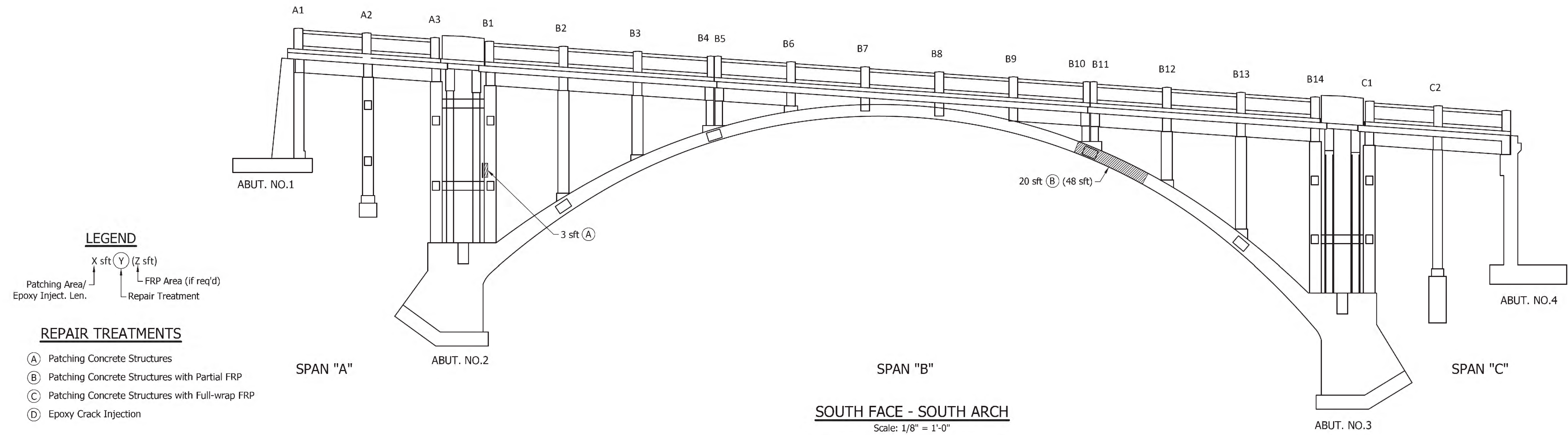
RECOMMENDED FOR APPROVAL	DESIGN ENGINEER	DATE
DESIGNED: TSW	DRAWN: LLB	
CHECKED: AE	CHECKED: TSW	

INDIANA  
DEPARTMENT OF TRANSPORTATION

GENERAL PLAN  
TYPICAL SECTIONS

HORIZONTAL SCALE	BRIDGE FILE
3/8" = 1'-0"	P000-39-06876 C
VERTICAL SCALE	DESIGNATION
3/8" = 1'-0"	2200147
DRAWING NO.	SHEETS
C3 of C7	9 of 14
CONTRACT	PROJECT
B-44217	2200147



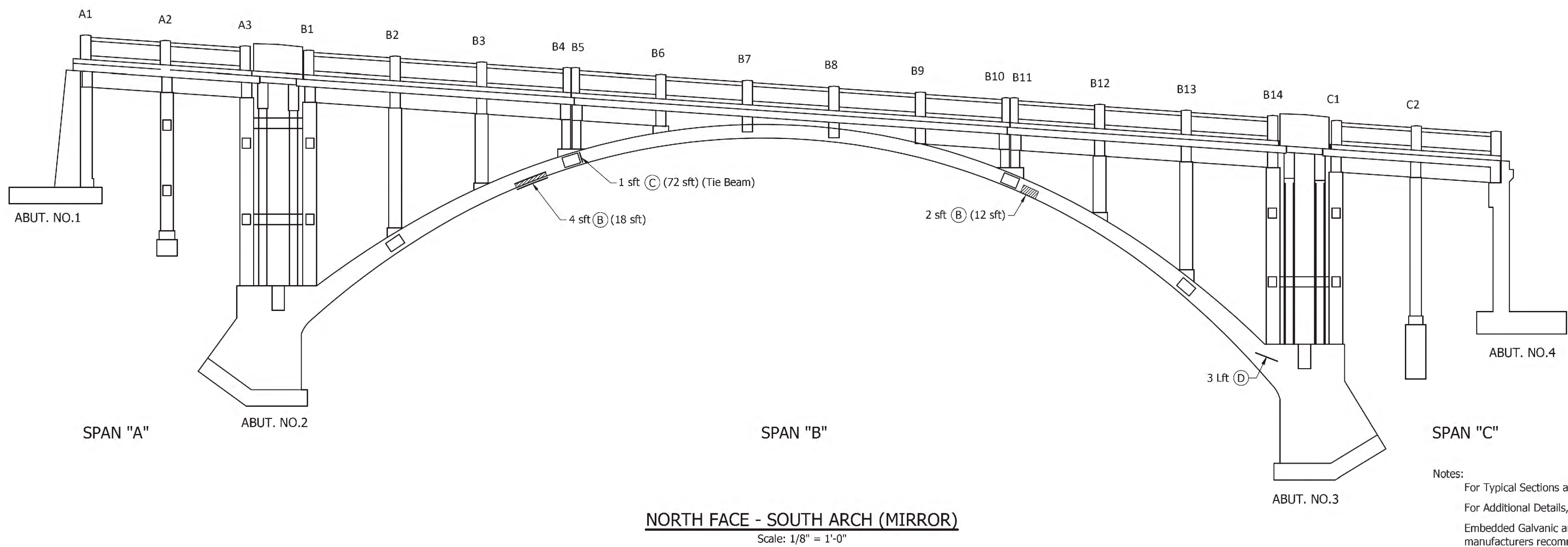


**LEGEND**

X sft (Y) (Z sft)  
 Patching Area/  
 Epoxy Inject. Len.      FRP Area (if req'd)  
 Repair Treatment

**REPAIR TREATMENTS**

(A) Patching Concrete Structures  
 (B) Patching Concrete Structures with Partial FRP  
 (C) Patching Concrete Structures with Full-wrap FRP  
 (D) Epoxy Crack Injection



Notes:  
 For Typical Sections and General Notes, see Dwg. C3.  
 For Additional Details, see Dwgs. C5-C7.  
 Embedded Galvanic anodes shall be placed in accordance with the manufacturers recommendations in all patches 4 sft or greater.

	RECOMMENDED FOR APPROVAL _____	INDIANA DEPARTMENT OF TRANSPORTATION	HORIZONTAL SCALE 1/8" = 1'-0"	BRIDGE FILE P000-39-06876 C
	DESIGNED: TSW	DRAWN: LLB	VERTICAL SCALE 1/8" = 1'-0"	DESIGNATION 2200147
	CHECKED: AE	CHECKED: TSW	DRAWING NO. SHEETS C4 of C7 10 of 14	
	SPANDREL ARCH & COLUMN DETAILS		CONTRACT PROJECT B-44217 2200147	

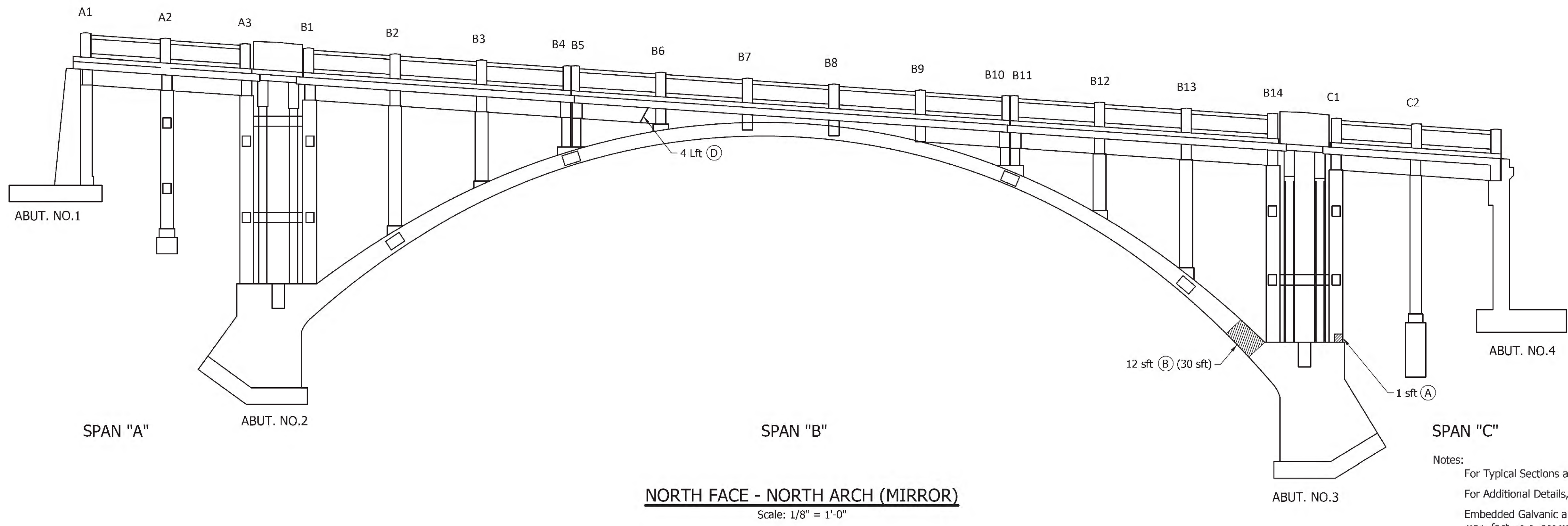
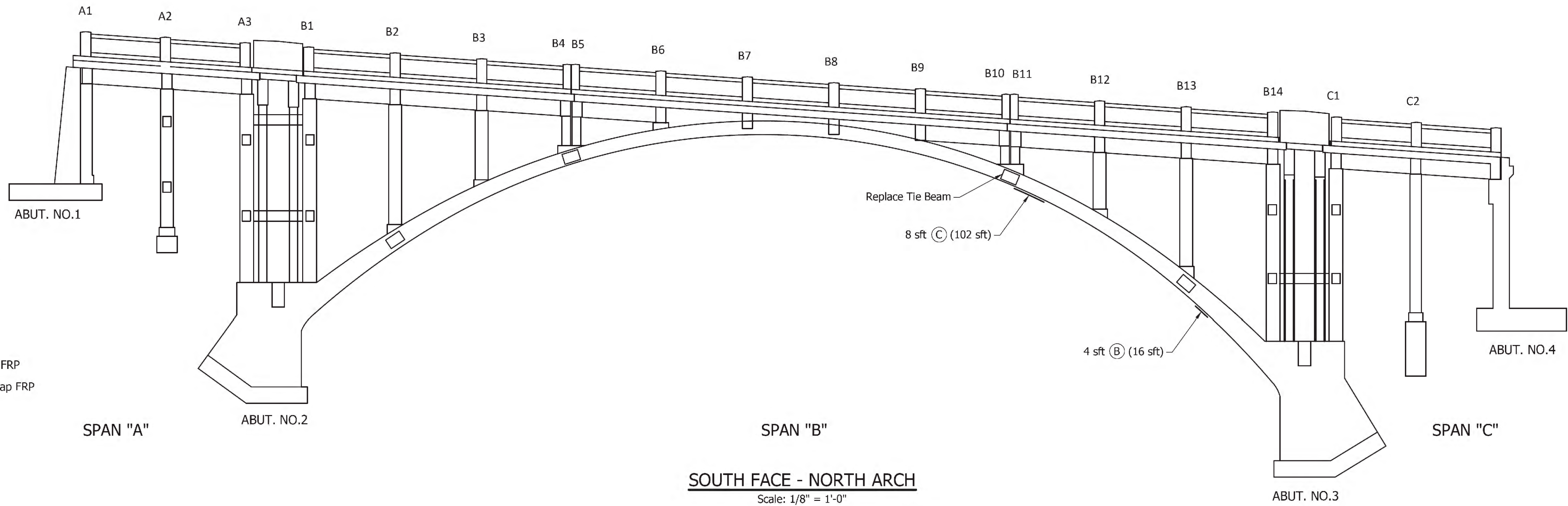


**LEGEND**

X sft (Y) (Z sft)  
 Patching Area/  
 Epoxy Inject. Len.      FRP Area (if req'd)  
 Repair Treatment

**REPAIR TREATMENTS**

- (A) Patching Concrete Structures
- (B) Patching Concrete Structures with Partial FRP
- (C) Patching Concrete Structures with Full-wrap FRP
- (D) Epoxy Crack Injection



Notes:  
 For Typical Sections and General Notes, see Dwg. C3.  
 For Additional Details, see Dwgs. C4, C6-C7.  
 Embedded Galvanic anodes shall be placed in accordance with the manufacturers recommendations in all patches 4 sft or greater.

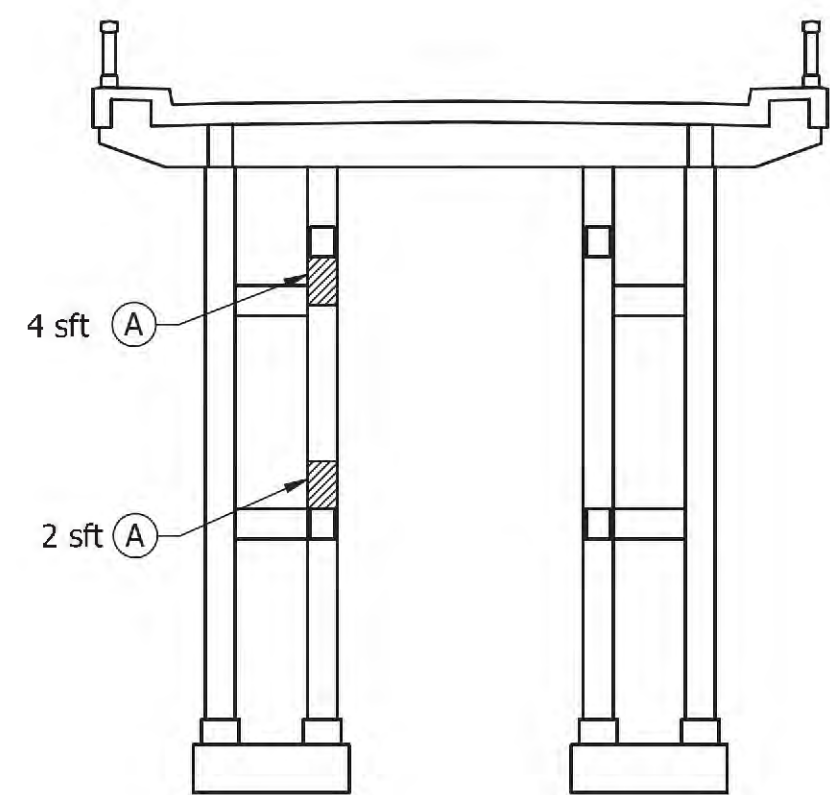
RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: TSW	DRAWN: LLB	
CHECKED: AE	CHECKED: TSW	

INDIANA  
 DEPARTMENT OF TRANSPORTATION

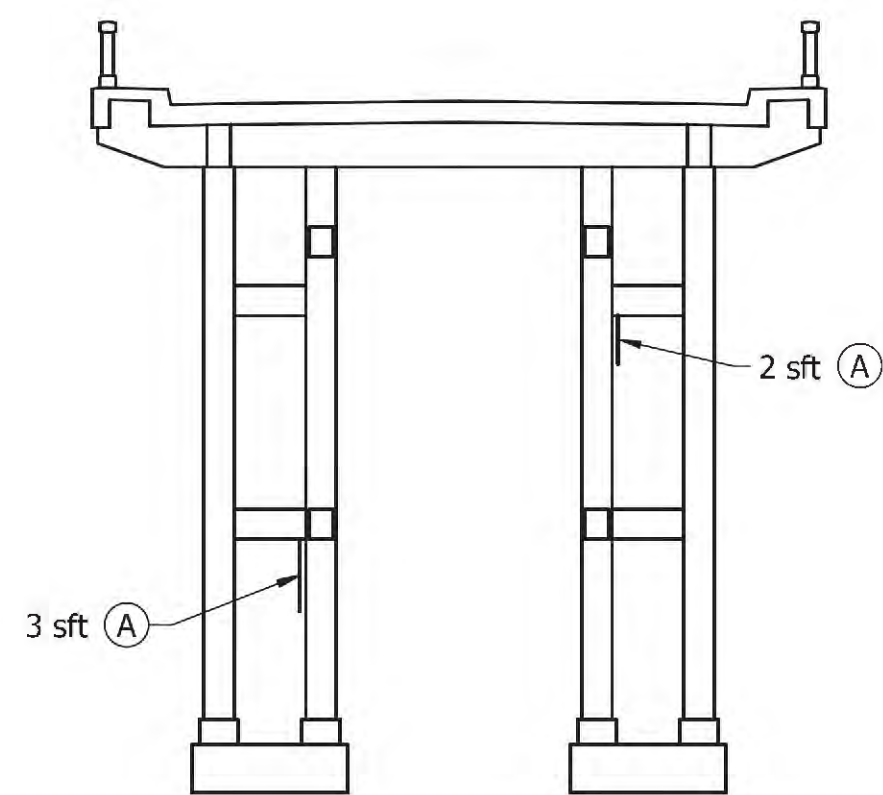
**SPANDREL ARCH & COLUMN DETAILS**

HORIZONTAL SCALE	BRIDGE FILE
1/8" = 1'-0"	P000-39-06876 C
VERTICAL SCALE	DESIGNATION
1/8" = 1'-0"	2200147
DRAWING NO.	SHEETS
C5 of C7	11 of 14
CONTRACT	PROJECT
B-44217	2200147

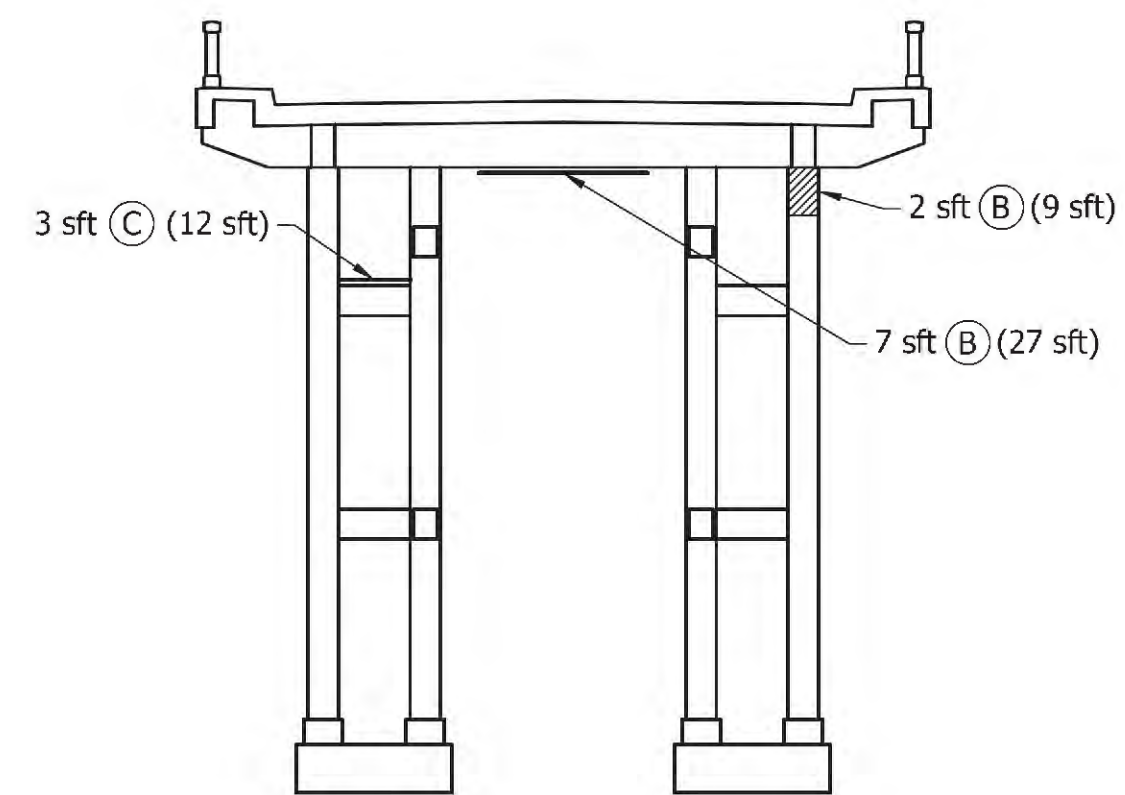




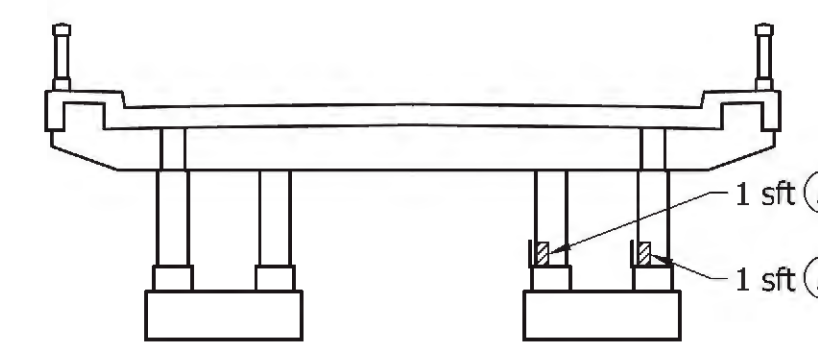
**COLUMN A3 - EAST FACE**  
Scale: 1/8" = 1'-0"



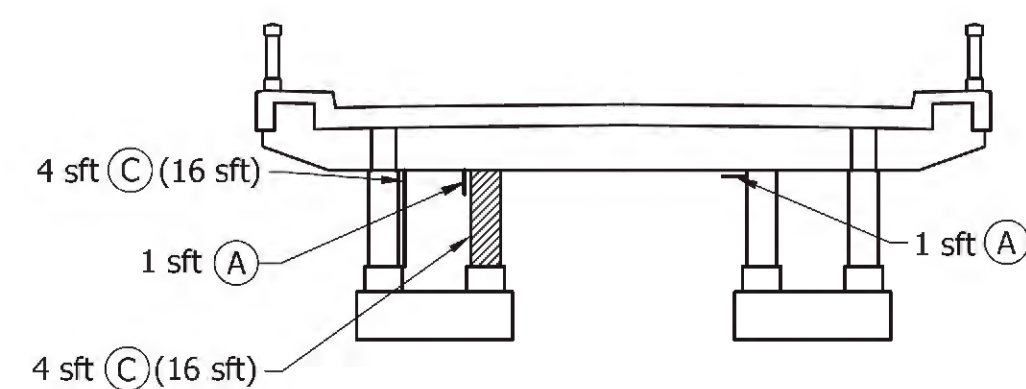
**COLUMN B1 - WEST FACE**  
Scale: 1/8" = 1'-0"



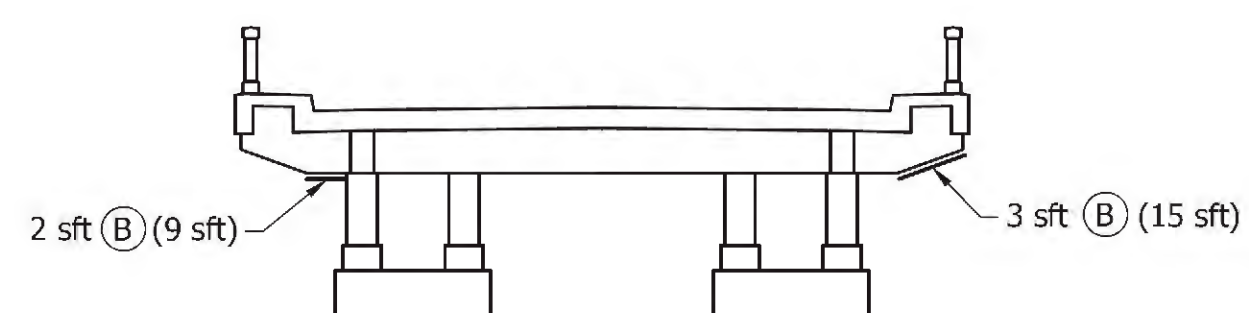
**COLUMN B1 - EAST FACE**  
Scale: 1/8" = 1'-0"



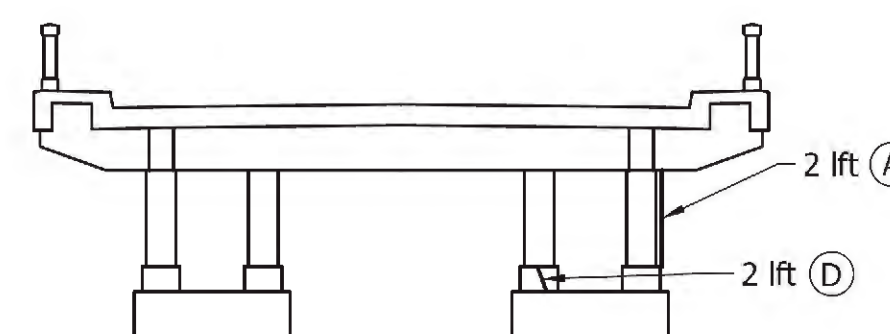
**COLUMN B4 - EAST FACE**  
Scale: 1/8" = 1'-0"



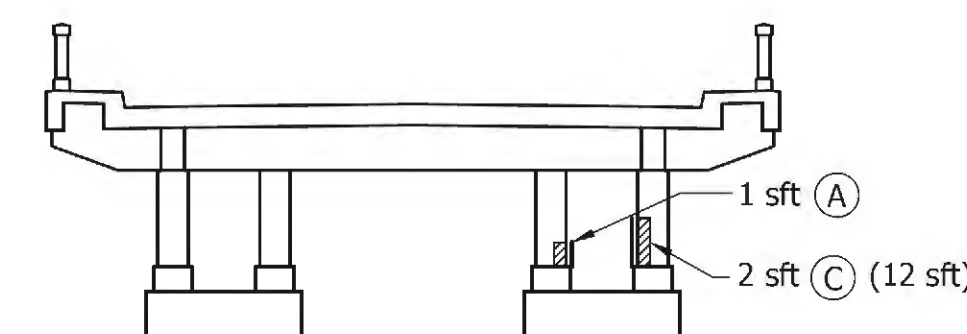
**COLUMN B5 - EAST FACE**  
Scale: 1/8" = 1'-0"



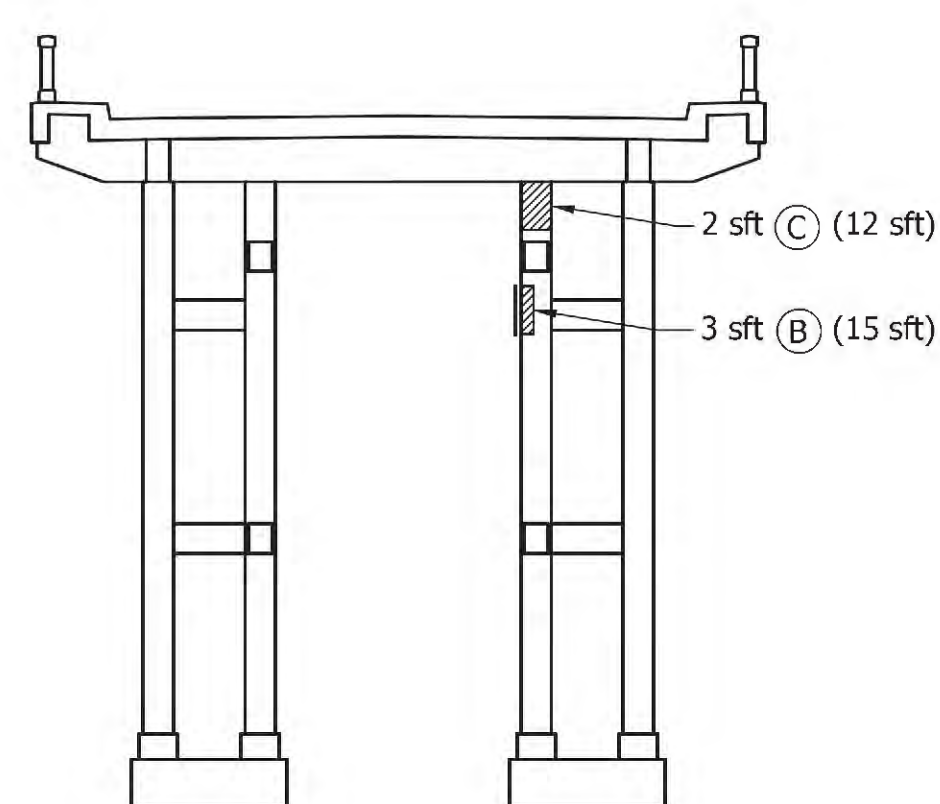
**COLUMN B10 - WEST FACE**  
Scale: 1/8" = 1'-0"



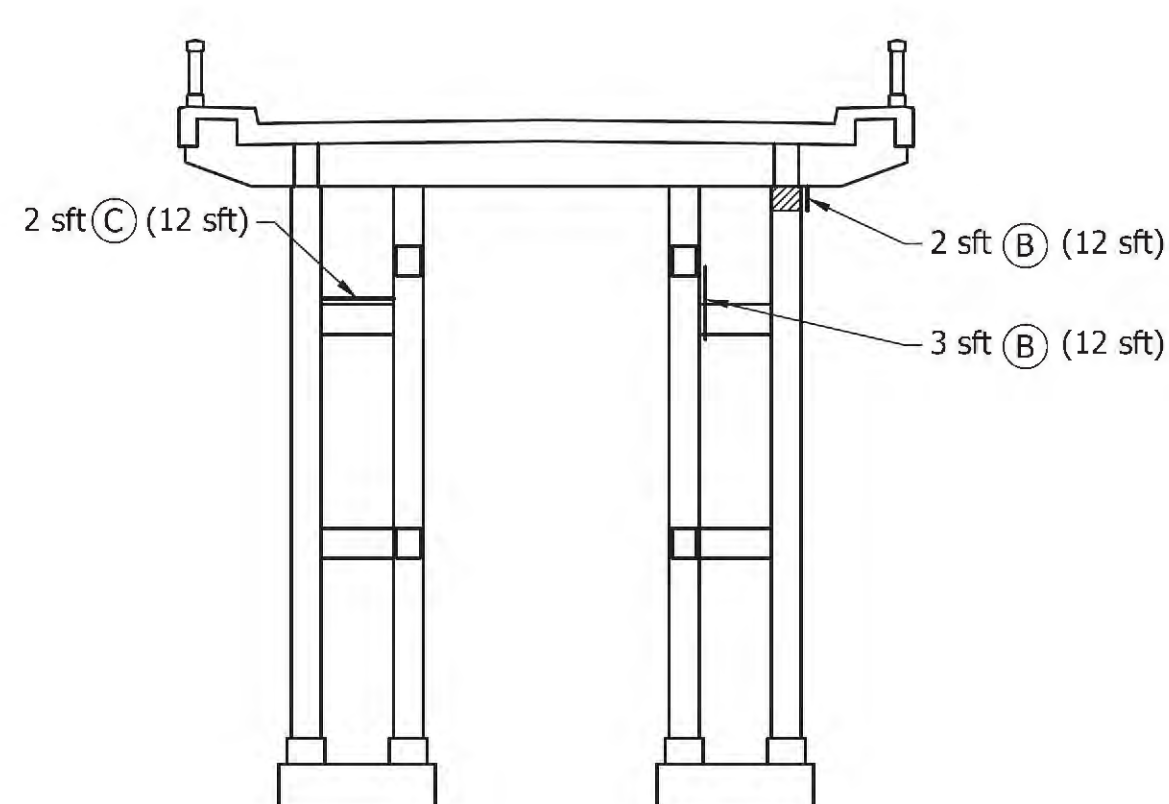
**COLUMN B11 - EAST FACE**  
Scale: 1/8" = 1'-0"



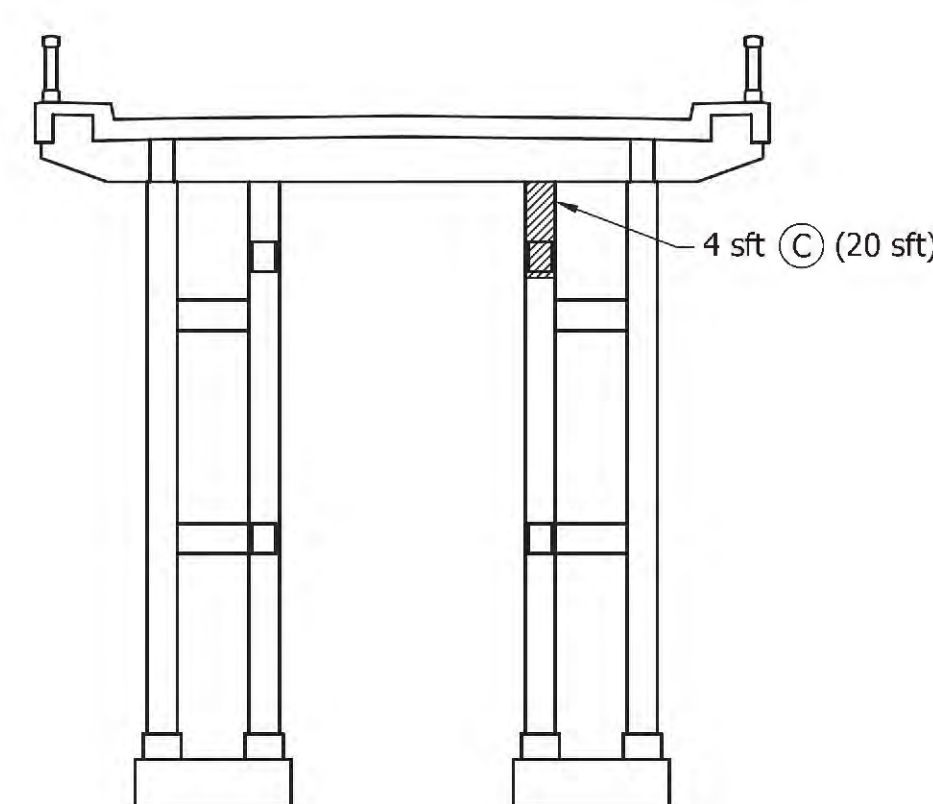
**COLUMN B12 - WEST FACE**  
Scale: 1/8" = 1'-0"



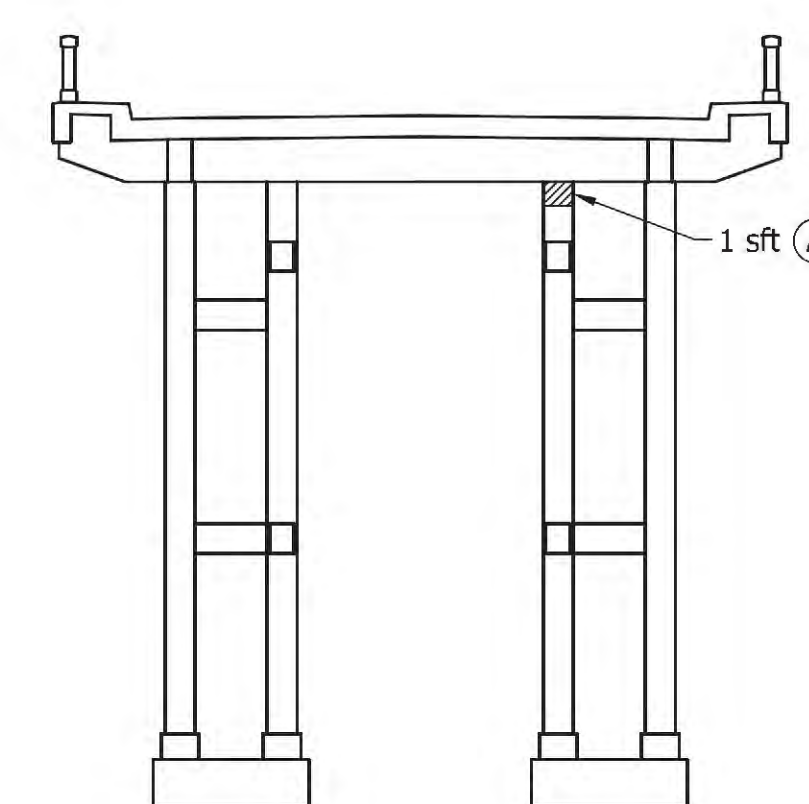
**COLUMN B14 - EAST FACE**  
Scale: 1/8" = 1'-0"



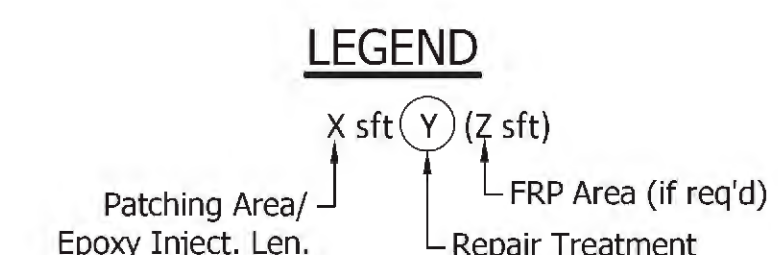
**COLUMN B14 - WEST FACE**  
Scale: 1/8" = 1'-0"



**COLUMN C1 - EAST FACE**  
Scale: 1/8" = 1'-0"



**COLUMN C1 - WEST FACE**  
Scale: 1/8" = 1'-0"



- REPAIR TREATMENTS**
- (A) Patching Concrete Structures
  - (B) Patching Concrete Structures with Partial FRP
  - (C) Patching Concrete Structures with Full-wrap FRP
  - (D) Epoxy Crack Injection

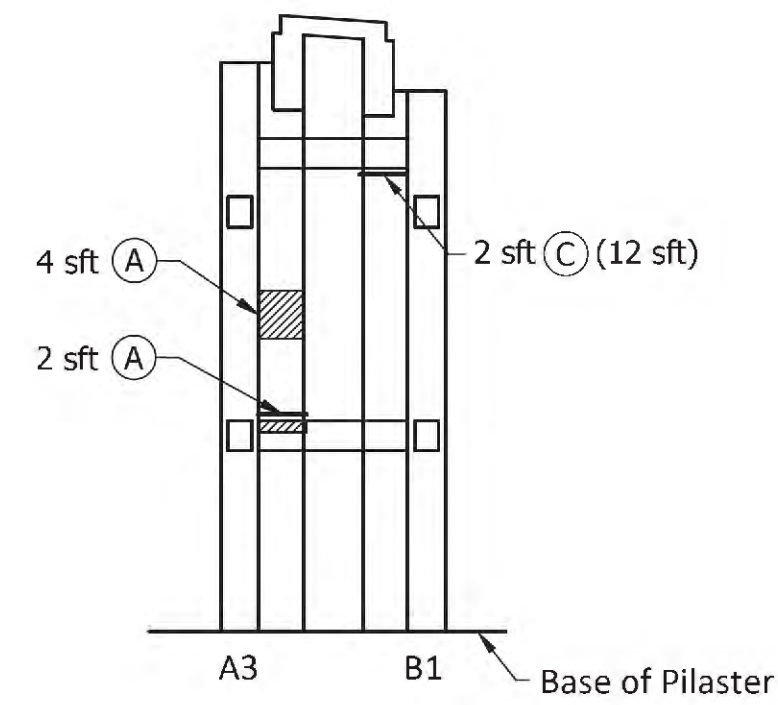
Notes:  
 For Typical Sections and General Notes, see Dwg. C3.  
 For Additional Details, see Dwgs. C4-C6 & C7.  
 Embedded Galvanic anodes shall be placed in accordance with the manufacturers recommendations in all patches 4 sft or greater.

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: TSW	DRAWN: LLB	
CHECKED: AE	CHECKED: TSW	

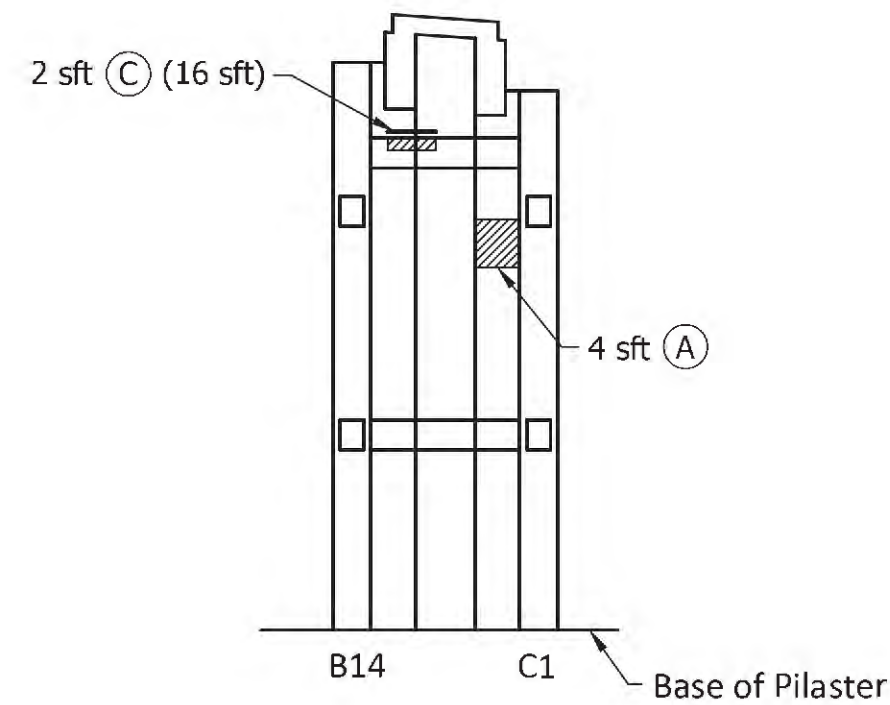
INDIANA  
 DEPARTMENT OF TRANSPORTATION  
 SPNADREL ARCH & COLUMN DETAILS

HORIZONTAL SCALE	BRIDGE FILE
1/8" = 1'-0"	P000-39-06876 C
VERTICAL SCALE	DESIGNATION
1/8" = 1'-0"	2200147
DRAWING NO.	SHEETS
C6 of C7	12 of 14
CONTRACT	PROJECT
B-44217	2200147

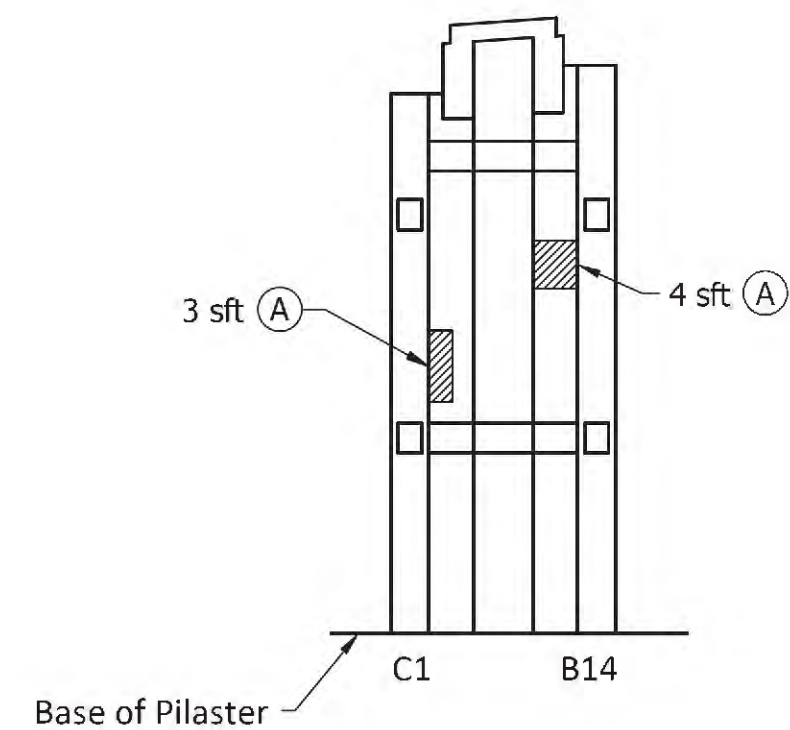




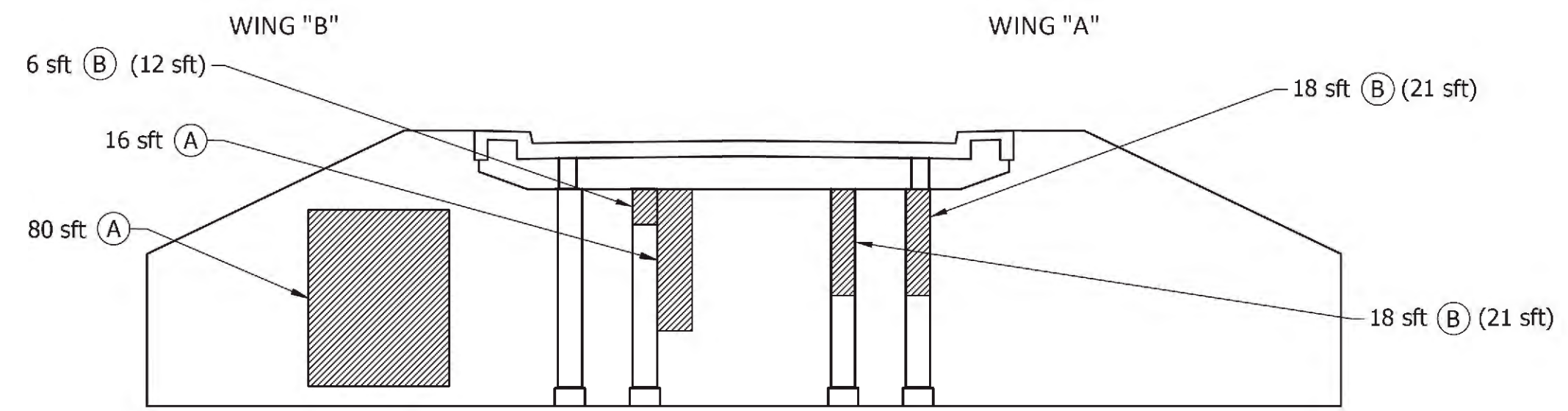
**PIER NO. 2 - NORTH PILASTER SOUTH FACE**  
Scale: 1/8" = 1'-0"



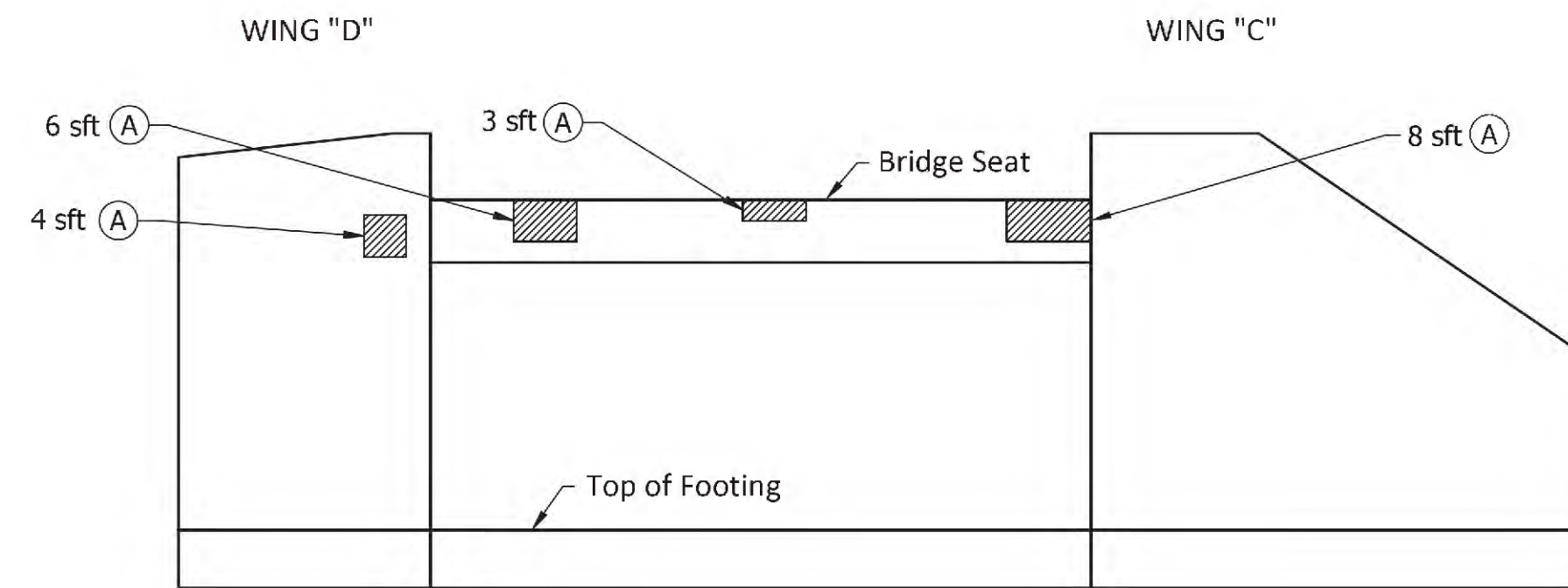
**PIER NO. 3 - NORTH PILASTER SOUTH FACE**  
Scale: 1/8" = 1'-0"



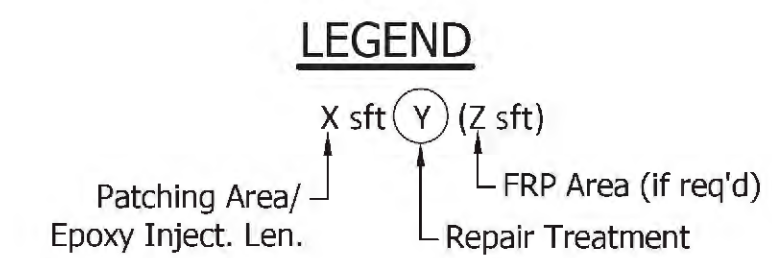
**PIER NO. 3 - SOUTH PILASTER NORTH FACE**  
Scale: 1/8" = 1'-0"



**ABUTMENT NO. 1 - EAST FACE**  
Scale: 1/8" = 1'-0"



**ABUTMENT NO. 4 - WEST FACE**  
Scale: 1/8" = 1'-0"



**REPAIR TREATMENTS**

- (A) Patching Concrete Structures
- (B) Patching Concrete Structures with Partial FRP
- (C) Patching Concrete Structures with Full-wrap FRP
- (D) Epoxy Crack Injection

Notes:  
For Typical Sections and General Notes, see Dwg. C3.  
For Additional Details, see Dwgs. C4-C6.  
Embedded Galvanic anodes shall be placed in accordance with the manufacturers recommendations in all patches 4 sft or greater.

RECOMMENDED FOR APPROVAL _____	DESIGN ENGINEER _____	DATE _____
DESIGNED: TSW	DRAWN: LLB	
CHECKED: AE	CHECKED: TSW	

INDIANA  
DEPARTMENT OF TRANSPORTATION

SPANDREL ARCH & COLUMN DETAILS

HORIZONTAL SCALE	BRIDGE FILE
1/8" = 1'-0"	P000-39-06876 C
VERTICAL SCALE	DESIGNATION
1/8" = 1'-0"	2200147
DRAWING NO.	SHEETS
C7 of C7	13 of 14
CONTRACT	PROJECT
B-44217	2200147